



Updated:

7/25/15 @ 1:00pm

Added communications for the following items:

Item 2 - Rezoning 711 E 23rd St

Item 4 - Special Use Permit 1501 Learnard Ave

Item 5C - Preliminary Plat Naismith Creek Addition 751 W 29th Terr

7/21/16 @ 11:00am

Added Outcome of Oread Design Guidelines at Historic Resources Commission

7/20/16 @ 11:30am

Added Item 3 - Rezoning 1300 Research Park Dr

7/19/16 @ 4:30pm

The following will be added when available:

Item 3 - Rezoning 1300 Research Park Dr

Memo Regarding Outcome of Oread Design Guidelines at Historic Resources Commission

LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION

CITY HALL, 6 EAST 6TH STREET, CITY COMMISSION MEETING ROOM

AGENDA FOR PUBLIC & NON-PUBLIC HEARING ITEMS

JULY 25 ~~& 27~~, 2016 6:30PM - 10:30PM

GENERAL BUSINESS:

PLANNING COMMISSION MINUTES

Receive and amend or approve the minutes from the Planning Commission meeting of June 20, 2016.

COMMITTEE REPORTS

Receive reports from any committees that met over the past month.

COMMUNICATIONS

- a) Receive written communications from the public.
- b) Receive written communications from staff, Planning Commissioners, or other commissioners.
- c) Receive written action of any waiver requests/determinations made by the City Engineer.
- d) Disclosure of ex parte communications.
- e) Declaration of abstentions from specific agenda items by commissioners.

AGENDA ITEMS MAY BE TAKEN OUT OF ORDER AT THE COMMISSION'S DISCRETION

REGULAR AGENDA (JULY 25, 2016) MEETING

PUBLIC HEARING ITEMS:

ITEM NO. 1 CAPITAL IMPROVEMENT PLAN

Review projects proposed for inclusion in the 2017 – 2021 Capital Improvement Plan.

ITEM NO. 2 GPI TO IG; 7.7 ACRES; 711 E 23RD ST (MKM)

USD 497 Vehicle Storage: **Z-16-00147**: Consider a request to rezone approximately 7.7 acres located at 711 E 23rd St from GPI (General Public and Institutional Use) District to IG (General Industrial) District. Submitted by Lawrence Public Schools USD #497 and BG Consultants, Inc. on behalf of Douglas County, property owner of record. *Deferred by Planning Commission on 6/20/16.*

ITEM NO. 3 IBP TO IL; 2.11 ACRES; 1300 RESEARCH PARK DR (BJP)

Z-16-00215: Consider a request to rezone approximately 2.11 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, located at 1300 Research Park Drive, Lot 3, Block 3. Submitted by Wallace Engineering, for Lydia L. Neu and Robert M. Neu, property owners of record.

ITEM NO. 4 SPECIAL USE PERMIT; SEEDS FROM ITALY; 1501 LEARNARD AVE (MKM)

SUP-16-00217: Consider a Special Use Permit for Seeds From Italy, located at 1501 Learnard Avenue. The applicant proposes to renovate an existing building on the property for office and warehouse use supporting their mail-order garden seed business. Submitted by Lynn Byczynski, for Sunrise Green LLC, property owner of record.

ITEM NO. 5A RM12D TO RS5; 13.268 ACRES; 751 W 29TH TERR (SLD)

Z-16-00219: Consider a request to rezone approximately 13.268 acres from RM12D (Multi-Dwelling Residential) District to RS5 (Single-Dwelling Residential) District, located at 751 W 29th Terrace. Submitted by Landplan Engineering, for Savannah Holdings, LC, property owner of record.

ITEM NO. 5B RM12D-FP TO RS5-FP; 1.262 ACRES; 751 W 29TH TERR (SLD)

Z-16-00220: Consider a request to rezone approximately 1.262 acres from RM12D-FP (Multi-Dwelling Residential-Floodplain Overlay) District to RS5-FP (Single-Dwelling Residential-Floodplain Overlay) District, located at 751 W 29th Terrace. Submitted by Landplan Engineering, for Savannah Holdings, LC, property owner of record.

NON-PUBLIC HEARING ITEMS:

ITEM NO. 5C PRELIMINARY PLAT FOR NAISMITH CREEK ADDITION; 751 W 29TH TERR (SLD)

PP-16-00221: Consider a Preliminary Plat for Naismith Creek Addition, for 66 single-dwelling residential lots, located at 751 W 29th Terrace. The subdivision will take access from Alabama Street and W 29th Terrace east of Belle Haven Drive. Submitted by Landplan Engineering, for Savannah Holdings, LC, property owner of record.

ITEM NO. 6 FINAL DEVELOPMENT PLAN FOR PETSMART, BAUER FARM; 4820 BAUER FARM DR (SLD)

Bauer Farm: **FDP-16-00216**: Consider a Final Development Plan for PetSmart, located at 4820 Bauer Farm Dr. The plan proposes a new 18,000 sq. ft. building west of Sprout's. Submitted by Treanor Architects, for Wakarusa Investors, LLC, property owner of record.

RESUME PUBLIC HEARING:

ITEM NO. 7 TEXT AMENDMENT TO DEVELOPMENT CODE; OREAD DESIGN GUIDELINES

Oread Design Guidelines: Consider a Text Amendment, TA-12-00171, to the City of Lawrence Land Development Code, Chapter 20, Articles 3, 4, 5, 6 and 9 regarding the adoption of the Oread Design Guidelines. *Initiated by City Commission on 8/28/12.* Adopt on first reading, Ordinance No. 9211, for a Text Amendment (TA-12-00171) to the City of Lawrence Land Development Code, Chapter 20, Articles 3, 4, 5, 6 and 9 regarding the adoption of the Oread Design Guidelines. (PC Item 2; approved 8-0 on 3/21/16)

ITEMS NO. 8A-8F RECOMMEND THE ESTABLISHMENT & ADOPTION FOR AN URBAN CONSERVATION OVERLAY DISTRICT (-UC) FOR 190.8 ACRES WITHIN THE OREAD NEIGHBORHOOD BASED UPON ADOPTION OF THE *OREAD NEIGHBORHOOD DESIGN GUIDELINES. Districts 1-6 AS IDENTIFIED IN THE INTERACTIVE MAP:* http://lawrenceks.org/pds/draft_plans

Consider Rezoning, Z-12-00172, Oread Design Guidelines District 1 (Low Density), 38.1 Acres, from RM12 (Multi-Dwelling Residential) District, RM12D (Multi-Dwelling Residential) District, RM32 (Multi-Dwelling Residential) District, U-KU (University) District to RM12-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RM12D-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, U-KU-UC (University – Urban Conservation Overlay) District. Adopt on first reading, Ordinance No. 9212, to rezone (Z-12-00172) Oread Design Guidelines District 1 (Low Density), 38.1 Acres, from RM12 (Multi-Dwelling Residential) District, RM12D (Multi-Dwelling Residential) District, RM32 (Multi-Dwelling Residential) District, U-KU (University) District to RM12-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RM12D-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, U-KU-UC (University – Urban Conservation Overlay) District. (PC Item 3A; approved 8-0 on 3/21/16)

Consider Rezoning, Z-12-00175, Oread Design Guidelines District 2 (High Density), 43.7 Acres, from MU (Mixed Use) District, MU-PD (Mixed Use – Planned Development Overlay) District, PCD (Planned Commercial) District, RM32 (Multi-Dwelling Residential) District, RM32-PD (Multi-Dwelling Residential – Planned Development Overlay) District, RMG (Multi-Dwelling Residential – Greek Housing) District, RMO (Multi-Dwelling Residential – Office) District, U-KU (University) District to MU-UC (Mixed Use – Urban Conservation Overlay) District, MU-PD-UC (Mixed Use - Planned Development Overlay - Urban Conservation Overlay) District, PCD-UC (Planned Commercial – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RM32-PD-UC (Multi-Dwelling Residential – Planned Development Overlay – Urban Conservation Overlay) District, RMG-UC (Multi-Dwelling Residential – Greek Housing - Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office District - Urban Conservation Overlay) District, U-KU-UC (University - Urban Conservation Overlay) District. Adopt on first reading, Ordinance No. 9213, to rezone (Z-12-00175) Oread Design Guidelines District 2 (High Density), 43.7 Acres, from MU (Mixed Use) District, MU-PD (Mixed Use – Planned Development Overlay) District, PCD (Planned Commercial) District, RM32 (Multi-Dwelling Residential) District, RM32-PD (Multi-Dwelling Residential – Planned Development Overlay) District, RMG (Multi-Dwelling Residential – Greek Housing) District, RMO (Multi-Dwelling Residential – Office) District, U-KU (University) District to MU-UC (Mixed Use – Urban Conservation Overlay) District, MU-PD-UC (Mixed Use - Planned Development Overlay - Urban Conservation Overlay) District, PCD-UC (Planned Commercial – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RM32-PD-UC (Multi-Dwelling Residential – Planned Development Overlay – Urban Conservation Overlay) District, RMG-UC (Multi-Dwelling Residential – Greek Housing - Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office District - Urban Conservation Overlay) District, U-KU-UC (University - Urban Conservation Overlay) District. (Z-12-00172) (PC Item 3B; approved 8-0 on 3/21/16)

Consider Rezoning, Z-12-00177, Oread Design Guidelines District 3 (Medium Density), 63.5 Acres, from CS (Commercial Strip) District, RM32 (Multi-Dwelling Residential) District, RMO (Multi-Dwelling Residential – Office) District to CS-UC (Commercial Strip - Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential - Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office - Urban Conservation Overlay) District. Adopt on first reading, Ordinance No. 9214, to rezone (Z-12-00177) Oread Design Guidelines District 3 (Medium Density), 63.5 Acres, from CS (Commercial Strip) District, RM32 (Multi-Dwelling Residential) District, RMO (Multi-Dwelling Residential – Office) District to CS-UC (Commercial Strip - Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential - Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office - Urban Conservation Overlay) District. (PC Item 3C; approved 8-0 on 3/21/16)

Consider Rezoning, Z-12-00173, Oread Design Guidelines District 4 (Hancock Historic District), 4.8 Acres, from RM32 (Multi-Dwelling Residential – Urban Conservation Overlay) District to RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District. Adopt on first reading, Ordinance No. 9215, to rezone (Z-12-00173) Oread Design Guidelines District 4 (Hancock Historic District), 4.8 Acres, from RM32 (Multi-Dwelling Residential – Urban Conservation Overlay) District to RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District. (PC Item 3D; approved 8-0 on 3/21/16)

Consider Rezoning, Z-12-00174, Oread Design Guidelines District 5 (Oread Historic District), 28.9 Acres, from CS (Commercial Strip) District, RM32 (Multi-Dwelling Residential) District, RMO (Multi-Dwelling Residential – Office) District, RSO (Single-Dwelling Residential – Office) District to CS-UC (Commercial Strip – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office – Urban Conservation Overlay) District, RSO-UC (Single-Dwelling Residential – Office – Urban Conservation Overlay) District. Adopt on first reading, Ordinance No. 9216, to rezone (Z-12-00174) Oread Design Guidelines District 5 (Oread Historic District), 28.9 Acres, from CS (Commercial Strip) District, RM32 (Multi-Dwelling Residential) District, RMO (Multi-Dwelling Residential – Office) District, RSO (Single-Dwelling Residential – Office) District to CS-UC (Commercial Strip – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office – Urban Conservation Overlay) District, RSO-UC (Single-Dwelling Residential – Office – Urban Conservation Overlay) District. (PC Item 3E; approved 8-0 on 3/21/16)

Consider Rezoning, Z-16-00058, Oread Design Guidelines District 6 (Commercial), 11.9 Acres, from CN2 (Neighborhood Commercial) District, CS (Commercial Strip) District, RM32 (Multi-Dwelling Residential) District, RMO (Multi-Dwelling Residential – Office) District to CN2-UC (Neighborhood Commercial – Urban Conservation Overlay) District, CS-UC (Commercial Strip – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office – Urban Conservation Overlay) District. Adopt on first reading, Ordinance No. 9217, to rezone (Z-16-00058) Oread Design Guidelines District 6 (Commercial), 11.9 Acres, from CN2 (Neighborhood Commercial) District, CS (Commercial Strip) District, RM32 (Multi-Dwelling Residential) District, RMO (Multi-Dwelling Residential – Office) District to CN2-UC (Neighborhood Commercial – Urban Conservation Overlay) District, CS-UC (Commercial Strip – Urban Conservation Overlay) District, RM32-UC (Multi-Dwelling Residential – Urban Conservation Overlay) District, RMO-UC (Multi-Dwelling Residential – Office – Urban Conservation Overlay) District. (PC Item 3F; approved 8-0 on 3/21/16)

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

ADJOURN

CALENDAR

June 2016						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30		

July 2016						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
					1	2
3	4	5	6	7	8	9
10	11	12	13	14	15	16
17	18	19	20	21	22	23
24	25	26	27	28	29	30
31						

August 2016						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
	1	2	3	4	5	6
7	8	9	10	11	12	13
14	15	16	17	18	19	20
21	22	23	24	25	26	27
28	29	30	31			

PCCM Meeting:

(Generally 2nd Wednesday of each month, 7:30am-9:00am)

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<http://www.lawrenceks.org/subscriptions>

2016
LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION
MID-MONTH & REGULAR MEETING DATES

Mid-Month Meetings, Wednesdays 7:30 – 9:00 AM <i>** alternate day/time</i>	Mid-Month Topics	Planning Commission Meetings 6:30 PM, Mon & Wed	
Jan 13	Article 9 text amendments - Parking	Jan 25	Jan 27
<i>Feb 18 ** Thursday 6:00 PM meeting</i>	<i>Joint meeting with HRC – Oread Design Guidelines</i>	Feb 22	Feb 24
<i>Mar 9 ** Wednesday 5:30 PM meeting</i>	<i>Joint meeting with Sustainability Advisory Board [Meeting Room C – Lawrence Public Library – 707 Vermont Street]</i>	Mar 21	Mar 23
Apr 13	Retail Market Study	Apr 25	Apr 27
May 11	APA Conference recap & Nonconformities 101	May 23	May 25
Jun 8	<i>Cancelled</i>	Jun 20	Jun 22
Jul 13	Future Growth Factors	Jul 25	Jul 27
Aug 10	<i>TBD</i>	Aug 22	Aug 24
Sep 14	<i>TBD</i>	Sep 26	Sep 28
Oct 12	<i>TBD</i>	Oct 24	Oct 26
Nov 2	<i>TBD</i>	Nov 14	Nov 16
Nov 30	<i>TBD</i>	Dec 12	Dec 14
	Suggested topics for future meetings: <i>How City/County Depts interact on planning issues</i> <i>Stormwater Stds Update – Stream Setbacks</i> <i>Overview of different Advisory Groups – potential overlap on planning issues</i> <i>Joint meeting with other Cities’ Planning Commissions</i> <i>Joint meeting with other Cities and Townships – UGA potential revisions</i> <i>New County Zoning Codes</i> <i>Tour City/County Facilities</i> <i>Water Resources</i>	<i>Communication Towers – Stealth Design, # of co-locations, notice area</i> <i>WiFi Connectivity & Infrastructure Planning</i> <i>Oread Overlay Districts & Design Guidelines</i> <i>Comprehensive Plan – Goals & Policies</i> <i>Affordable Housing</i> <i>Retail Market Impacts</i> <i>Case Studies</i>	
Meeting Locations	The Planning Commission meetings are held in the City Commission meeting room on the 1 st floor of City Hall, 6 th & Massachusetts Streets, unless otherwise noticed.		
Planning & Development Services Lawrence-Douglas County Planning Division 785-832-3150 www.lawrenceks.org/pds			

2016 PLANNING COMMISSION ATTENDANCE

	Jan 25 2016	Feb 22 2016	Mar 21 2016	April 25 2016	May 23 2016	June 20 2016	July 25 2016	Aug 22 2016				
Britton	Yes	Yes	Yes	Yes	Yes	Yes						
Butler	Yes	Yes	Yes	Yes		Yes						
Carpenter	Yes	Yes	Yes	Yes	Yes	Yes						
Culver	No	Yes	Yes	Yes	Yes	Yes						
Denney	Yes	Yes										
Harrod						Yes						
Kelly	Yes	Yes	Yes	Yes	Yes	Yes						
Liese	Yes	Yes	No	Yes	Yes							
Sands	Yes	Yes	Yes	Yes	Yes	No						
Struckhoff	Yes	Yes	Yes	Yes	Yes	Yes						
von Achen	Yes	Yes	Yes	Yes	Yes	Yes						
Wiley						Yes						

2016 MID-MONTH ATTENDANCE

	Jan 13 2016	Feb 18 2016	Mar 9 2016	April 13 2016	May 11 2016	June 8 2016	July 13 2016	Aug 10 2016	Sept 14 2016	Oct 12 2016	Nov 2 2016	Nov 30 2016
Britton	Yes	Yes	Yes	Yes	Yes	-	No					
Butler	No	No	Yes	No	No	-	No					
Carpenter	Yes	Yes	Yes	Yes	Yes	-	No					
Culver	Yes	Yes	Yes	Yes	Yes	-	No					
Denney	Yes	Yes										
Harrod						-	No					
Kelly	Yes	Yes	Yes	Yes	No	-	Yes					
Liese	No	No	No	No	Yes							
Sands	No	Yes	No	No	Yes	-	Yes					
Struckhoff	Yes	No	No	Yes	Yes	-	Yes					
von Achen	Yes	Yes	Yes	Yes	Yes	-	Yes					
Wiley						-	Yes					



PLANNING COMMISSION MEETING

June 20, 2016

Meeting Action Summary

June 20, 2016 – 6:30 p.m.

Commissioners present: Britton, Butler, Carpenter, Culver, Harrod, Kelly, Struckhoff, von Achen, Willey

Staff present: McCullough, Stogsdill, Crick, Day, Ewert, Larkin, M. Miller

GENERAL BUSINESS

Introduction of new Planning Commissioners Lynn Harrod & Karen Willey.

PLANNING COMMISSION ACTION SUMMARY MINUTES

Receive and amend or approve the action summary minutes from the Planning Commission meeting of May 23, 2016.

Motioned by Commissioner Kelly, seconded by Commissioner Carpenter, to approve the May 23, 2016 Planning Commission minutes.

Unanimously approved 6-0-3, with Commissioners Butler, Harrod, and Willey abstaining.

COMMITTEE REPORTS

Receive reports from any committees that met over the past month.

Commissioner Kelly said the Horizon 2020 Steering Committee met earlier in the week and received early drafts of the format of what the report would look like. He said they looked at the policy around growth and had good discussion about it.

EX PARTE / ABSTENTIONS / DEFERRAL REQUEST

- Receive written communications from staff, Planning Commissioners, or other commissioners.
- Ex parte:
Commissioner von Achen said she received a call from Mr. Scott Zaremba regarding the access on/off 23rd Street for item 3.
- Abstentions:
Commissioner Kelly said he would abstain from item 3 since the applicant, Lawrence Public Schools, was his employer.
Commissioner Harrod said he would abstain from item 3 for the same reason.

ELECTION OF OFFICERS FOR 2016-2017

Complete audio & video from this meeting can be found online:
<http://www.lawrenceks.org/boards/planning-commission/agendas>

Accept nominations for and elect Chair and Vice-Chair for the coming year:

Motioned by Commissioner Britton, seconded by Commissioner Culver, to nominate Commissioner Kelly as Chair.

Motion carried 9-0.

Motioned by Commissioner Culver, seconded by Commissioner Butler, to nominate Commissioner Struckhoff as Vice-Chair.

Motion carried 9-0.

DRAFT

PC Minutes 6/20/16

**ITEM NO. 1 CONDITIONAL USE PERMIT; VINLAND FAIRGROUNDS; 1736 N 700 RD
(BJP)**

Vinland Fairgrounds: **CUP-16-00152**: Consider a Conditional Use Permit for Vinland Fairgrounds, located at 1736 N 700 Rd. Submitted by Vinland Fair Association, property owner of record. *Joint meeting with Baldwin City Planning Commission.*

Item 1 was withdrawn prior to the meeting.

DRAFT

PC Minutes 6/20/16

ITEM NO. 2 RM32 TO GPI; .78 ACRES; 1220 OREAD AVE (JSC)

City Water Tanks: **Z-16-00145**: Consider a request to rezone approximately .78 acres from RM32 (Multi-Dwelling Residential) District to GPI (General Public and Institutional Use) District, located at 1220 Oread Ave. Submitted by the City of Lawrence, property owner of record.

STAFF PRESENTATION

Mr. Jeff Crick presented the item.

APPLICANT PRESENTATION

Mr. Philip Ciesielski, City of Lawrence Utility Department, was present for questioning.

PUBLIC HEARING

No public comment.

COMMISSION DISCUSSION

Commissioner Carpenter asked why the rezoning had not happened before now.

Mr. McCullough said the redevelopment of it triggered the rezoning.

ACTION TAKEN

Motioned by Commissioner Carpenter, seconded by Commissioner Struckhoff, to approve the request to rezone approximately 0.78 acres from RM32 (Multi-Dwelling Residential) District to GPI (General Public and Institutional Use) District based on the findings presented in the staff report and forwarding it to the City Commission with a recommendation for approval.

Approved 9-0

PC Minutes 6/20/16

ITEM NO. 3 GPI TO IG; 7.7 ACRES; 711 E 23RD ST (MKM)

USD 497 Vehicle Storage: **Z-16-00147**: Consider a request to rezone approximately 7.7 acres located at 711 E 23rd St from GPI (General Public and Institutional Use) District to IG (General Industrial) District. Submitted by Lawrence Public Schools USD #497 and BG Consultants, Inc. on behalf of Douglas County, property owner of record.

STAFF PRESENTATION

Ms. Mary Miller presented the item.

APPLICANT PRESENTATION

Mr. Dan Harden, BG Consultants, was present for questioning.

Commissioner Struckhoff asked if there were other fleet storage areas in Lawrence.

Mr. Hardin said not that he was familiar with.

PUBLIC HEARING

Mr. Scott Zaremba expressed concern about egress and ingress. He said the site was not built to move busses back and forth across 23rd Street. He felt it created a conflict.

Ms. Marcia Heeb, 2114 Learnard Ave, expressed concern about the traffic moving through the nearby residential neighborhood. She did not feel the staff report adequately described the homes in the Barker Neighborhood. She felt the character of the neighborhood was not accurately described. She said the property could not be compliant with the Burrough's Corridor ideas due to the impacts on the neighborhood. She expressed concern about the ingress and egress of the site.

COMMISSION DISCUSSION

Commissioner von Achen asked staff to show the route the busses would presumably take.

Ms. Miller said they would have to develop their route. She said they were not allowed to cross 23rd Street. She said the Traffic Impact Study might indicate a particular route.

Commissioner von Achen felt it put Planning Commission in a difficult spot to make a recommendation based on information they did not have.

Mr. McCullough said the Traffic Impact Study would evaluate the routes and intersection.

Mr. Hardin said the Traffic Impact Study was part of the site plan process and he did not want to incur those costs until then. He said City Public Works Department had vehicles larger than school busses.

Commissioner Britton asked staff about the site plan process.

Mr. McCullough said the site plan process would be administrative and notice would be sent to property owners within 200'. He said there was an appeal process and any appeal would go to City Commission. He said the Traffic Impact Study would be looked at within the site planning process.

Complete audio & video from this meeting can be found online:
<http://www.lawrenceks.org/boards/planning-commission/agendas>

He said roads were designed to handle traffic so he would not be surprised if there was a solution. He said the City Traffic Engineer would look at the Traffic Impact Study and the impacts to the intersection. He said he would anticipate public roads would be used to support whatever operation would go on this property.

Commissioner Britton asked if they could require the site plan to come back to Planning Commission.

Mr. McCullough said in very rare occurrences.

Commissioner Carpenter inquired about the property notice being expanded.

Mr. McCullough said an extended courtesy notice could be provided.

Commissioner Kelly asked if further comment could be made about the Traffic Impact Study.

Mr. McCullough said Planning Commission had the ability to seek more information before making a recommendation to City Commission.

Commissioner Britton said the only reason to rezone was because of the fleet storage use. He said the notice range was probably insufficient for this particular piece of property. He suggested extending the property owner notice to 1000' or 500' to get enough of the neighborhoods nearby. He stated in terms of the site plan, he would be comfortable approving the rezoning now but conditioning the site plan approval on a public hearing at Planning Commission. He said if the traffic and impact to the neighborhood was the concern then there should be more of a hurdle to overcome and process to go through than the normal site plan.

Mr. McCullough said City Commission could have the same concerns. He said his preference would be to have more work done on the bus routing that way City Commission could benefit from that information as well. He suggested deferring the item.

Commissioner Culver asked if the information would include a full Traffic Impact Study.

Mr. McCullough said that is what staff would be looking for, as well as routing, and determine whether the streets were acceptable.

Commissioner Culver said if they were going to go through that process he wanted it to be thorough so Planning Commission could make a good decision and not delay it further.

Commissioner Britton asked the applicant for his thoughts on deferral for one month.

Mr. Hardin said he was okay with it.

ACTION TAKEN

Motioned by Commissioner Britton, seconded by Commissioner Willey, to defer one month, directing the applicant to bring information on bus traffic routing and impacts through a Traffic Impact Study to the Planning Commission and extending property owner notice to pick up neighbors located as far north as Learnard Ave and E. 22nd Terr. which will have potential to be affected by the traffic.

Motion carried 7-0-2, with Commissioners Harrod and Kelly abstaining.

Commissioner von Achen said the property to the south was zoned RS10 and was used for stormwater management by the City. She asked if that precluded it from becoming a residential property.

Ms. Miller said it had no frontage on any street so it could become a residential property with a variance but that it would be very rare. She said the property was for filtering water before it reached the wetlands and the intent was to keep it there. She said there was nothing that said it couldn't ever be sold but that it could not be developed separately the way it was now.

DRAFT

PC Minutes 6/20/16

ITEM NO. 4 IG TO CS; 1.56 ACRES; 706 E 23RD ST (MKM)

Microbrewery: **Z-16-00154**: Consider a request to rezone approximately 1.56 acres located at 706 E 23rd St from IG (General Industrial) District to CS (Strip Commercial) District. Submitted by Lockwood Craft Brewing, Co. LLC, for Lawrence Brothers LLC, property owner of record.

STAFF PRESENTATION

Ms. Mary Miller presented the item.

APPLICANT PRESENTATION

Mr. Cory Johnson was present for questioning.

Mr. Dean Grob, Grob Engineering, said he submitted applications today for a Special Use Permit and Preliminary Plat to move forward with the site.

PUBLIC HEARING

No public comment.

COMMISSION DISCUSSION

Commissioner Harrod inquired about the entrance.

Mr. Johnson said the entrance on to 23rd Street was the only entrance on 23rd Street and that the site extended to 22nd Street.

Mr. Grob said there was a gated entrance on the west side to 22nd Street.

ACTION TAKEN

Motioned by Commissioner Britton, seconded by Commissioner Struckhoff, to approve the rezoning request for approximately 1.56 acres from IG (General Industrial) District to (CS) Commercial Strip District and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report

Unanimously approved 9-0.

PC Minutes 6/20/16

**ITEM NO. 5 CONDITIONAL USE PERMIT; CLEAN RUBBLE FILL; 1736 E 1550 RD
(MKM)**

Clean Rubble Fill: **CUP-16-00105**: Consider a Conditional Use Permit for clean rubble landfill, on approximately 40 acres located at 1736 E 1550 Rd. Submitted by Grob Engineering Services, LLC, for Nunemaker-Ross, Inc., property owner of record.

STAFF PRESENTATION

Ms. Mary Miller presented the item.

APPLICANT PRESENTATION

Mr. Dean Grob, Grob Engineering, said the applicant was okay with all conditions except condition 1. He said there were no county roads to use to the site. He said clean rubble fill was not construction debris fill. He said clean rubble fill was well defined by the KDHE. He said the applicant was looking to fill the hole on the site and that this type of facility would not be a huge money maker.

PUBLIC HEARING

Mr. Ted Boyle, North Lawrence Improvement Association, expressed concern about truck traffic going through North Lawrence. He also expressed concern about clean rubble impacts to well water.

COMMISSION DISCUSSION

Commissioner von Achen asked staff to put the proposed truck route on the overhead.

Commissioner Britton asked if equipment would be stored out there.

Mr. Grob said a piece of equipment would be moved in as piles accumulated to push it down.

Commissioner Harrod asked if it was the Nunemaker-Ross family who owned the property to the south.

Mr. Grob nodded yes.

Commissioner Willey asked if the native grass had to be spelled out with the specific type. She didn't want to see it restricted unnecessarily.

Ms. Miller said the language could be changed.

Commissioner Carpenter asked about KDHE requirements for clean rubble.

Mr. Grob said there was not a permitting process. He said it was a highly visible spot so it would be difficult to bring in other material without people seeing it.

Commissioner Britton asked if safety impacts of the larger route were considered.

Ms. Miller said the main thing that staff looked at was the safety of North Lawrence residents.

Mr. McCullough said they could set up an annual joint inspection with KDHE to check the clean rubble fill.

Commissioner Struckhoff felt trucks should go around rather than thru North Lawrence. He supported condition 1.

Commissioner von Achen said all residents of North Lawrence shouldn't bear the burden of truck traffic.

Commissioner Carpetner said the truck routes in North Lawrence should be reevaluated at some point.

Britton inquired about the renewal process for the Conditional Use Permit.

Mr. McCullough said that was addressed by condition 4. He said the approval may be extended by the County Commission if a written request showing good cause was submitted prior to the expiration date.

Commissioner Culver said he was supportive of the staff recommendation.

ACTION TAKEN

Motioned by Commissioner Britton, seconded by Commissioner Struckhoff, to approve the Conditional Use Permit request for a clean rubble landfill at 1736 E 1550 Road subject to the following revised conditions: (new language is shown in **bold**):

1. Truck traffic associated with this use is not permitted on City streets except to access that portion of 9th Street as part of the following approved route: Hwy 40/59 – Hwy 24/40 – E 1600 Rd – N 1650 Rd – N 9th Street
2. Only 'Clean Rubble' shall be used to fill the Borrow Pit. 'Clean Rubble' shall be defined using the Kansas Department of Health and Environment's definition as follows: *"....the following types of construction and demolition waste: concrete and concrete products including reinforcing steel, asphalt pavement, brick, rock and uncontaminated soil as defined in rules and regulations adopted by the secretary."* **The landfill shall be reviewed annually by Planning and KDHE to insure that only clean rubble, as defined by KDHE, is being used for fill.**
3. When the Borrow Pit has been filled as shown on the plan, top soil shall be applied and the property seeded with brome or fescue type grasses, **or with other seed mix approved by the NRCS (Natural Resource Conservation Service)**. Top soil application and seeding shall be completed within 1 year following the completion of filling and grading activities on the site.
4. The Conditional Use Permit shall expire 5 years after the date of the Board of County Commissioners approval. The approval may be extended by the County Commission if a written request showing good cause is submitted prior to the expiration date.
5. Submittal of a revised plan which lists Conditions 1-4 above as the Conditions and Restrictions of Use.

Mr. Grob said the County inspected the feedlot each year and that the applicant had no objection.

Unanimously approved 9-0.

DRAFT

PC Minutes 6/20/16

**ITEM NO. 6 EXTENSION REQUEST FOR PRELIMINARY PLAT FOR GOING SOUTH
ADDITION; 2726 O'CONNELL RD & 2700 BLOCK O'CONNELL RD (SLD)**

Extension request for PP-13-00343, a Preliminary Plat for Going South Addition, located at 2726 O'Connell Rd and 2700 Block O'Connell Rd. Submitted by Grob Engineering Services, for Going South LLC and Heart of America, Teen Challenge, Inc., property owners of record.

STAFF PRESENTATION

Ms. Sandra Day presented the item.

APPLICANT PRESENTATION

Mr. Dean Grob, Grob Engineering, was present for questioning.

PUBLIC COMMENT

No public comment.

COMMISSION DISCUSSION

Commissioner Culver asked if the extension would be for 24 months.

Ms. Day said yes.

ACTION TAKEN

Motioned by Commissioner Struckhoff, seconded by Commissioner Britton, to approve the extension request for PP-13-00343, a Preliminary Plat for Going South Addition, located at 2726 O'Connell Rd and 2700 Block O'Connell Rd.

Unanimously approved 9-0.

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

Mr. McCullough said they were looking for possible Planning Commission training dates.

Commissioner Britton said they were looking at late August or late September. He said he could not do September 17 and 18 but that September 9 or 23 would work for him.

ADJOURN 8:31pm

*Complete audio & video from this meeting can be found online:
<http://www.lawrenceks.org/boards/planning-commission/agendas>*

DRAFT

Planning Commission

Key Links



City of Lawrence
Douglas County
PLANNING & DEVELOPMENT SERVICES



Plans & Documents

- [Horizon 2020](#)
- [Sector/Area Plans](#)
- [Transportation 2040](#)
- [2012 Retail Market Study](#)

Development Regulations

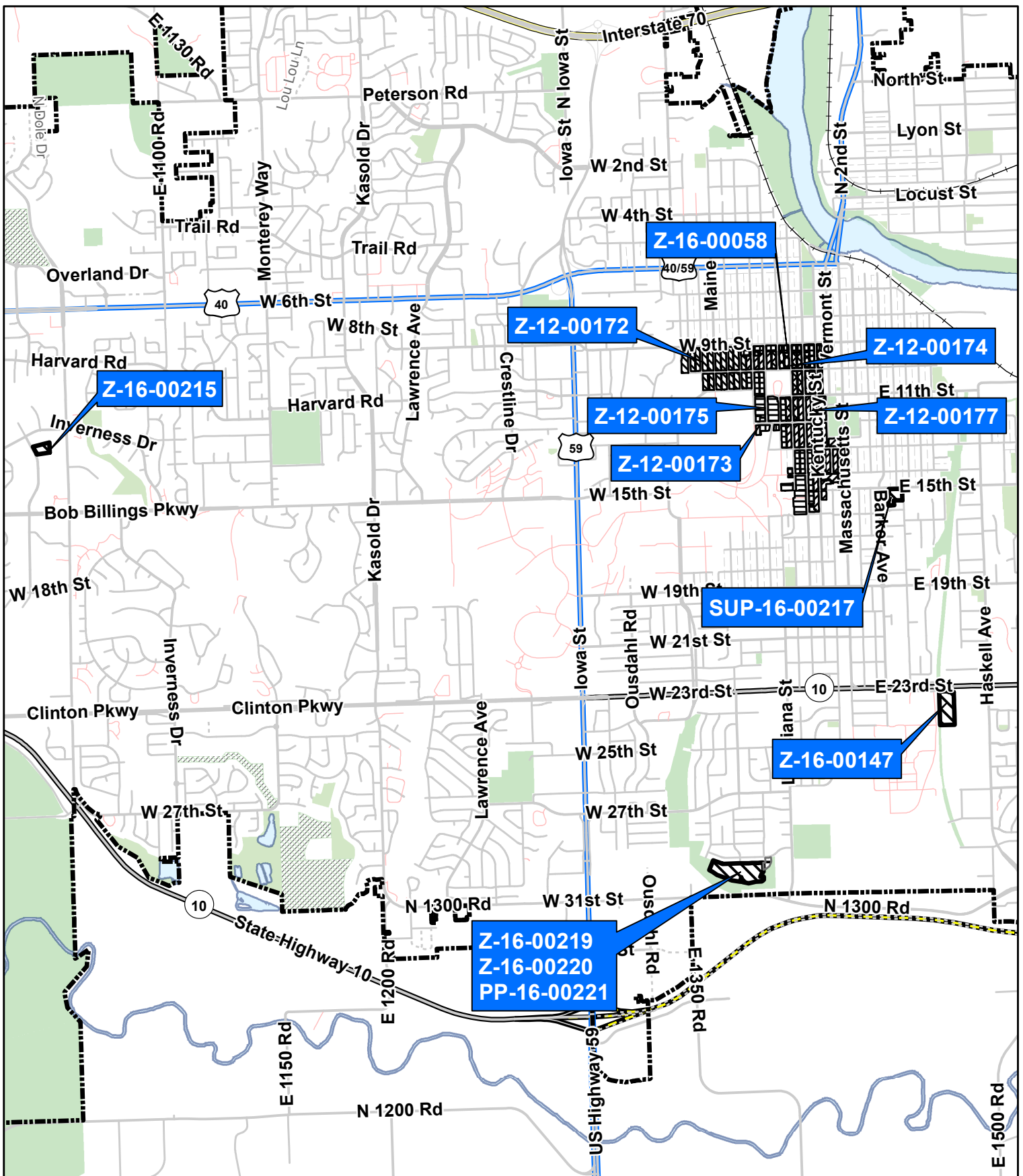
- [Community Design Manual](#)
- [County Zoning Regulations](#)
- [Land Development Code](#)
- [Subdivision Regulations](#)

Online Mapping

- [City of Lawrence Interactive GIS Map](#)
- [Douglas Co. Map Viewer](#)
- [Submittals to the Planning Office](#)

Planning Commission

- [Bylaws](#)
- [Mid-Months & Special Meetings](#)
- [Minutes](#)
- [Planning Commission Schedule/Deadlines](#)



Lawrence-Douglas County Planning Commission July 2016 Public Hearing Agenda Items



PLANNING COMMISSION REPORT
Regular Agenda – Public Hearing Item

ITEM NO. 1: 2017 – 2021 CAPITAL IMPROVEMENT PLAN

Review projects proposed for inclusion in the 2017 - 2021 Capital Improvement Plan (CIP) for the City of Lawrence.

STAFF RECOMMENDATION: Staff recommends the Planning Commission make a finding that the projects presented in the 2017 - 2021 CIP Recommended Projects List are in conformance with the City's comprehensive plan and forward a recommendation to the City Commission for approval.

GENERAL INFORMATION

KSA 12-748 provides the basis for the Capital Improvements Plan (CIP), which is a planning tool that helps guide City investments in constructing public facilities or utilities that are in conformance with the Comprehensive Plan. The statute emphasizes the link between *public improvements and adopted land use plans*. The Planning Commission's role in the CIP process is to review the multi-year plan that includes the attached CIP Recommended Projects List and determine if these proposed projects are consistent with the goals and policies in *Horizon 2020*.

For example, if a street improvement project was proposed that was located miles beyond the adopted Urban Growth Area, the Planning Commission would make a finding that the proposed improvement was not in conformance with the comprehensive plan and provide a report to the governing body identifying how the project did not conform. In staff's opinion, the proposed projects included in the attached CIP Project List do conform to the adopted policies in *Horizon 2020*.

BACKGROUND

To keep pace with the growth of Lawrence and to provide for many of the community's needs identified in the City's Comprehensive Plan requires public investments to be made annually in capital improvements. If needed improvements are not made annually, the condition of the City's infrastructure will deteriorate and eventually will not be able to be ignored. At that point, the cost will be much greater due to the size and scope of the needed improvements as well as the increase in construction costs.

In addition to growth, there are also older sections of the City that were developed without adequate facilities such as storm sewers and paved streets. Capital improvements are needed to address these inadequacies, benefiting not only the neighborhoods where they are located but the entire community.

Because the cost of addressing all of the City's capital needs in one budget year is too great, it is necessary to create a multi-year plan based on priority of need and the anticipated resources available each year. That plan is called the Capital Improvement Plan or CIP.

There are many benefits of having a Capital Improvement Plan. It provides an overall perspective of the development pattern of the city, and thus enables the citizens as well as the

City Commission and City staff to take a long-range view of the their future activities and responsibilities. It calls attention to the deficiencies of the city and stimulates action to correct them. It promotes coordination of projects across city departments and across overlapping governmental jurisdictions. It can also allow city staff to more effectively budget operating expenditures each year necessary to maintain new projects and stabilize personnel demands.

Capital Project Defined

A capital project is defined as a project with a minimum total cost of \$75,000 with a life expectancy of at least five years. These projects may be funded from any source or combination of sources, such as property and sales taxes, rate payer or user fees, general obligation debt or intergovernmental revenues. Project examples include construction or expansion of public buildings, new storm and sanitary sewers, water line upgrades and extensions, the acquisition of land for public use, planning and engineering costs, and street construction.

Capital Improvement Plan Development Process

For this year, capital project request forms were prepared by various City departments, agencies, and the public. Completed forms were submitted to the City Manager's Office for all needed improvements that should be constructed or started during the next five fiscal years. The request forms included a description of the scope and justification for a project as well as a budget for anticipated costs and expected funding sources. The departments also suggested a year for the project based on priorities and needs. If appropriate, Master Plans were used as a basis for recommendations.

The projects were reviewed by the City's management team staff using a priority matrix. The scores were translated into priority rankings and a funding plan was developed for the highest priority projects. This information was used to develop the City Manager's recommended Capital Improvement Plan. The 2017 recommended CIP projects are those that, by virtue of priority and need, are recommended for inclusion in the 2017 budget.

The plan includes a list of the recommended projects by department and by funding sources and indicates expected expenditures in each year of the 5-year plan. In addition to the project list, the CIP includes project summary sheets for each of the recommended projects included in the plan.

The recommended CIP is now submitted to the Planning Commission, for review of the Capital Improvement Plan to ensure all projects included are consistent with the City's Comprehensive Plan, *Horizon 2020*. The Plan is ultimately approved by the City Commission and implemented through the adopted annual budgets.

CIP RECOMMENDED PROJECTS LIST

The CIP list includes projects underway this year or anticipated for construction/implementation in the next five years. The list is presented by departments & programs and also by funding source. There is a significant emphasis on infrastructure reinvestment in the City's water and wastewater systems; street and facility maintenance, and operational equipment and vehicles. In addition to the expected expenditures for facilities & maintenance, infrastructure, and equipment; the plan includes funding for land acquisition and social justice initiatives.

- *Facilities & Maintenance* projects include new buildings; upkeep or upgrades to existing sites; or work group projects proposed to increase efficiencies in the organization.

- *Infrastructure* encompasses projects that are constructing and/or developing public infrastructure (primarily street and stormwater improvements; sanitary sewer and water improvements; and service delivery programs); non-motorized transportation improvements; and public transit facilities.
- *Equipment* includes projects such as vehicle and machinery replacements; transit shelter amenities; and public safety equipment.
- *Land Acquisition* projects identify the need for future parkland and other organizational needs, such as airport improvements.
- *Social Justice Initiatives* include identified funding for affordable housing projects and programs.

As noted above, the Recommended Projects list is followed by a series of project summary sheets which provide a description and justification explaining the details of the project, and the necessary reasons for undertaking the project. Projects are not specifically prioritized in this list, although funding is recommended in specific years within the plan. The City Commission prioritizes projects as part of the annual budget process.

PLANNING COMMISSION'S ROLE

Per KSA 12-748 the Planning Commission is required to review all Capital Improvement Projects included in the CIP to ensure conformance with the City's Comprehensive Plan, *Horizon 2020*. All of the projects listed are supported by goals and policies in *Chapter 10 – Community Facilities*.

GOAL 1: Provide Facilities and Services to Meet the Needs of the Community

Provide quality public and semi-public facilities equitably distributed throughout the community.

Policy 1.1: Maintain Existing Facilities

- a. Encourage the adaptive reuse or redevelopment of excess community facilities and sites.
- b. Maintain or upgrade existing facilities and services where necessary to serve existing development.

Many of the *Infrastructure* projects are specifically transportation projects. A number of the *Equipment and Facilities* projects, such as Emergency Vehicle Traffic Signal Preemption equipment and Mobile Radio upgrades, are related to security, mobility and public transportation. These projects are supported in *Chapter 8 – Transportation*:

Goal 1: Improve Safety & Security

Strategy 1.1.7: Support efforts to provide faster emergency responses through transportation system changes like the installation of signal pre-emption devices for EMS vehicles.

Goal 2: Focus on System Preservation and Economic Efficiency

Strategy 2.1.1: Maintain the existing road and bridge assets by adequately maintaining transportation facilities to preserve their intended function and maintain their useful life.
Strategy 2.2.4: Implement Intelligent Transportation Systems (ITS) and upgrade traffic signal equipment and communications and other technology to improve traffic flow with existing roadway capacity.

Goal 3: Maximize Accessibility and Mobility

Objective 3.1: Minimize delay and congestion to improve travel times through identifying and upgrading traffic signal technology and communications to improve traffic flow.

Objective 3.2: Provide viable transportation alternatives (transit, bicycle, pedestrian) with better interconnectivity for people and goods by considering transit, bikeway and pedestrian facility details in all new development site planning, and adhering to local Complete Streets policies.

Strategy 3.3.4: Enhance and maintain a coordinated transit system including special services for senior citizens and persons with disabilities, and connections to regional commuter services

Goal 4: Consider the Environment and Quality of Life

Strategy 4.1.4: Promote alternative-fueled vehicles that reduce emissions and support the development of needed infrastructure (e.g., charging stations, etc.) that will make the use of those vehicles feasible.

Strategy 4.1.5: Encourage the use of alternative modes of transportation and encourage development that minimizes reliance on the automobile, especially the single occupant car.

Strategy 4.2.1: Improve connectivity between existing employment centers, retail activity areas, and regional destinations as feasible to foster the continued growth and vitality of those areas.

Parks and Land Acquisition projects are supported in both *Chapter 10 – Community Facilities* and *Chapter 9 – Parks Recreation and Open Space*:

GOAL 2: Protect the Existing Parks, Recreation, and Open Space System

Maintain and enhance the existing parks, recreation, and open space system to meet an expressed community need for improvements to this system and to improve the overall community image.

GOAL 3: Expand the Existing Parks, Recreation, and Open Space System

Acquire new parkland and open space areas to stay ahead of growth and to meet anticipated community demand and locate such areas in a manner that is consistent with the coordinated planning and development efforts of the community. Within urban areas, work towards providing public green spaces within ¼ mile of each residence.

GOAL 4: Connectivity To, From, and Between Park, Recreation, and Open Space Areas and Facilities

Provide linkages to the parks, recreation, and open space system that improve community accessibility to such areas and that take into consideration the variety of linkage types available for active and passive recreational needs.

The street and utility improvements for E 19th Street reconstruction, as well as Farmland remediation projects, support the continued development of VenturePark. Airport improvements facilitate new private investment and continue to increase safety. These projects are supported by goals and policies in *Chapter 7 – Industrial and Employment-Related Land Use* and *Chapter 12 - Economic Development*:

GOAL 2: Criteria for Location of New Industrial and Employment-Related Development

Provide industrial and employment-related areas to meet the economic needs of the community.

GOAL 4: Transportation Considerations

Promote a multi-modal transportation system which provides or improves access and circulation within and adjacent to industrial and employment-related areas.

POLICY 6: Business Infrastructure

Priority 6.1: Business Park Development

Douglas County must identify land for future business and corporate business parks that takes advantage of the airport, the interstate/highway system, and the westward growth of Johnson County. Within the next few years, the City and County Commissions shall identify and designate at least 1000 acres of land for industrial expansion over the next 25 years. The commissions shall also put in motion the capitalization and infrastructure development needed to bring those 1000 acres into productive use. This area shall be identified in the HORIZON 2020 strategic plan for future opportunities in Lawrence and Douglas County.

A number of projects related to attraction of visitors to the community through Parks & Recreation sporting and golf tournaments; regional swim meets; National Heritage Area activities and maintenance of civic spaces; and Downtown Lawrence improvements are supported by the goals and policies in *Chapter 12 - Economic Development*:

Policy 5: Visitor Industry Development

Visitors play a vitally important role in economic development. Douglas County is attractive because of its higher-education resources and activities, its arts, its unique heritage, and its authentically historic downtown with locally-owned retail businesses. The extraordinary quality of life that attracts residents and businesses also attracts high-quality visitors.

The projects related to *Affordable Housing Initiatives* are supported by the goals and policies in *Chapter 5 – Residential Land Use*. The chapter is based on the **Neighborhood Concept**:

Neighborhoods are the building blocks of strong communities both physically and socially. Consequently, neighborhoods should be designed to promote social interaction, efficient use of automobiles, and encourage pedestrian and non-motorized activity. To enable this, neighborhoods should provide residents with a functional and aesthetic environment where a sense of identity is created, historic features and cultural traditions are respected, choices are offered, basic services are provided, and connections to a common past maintained.

The concept describes physical elements that support strong neighborhoods, including:

3. Mixed Housing Types

Different types, styles, sizes, densities, and price ranges should be incorporated. Where possible, rehabilitation of existing structures is encouraged. Live/work opportunities should be explored.

Goal 2: Create a Functional and Aesthetic Living Environment

Policy 2.7: Provide for a Variety of Housing Types

- a. Intersperse low- to moderate-income housing throughout the city.
- b. Encourage the use of a variety of housing types.

The Capital Improvement Plan includes a number of projects that will provide increased pedestrian & bicycle infrastructure throughout the city. These projects include the sidewalk gap program, reconstruction of major streets with bike lanes and sidewalks, downtown brick pavers, trail connections and public transit shelters & amenities. These projects are supported by the strategies in *Chapter 8 – Transportation*, as well as, goals and policies throughout the plan.

Chapter 16 – Environment includes a variety of policy statements that emphasize alternative transportation methods, encouraging development patterns that strengthen connectivity and investing in ways to support a sustainable, healthy and active community.

Policy 1.7: Develop stormwater management policies and programs in a manner that ensures water quality and properly controls runoff.

- c. Maintain an inventory of stormwater structures for ongoing inspection, compliance and maintenance procedures. Establish an inspection and maintenance plan with property owners as part of Best Management Practices (BMPs).
- d. Use nonstructural or natural approaches to stormwater system design and management. Encourage stormwater management that uses natural features, rain barrels, rain gardens, bio-retention swales, pervious paving materials, and limits the use of impervious surfaces, etc.

Policy 2.6: Preserve existing open space and create new open space areas to preserve and expand a sustainable green infrastructure system.

- a. To maximize the advantages to the community that the natural and built environments provide, open space preservation shall remain a goal especially as it relates to protecting and preserving natural features discussed in the comprehensive plan. This should be done through:
 - a.1 Maintaining and enhancing existing open space.
 - a.2 Creating new designated open space areas.
 - a.3 Creating a large interconnected network of open space.
- e. The acquisition and continued maintenance of open space that is publicly accessible shall be strongly encouraged.

Policy 3.1: Improve air quality through reduction in emissions from vehicle exhaust by reducing the number of vehicle miles traveled.

- g. Develop a walkable, complete street program stressing connectivity and street design that safely accommodates all users including non-motorized vehicular traffic.

Policy 3.2: Reduce emissions from vehicle exhaust and encourage the use of more energy efficient vehicles.

- c. The City and County should take a leadership role in reducing emissions from the city/county vehicles and public transit vehicles, purchasing more energy efficient vehicles, and reducing the number of miles traveled when possible.

Policy 5.1: Manage solid waste through a program that emphasizes the principles of Reduce, Reuse, and Recycle.

PROFESSIONAL STAFF RECOMMENDATION

Staff recommends the Planning Commission make a finding that the projects presented in the 2017 - 2021 CIP are in conformance with the City's comprehensive plan and forward a recommendation to the City Commission for approval.

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Citizen Input
Contact
Type Unassigned
Useful Life
Category Unassigned
Priority n/a
Status Active

Project # CI05

Project Name Affordable Housing Initiatives

Department Priority
Unfunded

Total Project Cost: \$1,650,000

Description

To Support the acquisition, rehabilitation, and development of affordable housing and supportive services so that all persons in the community have access to independent living with dignity

Justification

Quality housing for all income groups has been identified by the community as one of the most important issues facing the city. (Leisure Vision/ETC Institutes for the City of Lawrence/Douglas 2014)

Expenditures	2017	2018	2019	2020	2021	Total
Other	300,000	300,000	350,000	350,000	350,000	1,650,000
Total	300,000	300,000	350,000	350,000	350,000	1,650,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	300,000	300,000	350,000	350,000	350,000	1,650,000
Unfunded	2,700,000	2,700,000	2,650,000	2,650,000	2,650,000	13,350,000
Total	3,000,000	3,000,000	3,000,000	3,000,000	3,000,000	15,000,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Finance
Contact Finance Director
Type Equipment
Useful Life 5 years
Category Software
Priority 3 Important
Status Active

Project # FA1601CP
Project Name Upgrade Accounting System

Department Priority 3 Important
Unfunded

Total Project Cost: \$134,000

Description
 Upgrade the Accounting and Financial Reporting System. Need to contract with computer consultant. Installation may take up to 6 months, with additional training necessary.

Justification
 Old system does not support additional mandated capabilities for tax and other reporting. IBM AS/400 will now support this system, so makes sense to move it over.

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	10,000					10,000
Equip/Vehicles/Furnishings		124,000				124,000
Total	10,000	124,000				134,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	5,000	62,000				67,000
Utility - Water	5,000	62,000				67,000
Total	10,000	124,000				134,000

Budget Impact/Other
 Additional consulting service costs can be anticipated.

Budget Items	2017	2018	2019	2020	2021	Total
Other (Insurance, Utilities)	500	510				1,010
Total	500	510				1,010

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Fire Medical

Contact Fire Chief

Type Improvement

Useful Life 20 years

Category Buildings

Priority 1 Critical

Status Active

Project # FM1702CIP
Project Name Fire Station No. 1 Rehabilitation / Senior Center

Department Priority 1 Critical

Unfunded

Total Project Cost: \$6,200,000

Description

Design of exterior and interior renovation. Building requires significant renovation to become ADA compliant and provide security to the facility. Includes finishing lower level for Senior Services.

Justification

The Fire Medical Station No. 1, located in downtown Lawrence, was built as the City's main fire station in 1950 and renovations are needed to bring it up to current day operational needs of the City's Fire Medical Department. There are also ADA compliance issues that need to be addressed. The 2007 and 2012 accreditation evaluation recommended replacement or remodel of Station No. 1. Station No. 1's district provides necessary services of our community including North Lawrence-Airport and Central Business District.

This project is cost share with Douglas County at 25% of actual construction cost, estimated to be \$657,500.

Prior	Expenditures	2017	2018	2019	2020	2021	Total
200,000	Construction/Maintenance	6,000,000					6,000,000
Total	Total	6,000,000					6,000,000

Prior	Funding Sources	2017	2018	2019	2020	2021	Total
200,000	General Obligation Debt	5,480,000					5,480,000
	Intergovernmental	520,000					520,000
Total	Total	6,000,000					6,000,000

Budget Impact/Other

An annual debt service payment will be required. There may be a cost share with Douglas County as part of the City/County Ambulance Agreement.

Budget Items	2017	2018	2019	2020	2021	Total
Staff Cost	50,000					50,000
Total	50,000					50,000

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Fire Medical

Contact Fire Chief

Type Improvement

Useful Life 20 years

Category Buildings

Priority 1 Critical

Status Active

Project # FM1703CIP

Project Name Training Burn Tower Replacement

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$700,000

Description

Replacement of the Training Tower at the Training Center constructed in 1993 and has been on the department's CIP request since 2010. There were emergency structural repairs in 2003 that extended its life, however, we are currently experiencing significant maintenance issues and have to limit our live burns and continuously monitor structural stability due to rusting of wall sections.

Justification

The 2007 and 2012 accreditation evaluation recommended replacement or remodel of the Training Tower based on the following: "The Structure is in excess of 17 years old and has outlived its usefulness. This single story training structure does not allow for basement or high rise training exercises. In addition and more importantly, it is a safety hazard. The use of Class A combustibles for training evolutions does not afford the level of safety that newer burn towers provide, which have redundant safety components and sensors built into the system."

It is anticipated with the new construction that LPD and other Douglas County fire departments can utilize the facility.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	350,000	350,000				700,000
Total	350,000	350,000				700,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	350,000	350,000				700,000
Total	350,000	350,000				700,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Fire Medical

Contact Fire Chief

Type Equipment

Useful Life 20 years

Category Software

Priority 1 Critical

Status Active

Project # FM1705CIP

Project Name Emergency Vehicle Traffic Signal Preemption System

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$719,000

Description

Install Opticom 764 phase selectors in all new signal installations to allow for GPS and/or infrared technologies. Install Opticom GPS vehicle transmitters in all fire medical apparatus and traffic division vehicle.

Justification

The current traffic signal preemption system utilizes incompatible technologies. The transmitter is not performing at the intended distance. Improving this technology would reduce congestion and increase safety for Fire Medical responses to incidents.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	719,000					719,000
Total	719,000					719,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	719,000					719,000
Total	719,000					719,000

Budget Impact/Other

The 719,000 estimates includes installation costs. Anticipated that City Traffic Division would manage installation process. The cost does not include in trade-in value of existing equipment.

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Fire Medical
Contact Fire Chief
Type Equipment
Useful Life 20 years
Category Equipment
Priority 3 Important
Status Active

Project # FM1706CIP

Project Name SCBA Air Compressor Fire Station Number 1

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$50,000

Description

Replacement of the breathing air compressor at Fire Station No. 1. This purchase is to refill breathing air as needed without moving vehicles across the City to another station with a Breathing Air Compressor. This will reduce travel time and response time for apparatus by allowing them to stay in their respective districts.

Justification

Station No. 1 is currently using a 1995 breathing air compressor that has reached its life expectancy.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	50,000					50,000
Total	50,000					50,000

Funding Sources	2017	2018	2019	2020	2021	Total
Equipment Reserve Fund	50,000					50,000
Total	50,000					50,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Information Technology

Contact IT Manager

Type Maintenance

Useful Life 7 years

Category Equipment

Priority 1 Critical

Status Active

Project # IT1701CIP
Project Name Access Layer Switches

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$170,000

Description

Several network infrastructure pieces are now reaching the end of their useful life. These are network switches that PCs, Printers, and phones plug in to get network connectivity.

Justification

These critical pieces of our IT infrastructure must remain reliable and perform at the highest levels. The new VoIP phones are doubling our current network load on these devices. Our previous experience shows that after 7 years, the reliability of network equipment decreases significantly. Also, performance becomes an issue as old equipment becomes more obsolete. New security and monitoring features are becoming more critical as new threats emerge every day.

Critical pieces that need replaced include:

2017
15 switches
2018
18 switches
2019
8 switches

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	49,500	61,000	59,500			170,000
Total	49,500	61,000	59,500			170,000

Funding Sources	2017	2018	2019	2020	2021	Total
Equipment Reserve Fund	49,500	61,000	59,500			170,000
Total	49,500	61,000	59,500			170,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Parks and Recreation
Contact PR Director
Type Maintenance
Useful Life 10years
Category Park Improvements
Priority 1 Critical
Status Active

Project # PR1701CIP

Project Name Parks & Recreation Maintenance and Repairs

Department Priority 1 Critical
Unfunded

Total Project Cost: \$700,000

Description

This money is budgeted annually for repairs and improvement to all Parks & Recreation Facility and Properties

Projects range from \$1,000 - \$70,000 and are prioritized weekly as repairs are needed

Justification

The City has invested a great deal of capital funding for Park & Recreation infrastructure over the past 20 years. Failure to fund repairs and maintenance for this infrastructure will cause a rapid decline in facilities.

This fund is the only dedicated funding source for capital improvements for the department

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	700,000					700,000
Total	700,000					700,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	700,000					700,000
Total	700,000					700,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Parks and Recreation

Contact PR Director

Type Improvement

Useful Life 15 years

Category Buildings

Priority 1 Critical

Status Active

Project # PR1702CIP

Project Name Replace HVAC Units- Indoor Pool

Department Priority 1 Critical

Unfunded

Total Project Cost: \$330,000

Description

The Lawrence Indoor Aquatics Center has three large HVAC units that are in poor condition and are in need of replacement. Recommend replacing one in 2016, 2017 and one in 2018

Justification

The Lawrence Indoor Aquatics Center was constructed in 1995. The life expectancy of the HVAC units was estimated to be 15 years. In the past several years, the department has spent a great deal of money to repair these units, and on general maintenance to keep them running properly. The engineering firm of Hoss and Brown performed an analysis of the units, and recommended them for replacement.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	330,000					330,000
Total	330,000					330,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	330,000					330,000
Total	330,000					330,000

Budget Impact/Other

With the replacement of the HVAC units, we expect the total cost of heating and air conditioning to drop.

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Parks and Recreation

Contact PR Director

Type Improvement

Useful Life 30 years

Category Park Improvements

Priority 2 Very Important

Status Active

Project # PR1704CIP
Project Name Install additional restrooms at YSC (ADA comp)

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$120,000

Description

Install additional restrooms at west end of YSC complex (ADA compliance)

Justification

To provide more restrooms at west end of YSC complex. Right now this complex is spreading out and we need more restrooms for this facility. This restroom will be ADA icompliant.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	120,000					120,000
Total	120,000					120,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	120,000					120,000
Total	120,000					120,000

Budget Impact/Other

This will add additional cost of daily clean up and additional cost of utilities.

Budget Items	2017	2018	2019	2020	2021	Total
Maintenance			1,000			1,000
Other (Insurance, Utilities)			2,000			2,000
Total			3,000			3,000

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Parks and Recreation
Contact PR Director
Type Improvement
Useful Life 30 years
Category Park Improvements
Priority 2 Very Important
Status Active

Project #PR1707CIP

Project NameDowntown Brick Pavers

Department Priority 1 Critical
Unfunded

Total Project Cost: \$75,000

Description

Upgrades to brick pavers at mid-blocks and corners on Massachusetts St.

Justification

The brick surfaces at the mid-blocks and corners have been in place since the early 1970s and they are not in good repair. Many of these spots present a tripping hazard to pedestrians.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	75,000					75,000
Total	75,000					75,000

Funding Sources	2017	2018	2019	2020	2021	Total
Guest Tax Fund	75,000					75,000
Total	75,000					75,000

Budget Impact/Other

There should be no budget impact, once installation is complete.

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Parks and Recreation

Contact PR Director

Type Improvement

Useful Life 30 years

Category Park Improvements

Priority 4 Less Important

Status Active

Project # PR1708CIP

Project Name Upgrade EBGC Pro Shop,Concessions,Restroom

Department Priority 3 Important

Unfunded

Total Project Cost: \$500,000

Description

Eagle Bend Golf Course- Add a larger seating area and more restrooms to current facility.

Justification

Currently EBGC is very limited on the size of golf tournaments that we can host due to limited seating and restroom facilities. By expanding facilities, larger events can be booked at EBGC which will have a positive impact on annual revenue.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	500,000					500,000
Total	500,000					500,000

Funding Sources	2017	2018	2019	2020	2021	Total
Golf Course	500,000					500,000
Total	500,000					500,000

Budget Impact/Other

EBGC will be able to take two dollars out of all green fees and money out of tournament fees to cover the cost of the addition.

Budget Items	2017	2018	2019	2020	2021	Total
Maintenance		5,000				5,000
Total		5,000				5,000

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Parks and Recreation
Contact PR Director
Type Improvement
Useful Life 20 years
Category Park Improvements
Priority 2 Very Important
Status Active

Project #PR1713CIP

Project NameInstall Trail to Connect 29th St. to Haskell

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$75,000

Description

Install a concrete trail to connect the existing Haskell Rail Trail to the newly constructed South Lawrence Trafficway Multi-purpose Trail

Justification

This section will add another part to help with the goal of having a loop around the city.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	75,000					75,000
Total	75,000					75,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	75,000					75,000
Total	75,000					75,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Parks and Recreation
Contact PW Director
Type Equipment
Useful Life 15 years
Category Equipment
Priority 1 Critical
Status Active

Project #PR1781CIP

Project NameEquipment Replacement - Recreation Division Bus

Department Priority 1 Critical
Unfunded

Total Project Cost: \$85,000

Description

Replace Unit 582 - This bus is used primarily in Special Populations Programming for transportation of program patrons during in-town and out-of-town trips

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	85,000					85,000
Total	85,000					85,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	85,000					85,000
Total	85,000					85,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Planning & Development

Contact Planning Director

Type Improvement

Useful Life 30 years

Category Buildings

Priority 4 Less Important

Status Active

Project # PS1701CIP

Project Name One Stop Shop

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$2,080,000

Description

Create One Stop Shop to bring all divisions of Planning and Development Services under one roof, as well as representatives from other development-related departments - storm water, utilities, public works, and fire.

Justification

The Matrix Report, a report commissioned in the mid-2000s to review development processes and address shortcomings of the city's development review processes, recommended that the city create a One Stop Shop in order to place all development-related resources at one location so that customers had the most efficient service for their needs. Lawrence is 20 years behind the times on this issue, as many cities have implemented such a concept to ensure not only great customer service, but high coordination between review agencies.

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	30,000					30,000
Land Acquisition		2,000,000				2,000,000
Construction/Maintenance		50,000				50,000
Total	30,000	2,050,000				2,080,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	30,000	2,050,000				2,080,000
Total	30,000	2,050,000				2,080,000

Budget Impact/Other

There will be an impact related to the start up costs of presumed improvements to the space, building acquisition, and furnishings. There may be an impact to staffing costs if it is found that staffs from the departments outside of PDS need to be increased in order to provide resources at the One Stop Shop.

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Project # PD1701CIP
Project Name Mobile Data Computers

Department Priority 1 Critical
Unfunded

Department Police
Contact Police Chief
Type Equipment
Useful Life 12 years
Category Equipment
Priority 1 Critical
Status Active

Total Project Cost: \$672,100

Description

The Police Department and LDCFM are currently using Motorola's MW-810 Mobile Data Computers (MDC). Motorola has abruptly announced they will no longer offer this product, requiring both agencies to identify and implement a new solution for securely transmitting critical, emergency information to units deployed in the field twenty-four hours a day. Across both agencies, approximately 136 units with an extended five-year warranty will need to be purchased. Of the total number of devices, 106 will be installed in vehicles at an approximate cost of \$5350, and 30 units will be portable and cost approximately \$3500 each. The total project cost is estimated at \$672,100 and will be implemented over the two years following approval.

Justification

The LPD and LDCFM utilize Motorola's MW-810 Mobile Data Computer to send and receive critical information from Douglas County Emergency Communication Center and between units while responding to calls for service. This year, Motorola terminated production of these devices forcing emergency responders to identify a new device to fill this need. A more cost effective tablet solution has been identified to replace the 136 MDCs. The total estimated project cost for both departments, including installation, is expected to be at \$672,100.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	672,100					672,100
Total	672,100					672,100

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	672,100					672,100
Total	672,100					672,100

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Police
Contact Police Chief
Type Equipment
Useful Life 10 years
Category Vehicles
Priority 1 Critical
Status Active

Project # PD1702CIP
Project Name Vehicles

Department Priority 1 Critical
Unfunded

Total Project Cost: \$2,039,984

Description

Replace of Department vehicles.

Justification

Over the past several years the Department has received \$260,000 in CIP funds to replace patrol vehicles. The amount was achieved by estimating the cost of a new patrol vehicle to be at \$26,000. It also assumed that just 10 patrol vehicles would be replaced. That amount has not changed for the past several years. In 2016 the cost of new vehicles exceeded the \$26,000 estimate. For 2017 I am requesting the estimated amount be raised to \$28,000 per vehicle. I am also requesting an additional four vehicles to continue the replacement of aging administrative fleet. The amount requested for 2017 is for 14 vehicles at a cost of \$392,000.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	392,000	399,840	407,837	415,994	424,313	2,039,984
Total	392,000	399,840	407,837	415,994	424,313	2,039,984

Funding Sources	2017	2018	2019	2020	2021	Total
General Fund	392,000	399,840	407,837	415,994	424,313	2,039,984
Total	392,000	399,840	407,837	415,994	424,313	2,039,984

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Police

Contact Police Chief

Type Equipment

Useful Life 10 years

Category Equipment

Priority 2 Very Important

Status Active

Project # PD1705CIP

Project Name FARO Scanner

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$95,000

Description

With the technical advances in the visual documentation of crime scenes, the LPD has identified the need to purchase crime scene scanning technology.

Justification

The total price is quoted at \$85,000 if we agree to sign a statement for GSA pricing that will allow the government to use the device for a presidential declared emergency. If we do not agree to the statement, then the prices goes up to \$95,000.

The crime scene scanning technology can allow investigators to:

1. Unbiased, objective capture of evidence present at the scene as well as an overall portrait of the scene itself that could provide beneficial should a case need to be revisited at a later date.
2. Increase the ability to accurately document violent crime scenes while decreasing the impact to the scene.
3. Enable other investigators to virtually revisit a scene and take measurements should new information become available.
4. Eliminates any unintentional oversights that might have occurred using traditional photographic methods that only capture specific photos taken by an investigator.
5. Once a scan is complete and the data imported into crime scene specific software, a completely navigable and interactive model of the scene is available. This is highly advantageous for a jury since it can be provided with multiple perspectives; including points of view from witness perspective or follow a bullet's trajectory.
6. The measurements taken of a scene with scanning technology can also be used with video that may have been obtained in the area of a scene. This is particularly valuable as it can give proper aspect to the recovered video, possibly to the degree accurate measurement can be obtained from these video sources.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	95,000					95,000
Total	95,000					95,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	95,000					95,000
Total	95,000					95,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Police
Contact
Type Equipment
Useful Life 10 years
Category Equipment
Priority 1 Critical
Status Active

Project # PD1707CIP

Project Name Parking Garage Paystations

Department Priority
Unfunded

Total Project Cost: \$99,000

Description

New Pay Stations for New Hampshire & Riverfront Garages; New Printers/Ticket Writers

Justification

New Pay Stations for New Hampshire & Riverfront Garages; New Printers/Ticket Writers

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	99,000					99,000
Total	99,000					99,000

Funding Sources	2017	2018	2019	2020	2021	Total
Public Parking	99,000					99,000
Total	99,000					99,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Police
Contact
Type Improvement
Useful Life 30 years
Category Buildings
Priority n/a
Status Active

Project # PD1708CIP
Project Name Design Police Facility

Department Priority
Unfunded

Total Project Cost: \$1,500,000

Description

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	1,500,000					1,500,000
Total	1,500,000					1,500,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	1,500,000					1,500,000
Total	1,500,000					1,500,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Transit

Contact

Project # TI01

Project Name Multi Modal Facility

Type Improvement

Useful Life 40 + years

Category Buildings

Priority n/a

Status Active

Department Priority

Unfunded

Total Project Cost: \$30,600,000

Description

The facility will house a first floor transit transfer station that will act as the primary hub for the system. Also on the first floor will be amenities for transit users and drivers. Bicycling and pedestrian amenities will also be available on the lower level. The upper level of the facility will be parking for students and the general public.

Justification

Currently our major transfer activities occur in the downtown. With increases in service the operation has had difficulties operating in a safe and efficient manner. Development in the downtown has eliminated the availability of alternative location within the downtown. A more centrally located facility would allow for better use of our resources and would allow for a more efficient use of resources. A site location analysis has been performed and has found that a centrally located facility, on or around the KU campus, would benefit both students and the general public.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	30,600,000					30,600,000
Total	30,600,000					30,600,000

Funding Sources	2017	2018	2019	2020	2021	Total
Intergovernmental	26,600,000					26,600,000
Public Transit Fund	4,000,000					4,000,000
Total	30,600,000					30,600,000

Budget Impact/Other

These funds will be provided through funds previously reserved and will have no impact on the general budget.

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Transit
Contact
Type Unassigned
Useful Life 15 years
Category Unassigned
Priority n/a
Status Active

Project # TI02
Project Name Transit Shelters and Additional Amenities

Department Priority
Unfunded

Total Project Cost: \$750,000

Description
Passenger amenities such as shelters, bike rack, benches, leaning seats, signs and passenger info devices.

Justification
As part of restructuring service as the result of a new multimodal facility an amenity program will be implemented to improve the passenger environment. Sites will be evaluated based on our policy standards and amenities will be installed where deemed appropriate. Creating a better user environment will greatly improve the usability of the service.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	150,000	150,000	150,000	150,000	150,000	750,000
Total	150,000	150,000	150,000	150,000	150,000	750,000

Funding Sources	2017	2018	2019	2020	2021	Total
Public Transit Fund	150,000	150,000	150,000	150,000	150,000	750,000
Total	150,000	150,000	150,000	150,000	150,000	750,000

Budget Impact/Other
These funds will be provided through the Transit Reserve Fund and will have no impact on the general budget.

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Transit
Contact
Type Equipment
Useful Life 12 years
Category Vehicles
Priority n/a
Status Active

Project # TI03
Project Name Fixed Route Transit Buses

Department Priority
Unfunded

Total Project Cost: \$3,500,000

Description
Heavy-duty fixed route transit buses that are used in the delivery of regularly scheduled service. These buses will be fully accessible under ADA and will be equipped with bicycle racks.

Justification
These buses will replace older buses that are expected to have accumulated high mileage. High mileage causes for high expense in upkeep. Higher mileage vehicles are often less dependable and result in numerous service failures. These failures result in passengers not being able to reach their destination to places such as work and doctors appointments.

Expenditures	2017	2018	2019	2020	2021	Total	Future
Equip/Vehicles/Furnishings	1,500,000		500,000	500,000	500,000	3,000,000	500,000
Total	1,500,000		500,000	500,000	500,000	3,000,000	Total

Funding Sources	2017	2018	2019	2020	2021	Total
Public Transit Fund	1,500,000	500,000	500,000	500,000	500,000	3,500,000
Total	1,500,000	500,000	500,000	500,000	500,000	3,500,000

Budget Impact/Other
These funds will be provided through the Transit Reserve Fund and will have no impact on the general budget.

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works

Contact PW Director

Type Improvement

Useful Life 50 years

Category Street Reconstruction

Priority 2 Very Important

Status Active

Project # CI09
Project Name Bike / Ped Improvements / ADA Ramps

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$2,250,000

Description
Bicycle-Pedestrian Task force recommended \$500,000 annual allocation to improving bicycle network to increase safety, promote health and provide alternative mode of transportation.
Projects identified as priorities by the Bicycle-Pedestrian Task Force (see descriptions on following pages).
Shared Use Path, 6th Street from Monterey to Wisconsin: \$135,500 Bike Boulevard, 21st Street: \$120,000 Other Projects based on PBTF Implementation Priorities: \$244,500 Total \$500,000
\$500,000 annual improvements recommended by Pedestrian Bicycle Issues Task Force.
Provide Safe Routes to Schools by filling gaps, repairing and maintaining sidewalks.
Connect residents to neighborhood destinations by filling gaps in the arterial and collector street network.
Invest in facilities that provide safer conditions and access for seniors and people with disabilities.
Projects identified as priorities by the Bicycle-Pedestrian Task Force (see descriptions on following pages).
Safe Routes to Schools Sidewalk Gaps and ADA Ramps: \$300,000 Sidewalk Gap Fill on One Side of Streets: \$50,000 Other Projects based on PBTF Implementation Priorities: \$150,000 Total \$500,000

Justification
To promote community desire for multimodal transportation.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	450,000	450,000	450,000	450,000	450,000	2,250,000
Total	450,000	450,000	450,000	450,000	450,000	2,250,000

Funding Sources	2017	2018	2019	2020	2021	Total
Capital Improvement Reserve	200,000	200,000	200,000	200,000	200,000	1,000,000
Fund - Infrastructure						
General Obligation Debt	250,000	250,000	250,000	250,000	250,000	1,250,000
Unfunded	550,000	550,000	550,000	550,000	550,000	2,750,000
Total	1,000,000	1,000,000	1,000,000	1,000,000	1,000,000	5,000,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact
Type Unassigned
Useful Life
Category Unassigned
Priority n/a
Status Active

Project # PW1701Kcip
Project Name KLINK

Department Priority
Unfunded

Total Project Cost: \$3,000,000

Description

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	600,000	600,000	600,000	600,000	600,000	3,000,000
Total	600,000	600,000	600,000	600,000	600,000	3,000,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	300,000	300,000	300,000	300,000	300,000	1,500,000
Intergovernmental	300,000	300,000	300,000	300,000	300,000	1,500,000
Total	600,000	600,000	600,000	600,000	600,000	3,000,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Equipment
Useful Life 15 years
Category Vehicles
Priority 2 Very Important
Status Active

Project # PW1702CIP

Project Name Annual Vehicle Replacement Program

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$2,500,000

Description

Annual Vehicle Replacement Program

Vehicles to be replaced are recommended by fleet manager

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	500,000	500,000	500,000	500,000	500,000	2,500,000
Total	500,000	500,000	500,000	500,000	500,000	2,500,000

Funding Sources	2017	2018	2019	2020	2021	Total
Capital Improvement Reserve Fund	500,000	500,000	500,000	500,000	500,000	2,500,000
Total	500,000	500,000	500,000	500,000	500,000	2,500,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Improvement
Useful Life 5 years
Category Buildings
Priority 4 Less Important
Status Active

Project # PW17B10CIP

Project Name Parking garage assessments/maintenance

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$150,000

Description

Parking garages need engineering assessments conducted every 5 years. Including some know concrete work and painting that needs completed to ensure and maintain the structural integrity of the parking surfaces. This will only be an assessment and does not consider or account for additional funding for repairs to the decks. This funding also allows for repair of preexisting concerns that are known.

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	150,000					150,000
Total	150,000					150,000

Funding Sources	2017	2018	2019	2020	2021	Total
Public Parking	150,000					150,000
Total	150,000					150,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Improvement
Useful Life 30 years
Category Buildings
Priority 4 Less Important
Status Active

Project # PW17B15CIP

Project Name Street Div Facility

Department Priority 3 Important
Unfunded

Total Project Cost: \$0

Description

Acquire Building @ 31st and Haskell for Street Division

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Other	0					0
Total	0					0

Funding Sources	2017	2018	2019	2020	2021	Total
Unfunded	150,000					150,000
Total	150,000					150,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 30 years
Category Buildings
Priority 4 Less Important
Status Active

Project # PW17B1CIP

Project Name Roof replacement F/M #3

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$140,000

Description

The roof for Fire Medical #3 is past its useful life expectancy, It currently leaks during every rain fall and attempts to repair the leaks are no success due to age.

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	140,000					140,000
Total	140,000					140,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	140,000					140,000
Total	140,000					140,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 30 years
Category Buildings
Priority 4 Less Important
Status Active

Project # PW17B2CIP

Project Name Fire Medical roof and drainage Maintenance

Department Priority 3 Important
Unfunded

Total Project Cost: \$90,000

Description

F/M #3 retaining wall / water diversion = \$10,000.00
F/M #5Drainiage & sidewalk repairs = \$50,000.00
F/M stone barn terrace roof replacement = \$30,000.00

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	90,000					90,000
Total	90,000					90,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	90,000					90,000
Total	90,000					90,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 30 years
Category Buildings
Priority 3 Important
Status Active

Project # PW17B3CIP

Project Name Downtown lighting pole replacement

Department Priority 3 Important
Unfunded

Total Project Cost: \$0

Description

Replacement of downtown street light poles.

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	0	0	0	0	0	0
Total	0	0	0	0	0	0

Funding Sources	2017	2018	2019	2020	2021	Total
Unfunded	200,000	200,000	200,000	200,000	200,000	1,000,000
Total	200,000	200,000	200,000	200,000	200,000	1,000,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 30 years
Category Buildings
Priority 2 Very Important
Status Active

Project # PW17B4CIP
Project Name Downtown Canopy Lighting sidewalk and roofing

Department Priority 3 Important
Unfunded

Total Project Cost: \$350,000

Description
Upgrade of the canopy lighting to LED fix gutters and roofing. Sidewalks and curb and storm sewer needs replaced under the canopy

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	350,000					350,000
Total	350,000					350,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	350,000					350,000
Total	350,000					350,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 20years
Category Buildings
Priority 5 Future Consideration
Status Active

Project # PW17B5CIP
Project Name Community Health Boiler replacement

Department Priority 1 Critical
Unfunded

Total Project Cost: \$225,000

Description
Existing Boilers in the Community Health facility are being taken out of service due to unavailability of replacement parts. This is a critical repair 8 of 10 original are currently operational

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	225,000					225,000
Total	225,000					225,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	112,500					112,500
Intergovernmental	112,500					112,500
Total	225,000					225,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 20years
Category Buildings
Priority 4 Less Important
Status Active

Project # PW17B6CIP

Project Name Community Health Chiller replacement

Department Priority 3 Important
Unfunded

Total Project Cost: \$325,000

Description

Chiller is nearing end of useful life and needs replaced. Also the BAS is no longer supported and controls need replaced

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	325,000					325,000
Total	325,000					325,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	162,500					162,500
Intergovernmental	162,500					162,500
Total	325,000					325,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Maintenance
Useful Life 50 years
Category Buildings
Priority 4 Less Important
Status Active

Project # PW17B7CIP

Project Name Community Health Building Stabilization

Department Priority 1 Critical
Unfunded

Total Project Cost: \$75,000

Description

Community health Building has several exam rooms that the slab is heaving and settling due to below grade settling and movement. Needs mud jacked, will require new flooring and window work. See attached document for affected rooms.

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	75,000					75,000
Total	75,000					75,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	37,500					37,500
Intergovernmental	37,500					37,500
Total	75,000					75,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Improvement
Useful Life 50 years
Category Street Reconstruction
Priority 1 Critical
Status Active

Project # PW17E1CIP
Project Name Kasold Reconstruction - 6th St to Bob Billings Pkwy

Department Priority 1 Critical
Unfunded

Total Project Cost: \$5,450,000

Description
Reconstruction of Kasold from 6th Street to Bob Billings Parkway.
\$500k highway safety funds for roundabout at Kasold & Harvard

Justification
Arterial street needs to be reconstructed. Above annual maintenance expenses. 2012 PCI = 47

Prior	Expenditures	2017	2018	2019	2020	2021	Total
450,000	Construction/Maintenance	5,000,000					5,000,000
Total	Total	5,000,000					5,000,000

Prior	Funding Sources	2017	2018	2019	2020	2021	Total
450,000	Capital Improvement Reserve	4,500,000					4,500,000
Total	Fund - Infrastructure						
	Intergovernmental	500,000					500,000
	Total	5,000,000					5,000,000

Budget Impact/Other
Reconstruction would save funds from street maintenance that are annually incurred for full depth patching.

Budget Items	2017	2018	2019	2020	2021	Total
Supplies/Materials	5,500,000					5,500,000
Total	5,500,000					5,500,000

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works

Contact PW Director

Type Improvement

Useful Life 50 years

Category Street Reconstruction

Priority 1 Critical

Status Active

Project # PW17E2CIP

Project Name Wakarusa Reconstruction - Inverness to 6th Street

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$3,000,000

Description

Reconstruction of Wakarusa from north of Inverness Drive to 6th Street

\$600k highway safety funds from KDOT for roundabout at Wakarusa & Harvard

Justification

Street reconstruction is part of the infrastructure sales tax plan. Arterial street in poor condition with 2015 PCI = 53.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	3,000,000					3,000,000
Total	3,000,000					3,000,000

Funding Sources	2017	2018	2019	2020	2021	Total
Capital Improvement Reserve	2,500,000					2,500,000
Fund - Infrastructure						
Intergovernmental	500,000					500,000
Total	3,000,000					3,000,000

Budget Impact/Other

Reconstruction will save on annual maintenance costs

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works
Contact PW Director
Type Improvement
Useful Life 50 years
Category Street New Construction
Priority 1 Critical
Status Active

Project #	PW17E4CIP
Project Name	Queens Road Improvements - 6th to City Limits

Department Priority 5 Future Consideration
Unfunded

Description	Total Project Cost: \$3,800,000
Queens Road Improvements from 6th Street to north city limits including the signalization of 6th & Queens Road. Improvements include concrete pavement, sidewalk, storm sewer and waterline. *Possible city share for owner occupied east side only; special assessment benefit district will fund remaining cost	

Justification
Additional development in the area justifies the need for city infrastructure to be extended to serve the area. Currently a gravel road that doesn't support increasing traffic.

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	300,000					300,000
Construction/Maintenance		3,500,000				3,500,000
Total	300,000	3,500,000				3,800,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	300,000	3,500,000				3,800,000
Total	300,000	3,500,000				3,800,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Improvement
Useful Life 50 years
Category Street New Construction
Priority 2 Very Important
Status Active

Project # PW17E7CIP

Project Name CDBG Sidewalk Gap Program

Department Priority 3 Important
Unfunded

Total Project Cost: \$100,000

Description

This amount is historical average allocation of CDBG funds for completion of gap sidewalk in low/mod income areas.

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	100,000					100,000
Total	100,000					100,000

Funding Sources	2017	2018	2019	2020	2021	Total
Intergovernmental	100,000					100,000
Total	100,000					100,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Improvement
Useful Life 50 years
Category Street Reconstruction
Priority 4 Less Important
Status Active

Project # PW17E8CIP

Project Name Traffic Calming

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$1,000,000

Description

There are 23 unfunded traffic calming projects (\$950,000 total) that have been approved by the City Commission.

Justification

Improve neighborhoods by decreasing traffic speeds and volume of cut through traffic.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	200,000	200,000	200,000	200,000	200,000	1,000,000
Total	200,000	200,000	200,000	200,000	200,000	1,000,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	200,000	200,000	200,000	200,000	200,000	1,000,000
Total	200,000	200,000	200,000	200,000	200,000	1,000,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Equipment
Useful Life 10years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F10CIP

Project Name 313 Bobcat Replacement

Department Priority 3 Important
Unfunded

Total Project Cost: \$75,000

Description

Skid steer replacement for unit 313. This is a combo purchase, one skidsteer, 4 in 1 bucket, and new mill attachment. Total estimated price. This is not accounting for trade of old unit. Trade may bring the total below CIP threshold.

Justification

Skid steer loaders provide a major component to street maintenance functions. Skid steers are used in curb repair, asphalt maintenance, storm water maintenance, milling operations, and all other material handling situations.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	75,000					75,000
Total	75,000					75,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	75,000					75,000
Total	75,000					75,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Equipment
Useful Life 7 years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F1CIP

Project Name Street Sweeper

Department Priority 3 Important
Unfunded

Total Project Cost: \$280,000

Description

Replacement for unit #364 as part of street maintenance and stormwater infrastructure deterioration prevention.

Justification

Replacement of high cost infrastructure maintenance equipment

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	280,000					280,000
Total	280,000					280,000

Funding Sources	2017	2018	2019	2020	2021	Total
Stormwater Fund	280,000					280,000
Total	280,000					280,000

Budget Impact/Other

Street sweeping reduces the deterioration of the city's stormwater infrastructure and removes debris that can contribute to air quality issues.

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Public Works

Contact PW Director

Type Equipment

Useful Life 10years

Category Vehicles

Priority 3 Important

Status Active

Project # PW17F2CIP
Project Name 366 Class 5 dump truck replacement

Department Priority 3 Important

Unfunded

Total Project Cost: \$105,000

Description

Replacement of a one ton plow, spreader, dump truck to a right sized class 5 truck

Justification

unit 366 is part of the street maintenance program and is instrumental in snow removal operations, transport of equipment, and trailer towing applications.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	105,000					105,000
Total	105,000					105,000

Funding Sources	2017	2018	2019	2020	2021	Total
Stormwater Fund	105,000					105,000
Total	105,000					105,000

Budget Impact/Other

366 is experiencing high repair costs and has trouble with the demands of winter snow operations. Rightsizing equipment will help with vehicle downtime and load demands of operations.

Budget Items	2017	2018	2019	2020	2021	Total
Supplies/Materials	0					0
Total	0					0

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Public Works

Contact PW Director

Type Equipment

Useful Life 7 years

Category Vehicles

Priority 4 Less Important

Status Active

Project # PW17F3CIP

Project Name 331 Tandam axle dump truck replacement

Department Priority 3 Important

Unfunded

Total Project Cost: \$195,000

Description

Replacement of tandem axle dump truck unit 331. Unit 331 includes a spreader and plow as part of the replacement package.

Justification

Dump trucks serve a vital roll in street maintenance. Material hauling to include aggregates, asphalt, salt, etc. Dump trucks are a non-expendable component of snow and ice operations.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	195,000					195,000
Total	195,000					195,000

Funding Sources	2017	2018	2019	2020	2021	Total
Special Gas Tax Fund	195,000					195,000
Total	195,000					195,000

Budget Impact/Other

Replacement cycle of dump trucks can affect operations not only in down time but can cause budgetary concerns as it ages with high repair costs and undependable contributions to operations.

Budget Items	2017	2018	2019	2020	2021	Total
Supplies/Materials	195,000					195,000
Total	195,000					195,000

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Equipment
Useful Life 10years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F4CIP
Project Name 337 Class 5 dump truck replacement

Department Priority 3 Important
Unfunded

Total Project Cost: \$105,000

Description
Replacement of a one ton plow, spreader, dump truck to a right sized class 5 truck

Justification
unit 337 is part of the street maintenance program and is instrumental in snow removal operations, transport of equipment, and trailer towing applications.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	105,000					105,000
Total	105,000					105,000

Funding Sources	2017	2018	2019	2020	2021	Total
Special Gas Tax Fund	105,000					105,000
Total	105,000					105,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Equipment
Useful Life 5 years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F9CIP
Project Name 394 Rubber tire loader replacement

Department Priority 3 Important
Unfunded

Total Project Cost: \$75,000

Description
unit 394 is scheduled for a buyback program in 2017. Streets utilize a rubber tired loader for material handling. This unit is instrumental in snow operations, street project operations, and supporting other divisions.

Justification
Streets has implemented a buyback program to make sure this vital piece of equipment can maximize uptime and utilize budget saving components with full machine warranty.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	75,000					75,000
Total	75,000					75,000

Funding Sources	2017	2018	2019	2020	2021	Total
Stormwater Fund	75,000					75,000
Total	75,000					75,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works

Contact PW Director

Type Maintenance

Useful Life

Category Street Repair

Priority 1 Critical

Status Active

Total Project Cost: \$15,700,000

Project # PW17SM1CIP
Project Name 2017 Contracted Street Maintenance Program

Department Priority 1 Critical

Unfunded

Description
Contracted Street Maintenance Program to include: Microsurfacing/Patching, Milling and Overlay, Concrete Rehabilitation

Justification
See attached documents. To be updated annually with program updates and needs, please see attached & link http://lawrenceks.org/assets/agendas/cc/2016/02-23-16/pw_street_maintenance_update_memo.html . -Please refer to Pavement Management Program & 2006 Contracted street Repair Project memo from 02-27-06 CC agenda. Budget projections estimated \$6 million in maintenance needs per year to sustain pavement conditions. Attached & Link http://lawrenceks.org/assets/agendas/cc/2006/02-07-06/02-07-06h/pw_pavement_mgmt_memo.pdf -Per the 2015 Citizen Survey, the maintenance of streets remains a top priority for improvement. Link https://www.lawrenceks.org/citizen_survey

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	3,140,000	3,140,000	3,140,000	3,140,000	3,140,000	15,700,000
Total	3,140,000	3,140,000	3,140,000	3,140,000	3,140,000	15,700,000

Funding Sources	2017	2018	2019	2020	2021	Total
Capital Improvement Reserve Fund - Infrastructure	800,000	800,000	800,000	800,000	800,000	4,000,000
General Fund	2,000,000	2,000,000	2,000,000	2,000,000	2,000,000	10,000,000
Special Gas Tax Fund	200,000	200,000	200,000	200,000	200,000	1,000,000
Stormwater Fund	140,000	140,000	140,000	140,000	140,000	700,000
Unfunded	2,860,000	2,860,000	2,860,000	5,720,000	2,860,000	17,160,000
Total	6,000,000	6,000,000	6,000,000	8,860,000	6,000,000	32,860,000

Budget Impact/Other
A higher level budget for contracted street maintenance is important to sustain the current street conditions and to minimize the potential of regression in overall City street conditions.

Budget Items	2017	2018	2019	2020	2021	Total
Maintenance	6,000,000					6,000,000
Total	6,000,000					6,000,000

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Project # PW17SM3CIP
Project Name 2017 Brick Street Rehabilitation Program

Department Public Works
Contact PW Director
Type Maintenance
Useful Life
Category Street Repair
Priority 4 Less Important
Status Active

Department Priority 1 Critical
Unfunded

Total Project Cost: \$0

Description

Maintenance/ Rehabilitation of Brick or Brick Composed (w/ Asphalt Overlay) City Streets

Justification

-As of Dec.2015, brick streets avg PCI = 55.0 & brick in composition streets avg PCI = 76.2
 Recommendations:
 -Develop a maintenance policy for city streets paved in brick and with pavement over brick. Draft brick street maintenance restoration guidelines were presented to the City Commission on January 21, 2014. The Historic Resources Commission (HRC) reviewed the draft guidelines on May 15, 2014 and provided suggested revisions. Further review by neighborhood associations, the Lawrence Preservation Alliance and the HRC, along with final review by the City Commission, is anticipated. Attached & Link http://lawrenceks.org/assets/agendas/cc/2014/01-21-14/pw_brick_street_proposed_guideline_document.pdf

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	0	0	0	0	0	0
Total	0	0	0	0	0	0

Funding Sources	2017	2018	2019	2020	2021	Total
Unfunded	500,000	500,000	500,000	500,000	500,000	2,500,000
Total	500,000	500,000	500,000	500,000	500,000	2,500,000

Budget Impact/Other

A higher level budget for brick streets/ brick in composition streets maintenance and rehabilitation is important to sustain the current street conditions and to minimize the potential of regression in overall City brick street/ brick in composite street conditions.

Budget Items	2017	2018	2019	2020	2021	Total
Maintenance	500,000					500,000
Total	500,000					500,000

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Public Works

Contact PW Director

Type Maintenance

Useful Life 10years

Category Street Repair

Priority 4 Less Important

Status Active

Project # PW17SM4CIP

Project Name Contract Milling for In House Pavement Rehab

Department Priority 3 Important

Unfunded

Total Project Cost: \$500,000

Description

Contract milling for in house street maintenance pavement patching and overlay.

Justification

Current capabilities do not allow for street milling when in house asphalt pavement is rehabilitated.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	100,000	100,000	100,000	100,000	100,000	500,000
Total	100,000	100,000	100,000	100,000	100,000	500,000

Funding Sources	2017	2018	2019	2020	2021	Total
Special Gas Tax Fund	100,000	100,000	100,000	100,000	100,000	500,000
Total	100,000	100,000	100,000	100,000	100,000	500,000

Budget Impact/Other

By creating a contract milling portion to in house pavement rehab, a cost savings will be seen as apposed to total pavement removal, or topical overlays that raise the elevation of the street and fill curb and gutter sections creating the need for additional curb replacement.

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Public Works

Contact PW Director

Type Maintenance

Useful Life 7 years

Category Unassigned

Priority 2 Very Important

Status Active

Total Project Cost: \$780,606

Project # PW17SM5CIP

Project Name ITS Video Detection/ upgrade and replacement

Department Priority 2 Very Important

Unfunded

Description

Upgrade current video detection equipment at signalized intersections

Justification

Current equipment is out dated and the manufacture no longer supports equipment that is in the field.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	150,000	153,000	156,060	159,181	162,365	780,606
Total	150,000	153,000	156,060	159,181	162,365	780,606

Funding Sources	2017	2018	2019	2020	2021	Total
Special Gas Tax Fund	150,000	153,000	156,060	159,181	162,365	780,606
Total	150,000	153,000	156,060	159,181	162,365	780,606

Budget Impact/Other

The replacement cost is approximately \$30,000 per intersection. By doing 5 intersection annually it will be possible upgrade all intersection operated by the TOC on a 7 year rotation.

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works
Contact PW Director
Type Equipment
Useful Life 10years
Category Vehicles
Priority 3 Important
Status Active

Project # PW18F11CIP

Project Name 367 Bobcat replacement

Department Priority 3 Important
Unfunded

Total Project Cost: \$75,000

Description

Skid steer replacement for unit 367. This is a combo purchase, one skid steer, 4 in 1 bucket, and new mill attachment. Total estimated price. This is not accounting for trade of old unit. Trade may bring the total below CIP threshold.

Justification

Skid steer loaders provide a major component to street maintenance functions. Skid steers are used in curb repair, asphalt maintenance, storm water maintenance, milling operations, and all other material handling situations.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	75,000					75,000
Total	75,000					75,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt	75,000					75,000
Total	75,000					75,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works - Airport

Contact PW Director

Type Improvement

Useful Life 30 years

Category Buildings

Priority 2 Very Important

Status Active

Project # PW17A1CIP

Project Name Wildlife fence

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$900,000

Description

Safety - Installation of a wildlife fence city/FAA share program. Cities responsibility 90,000. Total estimated project cost is \$900,000

Justification

The airport is partially fenced with a three-strand barbed wire fence and does not have gates at most entrances. A key recommendation from the 2012 Wildlife Hazard Assessment of the airport was the installation of a woven wire wildlife fence on the airport perimeter following the property line. This project will significantly deter wildlife incursions on the field and lower the city's liability risk of wildlife and aircraft collisions. The new fence will include chain-link fencing in the main building area and will include manual and automatic gates for access control.

Prior	Expenditures	2017	2018	2019	2020	2021	Total
95,000	Construction/Maintenance	805,000					805,000
Total	Total	805,000					805,000

Prior	Funding Sources	2017	2018	2019	2020	2021	Total
95,000	Airport	80,500					80,500
Total	Intergovernmental	724,500					724,500
	Total	805,000					805,000

Budget Impact/Other

Project Cost \$900,000
FAA 90% - \$810,000
City 10% - \$90,000

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works - Airport

Contact PW Director

Type Maintenance

Useful Life 30 years

Category Unassigned

Priority 2 Very Important

Status Active

Total Project Cost: \$1,360,000

Project # PW18A2CIP
Project Name Reconstruct Terminal Apron

Department Priority

Unfunded

Description

Mill and overlay of terminal apron

Justification

The main apron at LWC has an old coal tar slurry that is cracked (egg-shelled) and needs to be resurfaced. There are areas on the apron that need to be reconstructed due to heavy business jet traffic along with typical weathering and cracking over time. The rehabilitation will include milling off the existing surface and applying an overlay. Reconstruction will include full-depth asphalt removal, preparation of the underlying base and reconstruction of the asphalt section.

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	136,000					136,000
Construction/Maintenance		1,224,000				1,224,000
Total	136,000	1,224,000				1,360,000

Funding Sources	2017	2018	2019	2020	2021	Total
General Obligation Debt		136,000				136,000
Intergovernmental	136,000	1,088,000				1,224,000
Total	136,000	1,224,000				1,360,000

Budget Impact/Other

Estimated Cost is \$1,360,000
FAA will pay 90% - \$1,224,000
City Share 10% - \$136,000

Will reduce future annual maintenance costs

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works - Solid Waste
Contact PW Director
Type Equipment
Useful Life 7 years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F5CIP
Project Name 426 Automated Side Load Refuse truck

Department Priority 3 Important
Unfunded

Total Project Cost: \$225,500

Description
Replace rear load refuse truck with automated side load refuse truck. This is in alignment with the automation restructuring with waste pickup.

Justification
Replacement of rear load refuse to automation reduces the fleet size with keeping the quality of service to the community. Automation has proven to increase route productivity.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	225,500					225,500
Total	225,500					225,500

Funding Sources	2017	2018	2019	2020	2021	Total
Solid Waste Fund	225,500					225,500
Total	225,500					225,500

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works - Solid Waste
Contact PW Director
Type Equipment
Useful Life 10years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F6CIP

Project Name 427 Dual purpose front load and ASL

Department Priority 3 Important
Unfunded

Total Project Cost: \$275,000

Description

Replace unit 427 with a Curotto-Can and front load truck. This unit allows the function of a front load refuse truck with the ability to attach a front load automated side load function to support a back up for an automated side load refuse body route. This unit is a versital combination to support two rolls in the solid waste division.

Justification

Replacement of unit 427 with a combination unit will help support the comercial waste service and increase the ability to provide a supporting role in residential service.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	275,000					275,000
Total	275,000					275,000

Funding Sources	2017	2018	2019	2020	2021	Total
Solid Waste Fund	275,000					275,000
Total	275,000					275,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works - Solid Waste
Contact PW Director
Type Equipment
Useful Life 10years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F7CIP
Project Name 421 Rear load refuse truck

Department Priority 3 Important
Unfunded

Total Project Cost: \$160,000

Description
Replace unit 421 with rear load refuse body.

Justification
Solid waste division will need to continue to maintain its rear load fleet to provide yard waste and residential services.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	160,000					160,000
Total	160,000					160,000

Funding Sources	2017	2018	2019	2020	2021	Total
Solid Waste Fund	160,000					160,000
Total	160,000					160,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works - Solid Waste
Contact PW Director
Type Equipment
Useful Life 7 years
Category Vehicles
Priority 4 Less Important
Status Active

Project # PW17F8CIP
Project Name 489 Rolloff container truck repalcement

Department Priority 3 Important
Unfunded

Total Project Cost: \$140,000

Description
Replacement of unit 489. This roll off container truck provides delivery and pick up for container refuse material. Unit 489 has exceeded the 200000 mile mark and is qualified for normal replacement scheduling.

Justification
Unit 489 has met criteria for replacement and serves a vital role in container service to the community.

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	140,000					140,000
Total	140,000					140,000

Funding Sources	2017	2018	2019	2020	2021	Total
Solid Waste Fund	140,000					140,000
Total	140,000					140,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

Department Public Works - Solid Waste

City of Lawrence, Kansas

Contact PW Director

Project # PW17SW1

Type Unassigned

Project Name Solid Waste Facility at Kresge

Useful Life

Category Buildings

Priority 1 Critical

Status Active

Department Priority 5 Future Consideration

Unfunded

Total Project Cost: \$2,970,000

Description

Phase 2 of development at the Kresge property.

Property was purchased in 2014 for the development of a consolidated location for Solid Waste operations. The development was planned in two stages. Phase I constructed a new Household Hazardous Waste facility and provided storm detention infrastructure for the entire site. Phase 2 will create work and crew space for the solid waste crews, employee and truck parking.

Justification

Property was purchased in 2014 for the development of a consolidated location for Solid Waste operations. The development was planned in two stages. Phase I constructed a new Household Hazardous Waste facility and provided storm detention infrastructure for the entire site. Phase 2 will create work and crew space for the solid waste crews, employee and truck parking.

Phase 2 will replace the 2400 sf facility at 1140 Haskell that serves as crew base for over 80 employees. The existing structure is in the floodway, does not have adequate restroom or locker room facilities. Meetings with staff are standing room only in 2 to 3 different groups in order to accommodate all employees. There are no locker room facilities or areas for employees to eat lunch or take a break. There is no area to provide training for employees.

Phase 2 will also replace the maintenance shop at the SWAN facility. Baling operations have currently ceased there. The SWAN structure can be sold to assist with financing the Phase 2 construction at Kresge.

Operational efficiencies will occur when staff can operate from single location. Fueling will continue to occur at 11th and Haskell or the West 40. A space for fueling operations was designated on the site plan for a future development phase.

Prior	Expenditures	2017	2018	2019	2020	2021	Total
270,000	Construction/Maintenance	2,700,000					2,700,000
Total	Total	2,700,000					2,700,000

Prior	Funding Sources	2017	2018	2019	2020	2021	Total
270,000	Solid Waste Fund	2,700,000					2,700,000
Total	Total	2,700,000					2,700,000

Budget Impact/Other

Complete funding needs to be identified and a portion may need to be bonded, with repayment from the solid waste fund.

Budget Items	2017	2018	2019	2020	2021	Total
Other (Insurance, Utilities)	30,000					30,000
Staff Cost	0					0
Total	30,000					30,000

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works - Stormwater
Contact PW Director
Type Equipment
Useful Life 10years
Category Storm Sewer/Drainage
Priority 3 Important
Status Active

Project # PW17S1CIP

Project Name Storm Sewer Video Inspection Unit

Department Priority 2 Very Important
Unfunded

Total Project Cost: \$150,000

Description

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Equip/Vehicles/Furnishings	150,000					150,000
Total	150,000					150,000

Funding Sources	2017	2018	2019	2020	2021	Total
Stormwater Fund	150,000					150,000
Total	150,000					150,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Public Works - Stormwater
Contact PW Director
Type Improvement
Useful Life 50 years
Category Storm Sewer/Drainage
Priority 4 Less Important
Status Active

Project # PW17S2CIP

Project Name 13th Brook Drainage Improvement Improvements

Department Priority 3 Important
Unfunded

Total Project Cost: \$250,000

Description

Install open span structure to improve overall drainage. This project would be designed and constructed by in house staff.

Justification

Work to improve drainage in drainage basin, eliminate street and property flooding

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	250,000					250,000
Total	250,000					250,000

Funding Sources	2017	2018	2019	2020	2021	Total
Stormwater Fund	250,000					250,000
Total	250,000					250,000

Budget Impact/Other

Will decrease annual maintenance cost caused by several high water events each year.

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Public Works - Stormwater

Contact

Project # PW17S3CIP

Project Name Storm Water Culvert Lining

Type Maintenance

Useful Life 30 years

Category Unassigned

Priority 3 Important

Status Active

Department Priority 2 Very Important

Unfunded

Total Project Cost: \$1,250,000

Description

Rehabilitate failing storm water culverts by rehabilitation without doing dig and replace. Projects would be completed by in house forces and contract maintenance projects

Justification

With over 150 miles of underground storm water drainage pipes and culverts with many miles under arterials and collector routes this provides an option to be proactive in the maintenance of the system and provide for a method to rehabilitate without long term street closures and traffic disruptions. Currently we are only reactive after a failure.

Expenditures	2017	2018	2019	2020	2021	Total
Construction/Maintenance	250,000	250,000	250,000	250,000	250,000	1,250,000
Total	250,000	250,000	250,000	250,000	250,000	1,250,000

Funding Sources	2017	2018	2019	2020	2021	Total
Stormwater Fund	250,000	250,000	250,000	250,000	250,000	1,250,000
Total	250,000	250,000	250,000	250,000	250,000	1,250,000

Budget Impact/Other

In place rehabilitation and repair can provide a savings of 20% over the cost of dig and replace. By being proactive we can eliminate we can reduce the need for more costly replacement.

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Utilities
Contact UT Director
Type Improvement
Useful Life 100 years
Category Wastewater
Priority 1 Critical
Status Active

Project # UT1304CIP
Project Name Wakarusa WWTP and Conveyance Corridor

Department Priority
Unfunded

Total Project Cost: \$73,916,272

Description

Project UT1304 includes the Wakarusa WWTP, Pump Station 10 in the area of 31st and Louisiana, and required force mains and gravity sewers to convey sewerage to and from this point to the Wakarusa WWTP. The project also includes a lab expansion and new final clarifier equipment at the existing Kansas River WWTP.

The Wakarusa WWTP has been designed as a 2.5 MGD biological nutrient removal (BNR) treatment facility. Pump Station 10 will have increased operational flexibility and will be able to pump a range of flows from 1.0 MGD to 12.5 MGD to the Wakarusa WWTP through the 16" and 24" force mains installed between the two facilities.

Justification

The Wakarusa Wastewater Treatment Plant (WWTP) and Conveyance Corridor facilities will:

1. Address current wet weather overloading of major sanitary sewer lines along the 31st Street corridor that serve much of the Wakarusa drainage system west of Iowa and north to 6th Street as shown on the attached service area map.
2. Help reduce and manage wet weather flows at the Kansas River WWTP. Peak flows exceed the 65 million gallon per day (MGD) peak treatment capacity by 20 MGD.
3. Provide 20 percent additional treatment capacity for growth of the community. The current treatment plant is at or near its' rated capacity for treatment of organics.
4. Take the first step as adopted in the City's Integrated Plan, and as required by the latest NPDES permits, to meet new environmental regulations that require reduction of nutrients from treated effluents, specifically nitrogen and phosphorous. The current treatment processes do not reduce nutrients.

Prior	Expenditures	2017	2018	2019	2020	2021	Total
44,616,272	Construction/Maintenance	29,000,000					29,000,000
	Equip/Vehicles/Furnishings	300,000					300,000
Total	Total	29,300,000					29,300,000

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Wastewater	29,300,000					29,300,000
Total	29,300,000					29,300,000

Budget Impact/Other

City Manager's Recommended Projects

City of Lawrence, Kansas

Data in Year 2017

Department Utilities
Contact UT Director
Type Maintenance
Useful Life 100 years
Category Water
Priority 1 Critical
Status Active

Project # UT1307CIP

Project Name Oread Storage Tank & Booster Pump Stn Replacement

Department Priority
Unfunded

Total Project Cost: \$5,081,220

Description

Replacement of the south 1931, and north 1954 Oread water storage tanks and booster pump station.

Justification

Project was identified in the 2012 Integrated Water Utility Plan and Capital Improvements Plan. Both tanks are exhibiting severe corrosion, loss of structural members, holes in the exteriors, deteriorated foundations and failing interior and exterior coatings. In addition the tanks do not meet several current safety and American Water Works Association standards. The pump station is in need of replacement due to electrical equipment, valving, maintenance and operational issues.

Prior	Expenditures	2017	2018	2019	2020	2021	Total
851,220	Construction/Maintenance	4,230,000					4,230,000
Total	Total	4,230,000					4,230,000

Prior	Funding Sources	2017	2018	2019	2020	2021	Total
851,220	Utility - Water	4,230,000					4,230,000
Total	Total	4,230,000					4,230,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Utilities
Contact UT Director
Type Maintenance
Useful Life 100 years
Category Water
Priority 1 Critical
Status Active

Project # UT1518CIP
Project Name 19th Street Utilities Relocation

Department Priority
Unfunded

Total Project Cost: \$1,531,916

Description

This project will replace waterline on 19th Street from Iowa Street to Alabama Street. The existing waterline is mostly 6-inch and 8-inch cast iron or transite pipe from the late-1940's to mid-1950's that will be replaced with new 8-inch and 12-inch PVC waterlines.

The proposed improvements to 19th Street will also require replacement and/or relocation of portions of the existing 10-inch and 12-inch clay sanitary sewer from Naismith Drive to Stewart Avenue.

Justification

The 19th Street Utilities Relocation project will replace waterline on 19th Street from Iowa Street to Alabama Street. The existing waterline is mostly 6-inch and 8-inch cast iron or transite pipe from the late-1940's to mid-1950's that will be replaced with new 8-inch and 12-inch PVC waterlines. Proposed improvements to 19th Street will also require relocation of the existing 10-inch and 12-inch clay sanitary sewer from Naismith Drive to Stewart Avenue.

Prior	Expenditures	2017	2018	2019	2020	2021	Total
110,996	Construction/Maintenance	1,420,920					1,420,920
Total	Total	1,420,920					1,420,920

Prior	Funding Sources	2017	2018	2019	2020	2021	Total
110,996	Utility - Water	1,420,920					1,420,920
Total	Total	1,420,920					1,420,920

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Utilities
Contact UT Director
Type Maintenance
Useful Life
Category Wastewater
Priority 1 Critical
Status Active

Project # UT1793CIP

Project Name 2017 Sewer Main Relocations for Road Projects

Department Priority
Unfunded

Total Project Cost: \$351,000

Description

2017 Sewer Main Relocations for Road Projects

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	70,200					70,200
Construction/Maintenance	280,800					280,800
Total	351,000					351,000

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Wastewater	351,000					351,000
Total	351,000					351,000

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Utilities
Contact UT Director
Type Maintenance
Useful Life
Category Wastewater
Priority 1 Critical
Status Active

Project # UT1795CIP
Project Name 2017 Rapid I/I Reduction Program

Department Priority
Unfunded

Total Project Cost: \$2,000,000

Description
2017 Rapid I/I Reduction Program

Justification
The Integrated 2012 Wastewater Utilities Plan and Capital Improvements Program recommended the implementation of a Rapid Inflow and Infiltration (I/I) Reduction Program. The objective of the Rapid I/I Reduction Program is an overall 35% reduction of I/I within the program area. By reducing I/I by 35%, we decrease the need for construction projects that add system capacity within the sewer system and the need for wet weather treatment capacity expansion at the Kaw WWTP.

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	400,000					400,000
Construction/Maintenance	1,600,000					1,600,000
Total	2,000,000					2,000,000

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Wastewater	2,000,000					2,000,000
Total	2,000,000					2,000,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Utilities
Contact UT Director
Type Improvement
Useful Life
Category Wastewater
Priority 1 Critical
Status Active

Project # UT1796CIP

Project Name Collection System Field Operations Building

Department Priority
Unfunded

Total Project Cost: \$7,000,000

Description

Collection System Field Operations Building

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	70,000	1,330,000				1,400,000
Construction/Maintenance	280,000	5,320,000				5,600,000
Total	350,000	6,650,000				7,000,000

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Wastewater	350,000	6,650,000				7,000,000
Total	350,000	6,650,000				7,000,000

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Utilities
Contact UT Director
Type Maintenance
Useful Life
Category Water
Priority 1 Critical
Status Active

Project # UT1797CIP
Project Name 2017 Clinton WTP Improvement Program

Department Priority
Unfunded

Total Project Cost: \$324,500

Description
2017 Clinton Water TP Annual Improvement Program

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	64,900					64,900
Construction/Maintenance	259,600					259,600
Total	324,500					324,500

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Water	324,500					324,500
Total	324,500					324,500

Budget Impact/Other

City Manager's Recommended Projects
City of Lawrence, Kansas

Data in Year 2017

Department Utilities
Contact UT Director
Type Improvement
Useful Life
Category Water
Priority 1 Critical
Status Active

Project # UT1798CIP
Project Name 2017 Kaw WTP Improvement Program

Department Priority
Unfunded

Total Project Cost: \$324,500

Description

2017 Kaw Water Treatment Plant Annual Improvement Program

Justification

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	64,900					64,900
Construction/Maintenance	259,600					259,600
Total	324,500					324,500

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Water	324,500					324,500
Total	324,500					324,500

Budget Impact/Other

City Manager's Recommended Projects

Data in Year 2017

City of Lawrence, Kansas

Department Utilities
Contact UT Director
Type Maintenance
Useful Life 50 years
Category Water
Priority 1 Critical
Status Active

Project # UT1799CIP
Project Name 2017 Watermain Replacement/Relocation Program

Department Priority
Unfunded

Total Project Cost: \$3,935,700

Description

2018 Watermain Replacement/Relocation Program, to include watermain assessment and maintenance activities through contractor arrangements and in-house at to-be-identified locations.

Justification

A program for the replacement and rehabilitation of deteriorated and undersized water distribution piping was identified in the 2012 Integrated Water Utility Plan and Capital Improvements Plan. Department of Utilities staff continuously evaluates the condition of the water distribution system based on main breaks, pipe material and age, capacity and fire protection needs. Based on these criteria, along with the criticality of the watermain within the system and the number of services affected, watermains are targeted for replacement.

Continued progress in water main replacement further increases the reliability of the City's water transmission system.

Expenditures	2017	2018	2019	2020	2021	Total
Planning/Design	787,140					787,140
Construction/Maintenance	3,148,560					3,148,560
Total	3,935,700					3,935,700

Funding Sources	2017	2018	2019	2020	2021	Total
Utility - Water	3,935,700					3,935,700
Total	3,935,700					3,935,700

Budget Impact/Other

City of Lawrence, Kansas
City Manager's Recommended Projects
2017 thru 2021

PROJECTS & FUNDING SOURCES BY DEPARTMENT

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
Citizen Input								
Affordable Housing Initiatives	CI05	n/a	300,000	300,000	350,000	350,000	350,000	1,650,000
<i>General Obligation Debt</i>			300,000	300,000	350,000	350,000	350,000	1,650,000
<i>Unfunded</i>			2,700,000	2,700,000	2,650,000	2,650,000	2,650,000	13,350,000
Animal Shelter Construction Partnership	CM1701CIP	n/a		7,500,000				7,500,000
<i>General Obligation Debt</i>						2,500,000		2,500,000
<i>Private Partnership</i>				5,000,000				5,000,000
Citizen Input Total			300,000	7,800,000	350,000	350,000	350,000	9,150,000
Finance								
Upgrade Accounting System	FA1601CP	3	10,000	124,000				134,000
<i>General Fund</i>			5,000	62,000				67,000
<i>Utility - Water</i>			5,000	62,000				67,000
Finance Total			10,000	124,000				134,000
Fire Medical								
Fire Station No. 1 Rehabilitation / Senior Center	FM1702CIP	1	6,000,000					6,000,000
<i>General Obligation Debt</i>			5,480,000					5,480,000
<i>Intergovernmental</i>			520,000					520,000
Training Burn Tower Replacement	FM1703CIP	1	350,000	350,000				700,000
<i>General Obligation Debt</i>			350,000	350,000				700,000
Emergency Vehicle Traffic Signal Preemption System	FM1705CIP	1	719,000					719,000
<i>General Obligation Debt</i>			719,000					719,000
SCBA Air Compressor Fire Station Number 1	FM1706CIP	3	50,000					50,000
<i>Equipment Reserve Fund</i>			50,000					50,000
Personal Protective Equipment Extractors	FM1804CIP	4		75,000				75,000
<i>Equipment Reserve Fund</i>				75,000				75,000
Apparatus Bay Exhaust System	FM1807CIP	1		200,000				200,000
<i>General Obligation Debt</i>				200,000				200,000
641 Replacement Quint 40	FM1817CIP	3		1,250,000				1,250,000
<i>General Obligation Debt</i>				1,250,000				1,250,000
Personal Protective Equipment Alternate Set	FM1908CIP	2			540,000			540,000
<i>General Obligation Debt</i>					540,000			540,000
Rescue Equipment for Rescue 5	FM1914CIP	2			75,000			75,000
<i>Equipment Reserve Fund</i>					75,000			75,000
645 Replacement Rescue 5	FM1920CIP	2			760,000			760,000
<i>General Obligation Debt</i>					760,000			760,000
Mobile Radios	FM2009CIP	1				600,000		600,000
<i>General Obligation Debt</i>						600,000		600,000
Portable Radios	FM2010CIP	1				900,000		900,000
<i>General Obligation Debt</i>						900,000		900,000
Training Center Remodel	FM2045CIP	4				250,000		250,000
<i>General Obligation Debt</i>						250,000		250,000

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
634 Replacement Investigation Unit 1 <i>General Obligation Debt</i>	FM2126CIP	3					375,000	375,000
							375,000	375,000
642 Replacement Quint 20 <i>General Obligation Debt</i>	FM2127CIP	2					1,287,500	1,287,500
							1,287,500	1,287,500
643 Replacement Quint 50 <i>General Obligation Debt</i>	FM2128CIP	2					1,287,500	1,287,500
							1,287,500	1,287,500
Fire Medical Total			7,119,000	1,875,000	1,375,000	1,750,000	2,950,000	15,069,000

Information Technology

Access Layer Switches <i>Equipment Reserve Fund</i>	IT1701CIP	1	49,500	61,000	59,500			170,000
			49,500	61,000	59,500			170,000
VMWare Hardware Refresh <i>Equipment Reserve Fund</i>	IT2001CIP	1				80,000		80,000
						80,000		80,000
Information Technology Total			49,500	61,000	59,500	80,000		250,000

Parks and Recreation

Parks & Recreation Maintenance and Repairs <i>General Fund</i>	PR1701CIP	1	700,000					700,000
			700,000					700,000
Replace HVAC Units- Indoor Pool <i>General Fund</i>	PR1702CIP	1	330,000					330,000
			330,000					330,000
Install additional restrooms at YSC (ADA comp) <i>General Fund</i>	PR1704CIP	2	120,000					120,000
			120,000					120,000
Resurface North Parking at Holcom Complex <i>General Fund</i>	PR1706CIP	3		104,000				104,000
				104,000				104,000
Downtown Brick Pavers <i>Guest Tax Fund</i>	PR1707CIP	2	75,000					75,000
			75,000					75,000
Upgrade EBGC Pro Shop,Concessions,Restroom <i>Golf Course</i>	PR1708CIP	4	500,000					500,000
			500,000					500,000
East Lawrence Center Parking Lot Addition <i>General Fund</i>	PR1710CIP	3		83,000				83,000
				83,000				83,000
Install Trail to Connect 29th St. to Haskell <i>General Fund</i>	PR1713CIP	2	75,000					75,000
			75,000					75,000
Equipment Replacement - Recreation Division Bus <i>General Fund</i>	PR1781CIP	1	85,000					85,000
			85,000					85,000
Parks & Recreation Maintenance and Repairs <i>General Fund</i>	PR1801CIP	1		565,000				565,000
				565,000				565,000
Install Concessions at West End of YSC Complex <i>General Fund</i>	PR1802CIP	4		125,000				125,000
				125,000				125,000
Replace Pool Slide at Outdoor Aquatics Center <i>General Fund</i>	PR1803CIP	3		95,000				95,000
				95,000				95,000
Replace Bad Concrete in Outdoor Pool Deck <i>General Fund</i>	PR1804CIP	2		75,000				75,000
				75,000				75,000
Outdoor Aquatic Center Pool Paint <i>General Fund</i>	PR1805CIP	2		75,000				75,000
				75,000				75,000
Speicher Road asphalt patches and 2 inch overlay <i>General Fund</i>	PR1808CIP	3		100,000				100,000
				100,000				100,000
Downtown Brick Pavers <i>Guest Tax Fund</i>	PR1809CIP	2		75,000				75,000
				75,000				75,000
Cemetery Roads <i>Cemetery Reserve</i>	PR1810CIP	3		75,000				75,000
				75,000				75,000
Replace HVAC Units - Indoor Aquatics Center <i>General Fund</i>	PR1811CIP	1		330,000				330,000
				330,000				330,000

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
Indoor Aquatic Center--Add Event Space <i>General Obligation Debt</i>	PR1812CIP	4		900,000 900,000				900,000 900,000
Holcom Park Center - HVAC Replacement (Gym) <i>General Fund</i>	PR1814CIP	2		250,000 250,000				250,000 250,000
HPSC Ball Diamond Light Replacement <i>General Fund</i>	PR1815CIP	2		500,000 500,000				500,000 500,000
Parks & Recreation Maintenance and Repairs <i>General Fund</i>	PR1901CIP	1			576,000 576,000			576,000 576,000
Prairie Park Nature Center Playground (ADA) <i>General Fund</i>	PR1903CIP	2			60,000 60,000			60,000 60,000
Broken Arrow Park Parking Lot <i>General Fund</i>	PR1905CIP	3			75,000 75,000			75,000 75,000
East Lawrence Recreation Center Renovation <i>General Fund</i>	PR1906CIP	2			75,000 75,000			75,000 75,000
Holcom Park Recreation Center Renovation <i>General Fund</i>	PR1907CIP	2			75,000 75,000			75,000 75,000
Carnegie Building tuck-point (Historic Building) <i>Guest Tax Fund</i>	PR1908CIP	2			150,000 150,000			150,000 150,000
Union Pacific Depot Renovations <i>General Fund</i>	PR1909CIP	4			75,000 75,000			75,000 75,000
Burcham Park/Upgrade Shelter and Playground <i>General Fund</i>	PR1910CIP	4			100,000 100,000			100,000 100,000
Veterans Park Courts <i>General Fund</i>	PR1911CIP	4			115,000 115,000			115,000 115,000
Peterson Park New Development <i>General Fund</i>	PR1912CIP	2			200,000 200,000			200,000 200,000
Downtown Pavers <i>Guest Tax Fund</i>	PR1913CIP	1			75,000 75,000			75,000 75,000
Clinton Lake Regional Park - 27th street <i>General Obligation Debt</i>	PR1914CIP	2			400,000 400,000			400,000 400,000
Replace Asphalt in Parking Lot at Arboretum <i>General Fund</i>	PR1915CIP	3			35,000 35,000			35,000 35,000
Park Land Acquisition - South <i>General Obligation Debt</i>	PR1916CIP	2			300,000 300,000			300,000 300,000
Parks & Recreation - Trash Truck Replacement <i>General Fund</i>	PR1981CIP	1			80,000 80,000			80,000 80,000
Parks & Recreation - Backhoe Replacement <i>General Fund</i>	PR1982CIP	1			80,000 80,000			80,000 80,000
Parks & Recreation Maintenance and Repairs <i>General Fund</i>	PR2001CIP	1				588,000 588,000		588,000 588,000
Overland Drive Park Development <i>General Obligation Debt</i>	PR2002CIP	4				200,000 200,000		200,000 200,000
SPL Equipment Storage Building <i>General Obligation Debt</i>	PR2003CIP	4				150,000 150,000		150,000 150,000
Develop Green Meadows Park <i>General Obligation Debt</i>	PR2004CIP	4				150,000 150,000		150,000 150,000
Indoor Aquatic Center Pool Paint <i>General Fund</i>	PR2005CIP	3				75,000 75,000		75,000 75,000
Lyons Park Playground/Shelter Replacement <i>General Fund</i>	PR2006CIP	2				120,000 120,000		120,000 120,000
Replace Shelter Add Restroom in Deerfield <i>General Fund</i>	PR2007CIP	4				120,000 120,000		120,000 120,000
McDonald Drive - KTA City Entrance landscape <i>General Fund</i>	PR2008CIP	4				75,000 75,000		75,000 75,000

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
Edgewood Park-Upgrade courts and ball diamond <i>General Fund</i>	PR2009CIP	4				75,000 75,000		75,000 75,000
Farmers Market <i>Guest Tax Fund</i>	PR2011CIP	1				200,000 200,000		200,000 200,000
Park Land Acquisition - West <i>General Obligation Debt</i>	PR2012CIP	1				300,000 300,000		300,000 300,000
Parks & Recreation - Rollback Truck <i>General Obligation Debt</i>	PR2081CIP	4				100,000 100,000		100,000 100,000
Parks & Recreation Maintenance and Repairs <i>General Fund</i>	PR2101CIP	1					600,000 600,000	600,000 600,000
Equipment Replacement - Chipper Truck <i>General Obligation Debt</i>	PR2181CIP	2					100,000 100,000	100,000 100,000
Parks and Recreation Total			1,885,000	3,352,000	2,471,000	2,153,000	700,000	10,561,000

Planning & Development

One Stop Shop <i>General Obligation Debt</i>	PS1701CIP	4	30,000 30,000	2,050,000 2,050,000				2,080,000 2,080,000
Planning & Development Total			30,000	2,050,000				2,080,000

Police

Mobile Data Computers <i>General Obligation Debt</i>	PD1701CIP	1	672,100 672,100					672,100 672,100
Vehicles <i>General Fund</i>	PD1702CIP	1	392,000 392,000	399,840 399,840	407,837 407,837	415,994 415,994	424,313 424,313	2,039,984 2,039,984
ITC Deferred Maintenance <i>General Obligation Debt</i>	PD1704CIP	1		619,000 619,000				619,000 619,000
FARO Scanner <i>General Obligation Debt</i>	PD1705CIP	2	95,000 95,000					95,000 95,000
Firearms Simulator <i>General Obligation Debt</i>	PD1706CIP	2		300,000 300,000				300,000 300,000
Parking Garage Paystations <i>Public Parking</i>	PD1707CIP	1	99,000 99,000					99,000 99,000
Design Police Facility <i>General Obligation Debt</i>	PD1708CIP	n/a	1,500,000 1,500,000					1,500,000 1,500,000
Police Total			2,758,100	1,318,840	407,837	415,994	424,313	5,325,084

Public Transit

Multi Modal Facility <i>Intergovernmental Public Transit Fund</i>	TI01	n/a	30,600,000 26,600,000 4,000,000					30,600,000 26,600,000 4,000,000
Transit Shelters and Additional Amenities <i>Public Transit Fund</i>	TI02	n/a	150,000 150,000	150,000 150,000	150,000 150,000	150,000 150,000	150,000 150,000	750,000 750,000
Fixed Route Transit Buses <i>Public Transit Fund</i>	TI03	n/a	1,500,000 1,500,000	500,000 500,000	500,000 500,000	500,000 500,000	500,000 500,000	3,000,000 3,500,000
Public Transit Total			32,250,000	150,000	650,000	650,000	650,000	34,350,000

Public Works

31st - Haskell to O'Connell <i>General Obligation Debt</i>	18P07S	1		1,000,000 1,000,000				1,000,000 1,000,000
Bike / Ped Improvements / ADA Ramps	CI09	2	450,000	450,000	450,000	450,000	450,000	2,250,000

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
<i>Capital Improvement Reserve Fund - Infrastructure</i>			200,000	200,000	200,000	200,000	200,000	1,000,000
<i>General Obligation Debt</i>			250,000	250,000	250,000	250,000	250,000	1,250,000
<i>Unfunded</i>			550,000	550,000	550,000	550,000	550,000	2,750,000
KLINK	PW1701Kcip	n/a	600,000	600,000	600,000	600,000	600,000	3,000,000
<i>General Obligation Debt</i>			300,000	300,000	300,000	300,000	300,000	1,500,000
<i>Intergovernmental</i>			300,000	300,000	300,000	300,000	300,000	1,500,000
Annual Vehicle Replacement Program	PW1702CIP	2	500,000	500,000	500,000	500,000	500,000	2,500,000
<i>Capital Improvement Reserve Fund</i>			500,000	500,000	500,000	500,000	500,000	2,500,000
Parking garage assessments/maintenance	PW17B10CIP	4	150,000					150,000
<i>Public Parking</i>			150,000					150,000
Roof replacement F/M #3	PW17B1CIP	4	140,000					140,000
<i>General Obligation Debt</i>			140,000					140,000
Fire Medical roof and drainage Maintenance	PW17B2CIP	4	90,000					90,000
<i>General Obligation Debt</i>			90,000					90,000
Downtown Canopy Lighting sidewalk and roofing	PW17B4CIP	2	350,000					350,000
<i>General Obligation Debt</i>			350,000					350,000
Community Health Boiler replacement	PW17B5CIP	5	225,000					225,000
<i>General Obligation Debt</i>			112,500					112,500
<i>Intergovernmental</i>			112,500					112,500
Community Health Chiller replacement	PW17B6CIP	4	325,000					325,000
<i>General Obligation Debt</i>			162,500					162,500
<i>Intergovernmental</i>			162,500					162,500
Community Health Building Stabilization	PW17B7CIP	4	75,000					75,000
<i>General Obligation Debt</i>			37,500					37,500
<i>Intergovernmental</i>			37,500					37,500
19th Street - Iowa to Naismith	PW17E10CIP	1		2,000,000				2,000,000
<i>Capital Improvement Reserve Fund - Infrastructure</i>				2,000,000				2,000,000
Kasold Reconstruction - 6th St to Bob Billings Pkwy	PW17E1CIP	1	5,000,000					5,000,000
<i>Capital Improvement Reserve Fund - Infrastructure</i>			4,500,000					4,500,000
<i>Intergovernmental</i>			500,000					500,000
Wakarusa Reconstruction - Inverness to 6th Street	PW17E2CIP	1	3,000,000					3,000,000
<i>Capital Improvement Reserve Fund - Infrastructure</i>			2,500,000					2,500,000
<i>Intergovernmental</i>			500,000					500,000
19th Street Reconstruction - Harper to O'Connell	PW17E3CIP	1		2,100,000				2,100,000
<i>General Obligation Debt</i>				1,350,000				1,350,000
<i>Intergovernmental</i>				750,000				750,000
Queens Road Improvements - 6th to City Limits	PW17E4CIP	1	300,000	3,500,000				3,800,000
<i>General Obligation Debt</i>			300,000	3,500,000				3,800,000
CDBG Sidewalk Gap Program	PW17E7CIP	2	100,000					100,000
<i>Intergovernmental</i>			100,000					100,000
Traffic Calming	PW17E8CIP	4	200,000	200,000	200,000	200,000	200,000	1,000,000
<i>General Obligation Debt</i>			200,000	200,000	200,000	200,000	200,000	1,000,000
Bowersock Dam	PW17E9CIP	2		1,000,000				1,000,000
<i>Utility - Water</i>				1,000,000				1,000,000
313 Bobcat Replacement	PW17F10CIP	4	75,000					75,000
<i>General Obligation Debt</i>			75,000					75,000
Street Sweeper	PW17F1CIP	4	280,000					280,000
<i>Stormwater Fund</i>			280,000					280,000
366 Class 5 dump truck replacement	PW17F2CIP	3	105,000					105,000
<i>Stormwater Fund</i>			105,000					105,000
331 Tandam axle dump truck replacement	PW17F3CIP	4	195,000					195,000
<i>Special Gas Tax Fund</i>			195,000					195,000
337 Class 5 dump truck replacement	PW17F4CIP	4	105,000					105,000
<i>Special Gas Tax Fund</i>			105,000					105,000
394 Rubber tire loader replacement	PW17F9CIP	4	75,000					75,000

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
<i>Stormwater Fund</i>			75,000					75,000
2017 Contracted Street Maintenance Program	PW17SM1CIP	1	3,140,000	3,140,000	3,140,000	3,140,000	3,140,000	15,700,000
<i>Capital Improvement Reserve Fund - Infrastructure</i>			800,000	800,000	800,000	800,000	800,000	4,000,000
<i>General Fund</i>			2,000,000	2,000,000	2,000,000	2,000,000	2,000,000	10,000,000
<i>Special Gas Tax Fund</i>			200,000	200,000	200,000	200,000	200,000	1,000,000
<i>Stormwater Fund</i>			140,000	140,000	140,000	140,000	140,000	700,000
<i>Unfunded</i>			2,860,000	2,860,000	2,860,000	5,720,000	2,860,000	17,160,000
Contract Milling for In House Pavement Rehab	PW17SM4CIP	4	100,000	100,000	100,000	100,000	100,000	500,000
<i>Special Gas Tax Fund</i>			100,000	100,000	100,000	100,000	100,000	500,000
ITS Video Detection/ upgrade and replacement	PW17SM5CIP	2	150,000	153,000	156,060	159,181	162,365	780,606
<i>Special Gas Tax Fund</i>			150,000	153,000	156,060	159,181	162,365	780,606
Fire/Medical HVAC&General repairs	PW18B1CIP	4		120,000				120,000
<i>General Obligation Debt</i>				120,000				120,000
Community Health Roof replacement	PW18B3CIP	4		500,000				500,000
<i>General Obligation Debt</i>				250,000				250,000
<i>Intergovernmental</i>				250,000				250,000
Generator replacement Community Health	PW18B4CIP	4		80,000				80,000
<i>General Obligation Debt</i>				80,000				80,000
City Hall fire alarm panel and general maint.	PW18B5CIP	4		120,000				120,000
<i>General Obligation Debt</i>				120,000				120,000
Remodel Traffic Bldg 445 Mississippi	PW18B6CIP	4		240,000				240,000
<i>General Obligation Debt</i>				240,000				240,000
Vehicle Maintenance Garage feasibilty study	PW18B8CIP	4		100,000				100,000
<i>General Fund</i>				100,000				100,000
Kasold - Clinton Pkwy to HyVee	PW18E2CIP	1		1,000,000				1,000,000
<i>General Obligation Debt</i>				1,000,000				1,000,000
23rd Street - Haskell Bridge to East City Limits	PW18E3CIP	1			6,500,000	2,500,000		9,000,000
<i>General Obligation Debt</i>					2,500,000	2,500,000		5,000,000
<i>Intergovernmental</i>					4,000,000			4,000,000
CDBG Sidewalk Gap Program	PW18E6CIP	2		100,000				100,000
<i>Intergovernmental</i>				100,000				100,000
397 Class 5 dump truck replacement	PW18F10CIP	4		100,000				100,000
<i>Special Gas Tax Fund</i>				100,000				100,000
367 Bobcat replacement	PW18F11CIP	3	75,000					75,000
<i>General Obligation Debt</i>			75,000					75,000
361 Street sweeper replacement	PW18F6CIP	4		290,000				290,000
<i>Stormwater Fund</i>				290,000				290,000
Backhoe Lease	PW18F7CIP	4		150,000				150,000
<i>Special Gas Tax Fund</i>				50,000				50,000
<i>Stormwater Fund</i>				100,000				100,000
328 Tandem axle dump truck replacement	PW18F8CIP	4		185,000				185,000
<i>General Obligation Debt</i>				185,000				185,000
396 Class 5 dump truck replacement	PW18F9CIP	3		100,000				100,000
<i>Special Gas Tax Fund</i>				100,000				100,000
Farmland Pond Cap	PW18V01CIP	n/a		1,000,000				1,000,000
<i>Farmland Remediation</i>				1,000,000				1,000,000
Pole lighting and HVAC upgrade Airport	PW19A1CIP	3			75,000			75,000
<i>General Obligation Debt</i>					75,000			75,000
Pavement Maintenance airport	PW19A2CIP	4			100,000			100,000
<i>General Obligation Debt</i>					100,000			100,000
Fire/medical HVAC replacement	PW19B1CIP	1			100,000			100,000
<i>General Obligation Debt</i>					100,000			100,000
Downtown parking lot maintenance	PW19B3CIP	3			200,000			200,000
<i>General Obligation Debt</i>					200,000			200,000
ITC Exterior repairs and HVAC	PW19B4CIP	1			750,000			750,000

[illegible]

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
Public Works Total			15,805,000	18,828,000	16,697,060	11,894,181	7,907,365	71,131,606
Public Works - Airport								
Wildlifence	PW17A1CIP	2	805,000					805,000
<i>Airport</i>			80,500					80,500
<i>Intergovernmental</i>			724,500					724,500
Reconstruct Terminal Apron	PW18A2CIP	2	136,000	1,224,000				1,360,000
<i>General Obligation Debt</i>				136,000				136,000
<i>Intergovernmental</i>			136,000	1,088,000				1,224,000
Reconstruct RWY15-33	PW19A3CIP	3		125,000	1,875,000			2,000,000
<i>General Obligation Debt</i>					200,000			200,000
<i>Intergovernmental</i>				125,000	1,675,000			1,800,000
Land Acquisition RWY1/19	PW19A4CIP	3			190,000			190,000
<i>Airport</i>					19,000			19,000
<i>Intergovernmental</i>					171,000			171,000
Drainage Improvements	PW20A5CIP	3			150,000	1,350,000		1,500,000
<i>Intergovernmental</i>						1,350,000		1,350,000
<i>Stormwater Fund</i>					150,000			150,000
Public Works - Airport Total			941,000	1,349,000	2,215,000	1,350,000		5,855,000
Public Works - Solid Waste								
426 Automated Side Load Refuse truck	PW17F5CIP	4	225,500					225,500
<i>Solid Waste Fund</i>			225,500					225,500
427 Dual purpose front load and ASL	PW17F6CIP	4	275,000					275,000
<i>Solid Waste Fund</i>			275,000					275,000
421 Rear load refuse truck	PW17F7CIP	4	160,000					160,000
<i>Solid Waste Fund</i>			160,000					160,000
489 Rolloff container truck repalcement	PW17F8CIP	4	140,000					140,000
<i>Solid Waste Fund</i>			140,000					140,000
Solid Waste Facility at Kresge	PW17SW1	1	2,700,000					2,700,000
<i>Solid Waste Fund</i>			2,700,000					2,700,000
475 Small rear load refuse truck replacement	PW18F1CIP	4		90,000				90,000
<i>Solid Waste Fund</i>				90,000				90,000
428 Rear load refuse truck replacement	PW18F2CIP	4		140,000				140,000
<i>Solid Waste Fund</i>				140,000				140,000
Automated Side Load Unit	PW18F3CIP	3		230,000				230,000
<i>Solid Waste Fund</i>				230,000				230,000
490 Rubber tire loader replacement	PW18F4CIP	4		195,000				195,000
<i>Solid Waste Fund</i>				195,000				195,000
439 Roll off container truck replacement	PW18F5CIP	4		160,000				160,000
<i>Solid Waste Fund</i>				160,000				160,000
413 Front load refuse truck replacement	PW19F1CIP	3			250,000			250,000
<i>Solid Waste Fund</i>					250,000			250,000
499 Small conatiner truck replacement	PW19F2CIP	3			110,000			110,000
<i>Solid Waste Fund</i>					110,000			110,000
430 Rear load refuse truck replacement	PW19F3CIP	3			170,000			170,000
<i>Solid Waste Fund</i>					170,000			170,000
434 Rear load refuse truck replacement	PW19F4CIP	3			170,000			170,000
<i>Solid Waste Fund</i>					170,000			170,000
435 Rear load refuse truck replacement	PW19F5CIP	3			170,000			170,000
<i>Solid Waste Fund</i>					170,000			170,000
468 Rear load refuse truck replacement	PW19F6CIP	3			170,000			170,000
<i>Solid Waste Fund</i>					170,000			170,000

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
447 Roll off container truck replacement <i>Solid Waste Fund</i>	PW20F1CIP	3				170,000 <i>170,000</i>		<i>170,000</i>
414 Front load refuse truck replacement <i>Solid Waste Fund</i>	PW20F2CIP	3				240,000 <i>240,000</i>		<i>240,000</i>
437 Automated side load refuse truck <i>Solid Waste Fund</i>	PW20F3CIP	3				230,000 <i>230,000</i>		<i>230,000</i>
436 Automated side load refuse truck <i>Solid Waste Fund</i>	PW20F4CIP	3				230,000 <i>230,000</i>		<i>230,000</i>
433 Rear load refuse truck replacement <i>Solid Waste Fund</i>	PW20F5CIP	3				170,000 <i>170,000</i>		<i>170,000</i>
449 Automated side load refuse truck <i>Solid Waste Fund</i>	PW21F1CIP	4					240,000 <i>240,000</i>	<i>240,000</i>
448 Automated side load refuse truck <i>Solid Waste Fund</i>	PW21F2CIP	4					240,000 <i>240,000</i>	<i>240,000</i>
445 Roll off container truck replacement <i>Solid Waste Fund</i>	PW21F3CIP	4					170,000 <i>170,000</i>	<i>170,000</i>
444 Roll off container truck replacement <i>Solid Waste Fund</i>	PW21F4CIP	4					170,000 <i>170,000</i>	<i>170,000</i>
486 Rubber tire loader replacement <i>Solid Waste Fund</i>	PW21F5CIP	4					200,000 <i>200,000</i>	<i>200,000</i>
415 Front load refuse truck replacement <i>Solid Waste Fund</i>	PW21F6CIP	4					255,000 <i>255,000</i>	<i>255,000</i>
Public Works - Solid Waste Total			3,500,500	815,000	1,040,000	1,040,000	1,275,000	<i>7,670,500</i>

Public Works - Stormwater

Storm Sewer Video Inspection Unit <i>Stormwater Fund</i>	PW17S1CIP	3	150,000 <i>150,000</i>					<i>150,000</i>
13th Brook Drainage Improvement Improvements <i>Stormwater Fund</i>	PW17S2CIP	4	250,000 <i>250,000</i>					<i>250,000</i>
Storm Water Culvert Lining <i>Stormwater Fund</i>	PW17S3CIP	3	250,000 <i>250,000</i>	250,000 <i>250,000</i>	250,000 <i>250,000</i>	250,000 <i>250,000</i>	250,000 <i>250,000</i>	<i>1,250,000</i>
Naismith Drainage Channel <i>Stormwater Fund</i>	PW18S1CIP	1		2,500,000 <i>2,500,000</i>				<i>2,500,000</i>
23rd & Ousdahl <i>Stormwater Fund</i>	PW18S2CIP	2		2,400,000 <i>2,400,000</i>				<i>2,400,000</i>
17th and Alabama Drainage Improvement <i>Stormwater Fund</i>	PW19S1CIP	1			2,500,000 <i>2,500,000</i>			<i>2,500,000</i>
Concrete Channel W of Arrowhead Princeton to Peter <i>Stormwater Fund</i>	PW20S1CIP	3				1,000,000 <i>1,000,000</i>		<i>1,000,000</i>
19th St Maple Ln to Brook <i>Stormwater Fund</i>	PW21S1CIP	2					2,000,000 <i>2,000,000</i>	<i>2,000,000</i>
Public Works - Stormwater Total			650,000	5,150,000	2,750,000	1,250,000	2,250,000	<i>12,050,000</i>

Utilities

Wakarusa WWTP and Conveyance Corridor <i>Utility - Wastewater</i>	UT1304CIP	1	29,300,000 <i>29,300,000</i>					<i>29,300,000</i>
Oread Storage Tank & Booster Pump Stn Replacement <i>Utility - Water</i>	UT1307CIP	1	4,230,000 <i>4,230,000</i>					<i>4,230,000</i>
23rd St Waterline Replacement, Ousdahl to Alabama <i>Utility - Water</i>	UT1504CIP	1		540,000 <i>540,000</i>				<i>540,000</i>
23rd and Ousdahl Waterline Replacement <i>Utility - Water</i>	UT1505CIP	1			210,000 <i>210,000</i>			<i>210,000</i>

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
19th Street Utilities Relocation <i>Utility - Water</i>	UT1518CIP	1	1,420,920 <i>1,420,920</i>					1,420,920 <i>1,420,920</i>
2017 Sewer Main Relocations for Road Projects <i>Utility - Wastewater</i>	UT1793CIP	1	351,000 <i>351,000</i>					351,000 <i>351,000</i>
2017 Rapid I/I Reduction Program <i>Utility - Wastewater</i>	UT1795CIP	1	2,000,000 <i>2,000,000</i>					2,000,000 <i>2,000,000</i>
Collection System Field Operations Building <i>Utility - Wastewater</i>	UT1796CIP	1	350,000 <i>350,000</i>	6,650,000 <i>6,650,000</i>				7,000,000 <i>7,000,000</i>
2017 Clinton WTP Improvement Program <i>Utility - Water</i>	UT1797CIP	1	324,500 <i>324,500</i>					324,500 <i>324,500</i>
2017 Kaw WTP Improvement Program <i>Utility - Water</i>	UT1798CIP	1	324,500 <i>324,500</i>					324,500 <i>324,500</i>
2017 Watermain Replacement/Relocation Program <i>Utility - Water</i>	UT1799CIP	1	3,935,700 <i>3,935,700</i>					3,935,700 <i>3,935,700</i>
2018 Sewer Main Relocations for Road Projects <i>Utility - Wastewater</i>	UT1886CIP	1		390,000 <i>390,000</i>				390,000 <i>390,000</i>
2018 WW Failed Infrastructure Contingency <i>Utility - Wastewater</i>	UT1887CIP	1		390,000 <i>390,000</i>				390,000 <i>390,000</i>
2018 Pump Station Annual Improvements <i>Utility - Wastewater</i>	UT1888CIP	1		140,000 <i>140,000</i>				140,000 <i>140,000</i>
2018 Kansas River WWTP Annual Improvements <i>Utility - Wastewater</i>	UT1889CIP	1		390,000 <i>390,000</i>				390,000 <i>390,000</i>
2018 Clay Pipe/Manhole Rehabilitation <i>Utility - Wastewater</i>	UT1890CIP	1		1,130,000 <i>1,130,000</i>				1,130,000 <i>1,130,000</i>
2018 Rapid I/I Reduction Program <i>Utility - Wastewater</i>	UT1891CIP	1		2,830,000 <i>2,830,000</i>				2,830,000 <i>2,830,000</i>
PS #8 Elimination - 21" Gravity Sewer <i>Utility - Wastewater</i>	UT1892CIP	1		450,000 <i>450,000</i>	4,160,000 <i>4,160,000</i>			4,610,000 <i>4,610,000</i>
2018 Clinton WTP Improvement Program <i>Utility - Water</i>	UT1893CIP	1					450,000 <i>450,000</i>	450,000 <i>450,000</i>
2018 Kaw WTP Improvement Program <i>Utility - Water</i>	UT1894CIP	1		400,000 <i>400,000</i>				400,000 <i>400,000</i>
2018 Watermain Replacement/Relocation Program <i>Utility - Water</i>	UT1895CIP	1		4,190,000 <i>4,190,000</i>				4,190,000 <i>4,190,000</i>
Vermont Bridge Concrete Main Assessment <i>Utility - Water</i>	UT1896CIP	1		400,000 <i>400,000</i>				400,000 <i>400,000</i>
2018 Kaw Water TP Infrastructure Rehab <i>Utility - Water</i>	UT1897CIP	1		1,000,000 <i>1,000,000</i>				1,000,000 <i>1,000,000</i>
Automated Meter Reading Installation <i>Utility - Water</i>	UT1898CIP	1		7,880,000 <i>7,880,000</i>	8,190,000 <i>8,190,000</i>			16,070,000 <i>16,070,000</i>
Stratford Tower Replacement <i>Utility - Water</i>	UT1899CIP	1		360,000 <i>360,000</i>	3,290,000 <i>3,290,000</i>			3,650,000 <i>3,650,000</i>
2019 Sewer Main Relocations for Road Projects <i>Utility - Wastewater</i>	UT1989CIP	1			400,000 <i>400,000</i>			400,000 <i>400,000</i>
2019 WW Failed Infrastructure Contingency <i>Utility - Wastewater</i>	UT1990CIP	1			400,000 <i>400,000</i>			400,000 <i>400,000</i>
2019 Pump Station Annual Improvements <i>Utility - Wastewater</i>	UT1991CIP	1			150,000 <i>150,000</i>			150,000 <i>150,000</i>
2019 Kansas River WWTP Annual Improvements <i>Utility - Wastewater</i>	UT1992CIP	1			400,000 <i>400,000</i>			400,000 <i>400,000</i>
2019 Clay Pipe/Manhole Rehabilitation <i>Utility - Wastewater</i>	UT1993CIP	1			3,510,000 <i>3,510,000</i>			3,510,000 <i>3,510,000</i>
2019 Rapid I/I Reduction Program <i>Utility - Wastewater</i>	UT1994CIP	1			2,950,000 <i>2,950,000</i>			2,950,000 <i>2,950,000</i>

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
Kansas River WWTP Nutrient Removal Pilot <i>Utility - Wastewater</i>	UT1995CIP	1			590,000 <i>590,000</i>	610,000 <i>610,000</i>		1,200,000 <i>1,200,000</i>
Pump Station 25 Expansion to 4 MG <i>Utility - Wastewater</i>	UT1996CIP	1			200,000 <i>200,000</i>			200,000 <i>200,000</i>
2019 Clinton WTP Improvement Program <i>Utility - Water</i>	UT1997CIP	1			410,000 <i>410,000</i>			410,000 <i>410,000</i>
2019 Kaw WTP Improvement Program <i>Utility - Water</i>	UT1998CIP	1			410,000 <i>410,000</i>			410,000 <i>410,000</i>
2019 Watermain Replacement/Relocation Program <i>Utility - Water</i>	UT1999CIP	1			4,360,000 <i>4,360,000</i>			4,360,000 <i>4,360,000</i>
2020 Sewer Main Relocations for Road Projects <i>Utility - Wastewater</i>	UT2084CIP	1				420,000 <i>420,000</i>		420,000 <i>420,000</i>
2020 WW Failed Infrastructure Contingency <i>Utility - Wastewater</i>	UT2085CIP	1				420,000 <i>420,000</i>		420,000 <i>420,000</i>
2020 Pump Station Annual Improvements <i>Utility - Wastewater</i>	UT2086CIP	1				150,000 <i>150,000</i>		150,000 <i>150,000</i>
2020 Kansas River WWTP Annual Improvements <i>Utility - Wastewater</i>	UT2087CIP	1				420,000 <i>420,000</i>		420,000 <i>420,000</i>
2020 Clay Pipe/Manhole Rehabilitation <i>Utility - Wastewater</i>	UT2088CIP	1				1,220,000 <i>1,220,000</i>		1,220,000 <i>1,220,000</i>
2020 Rapid I/I Reduction Program <i>Utility - Wastewater</i>	UT2089CIP	1				3,060,000 <i>3,060,000</i>		3,060,000 <i>3,060,000</i>
Pump Station 16 Upstream Interceptor Rehab <i>Utility - Wastewater</i>	UT2090CIP	1				1,270,000 <i>1,270,000</i>		1,270,000 <i>1,270,000</i>
Pump Station 23 Submersible <i>Utility - Wastewater</i>	UT2091CIP	1				420,000 <i>420,000</i>		420,000 <i>420,000</i>
Pump Station 9 Expansion to 14 MG <i>Utility - Wastewater</i>	UT2092CIP	1				3,160,000 <i>3,160,000</i>		3,160,000 <i>3,160,000</i>
2020 Clinton WTP Improvement Program <i>Utility - Water</i>	UT2093CIP	1				430,000 <i>430,000</i>		430,000 <i>430,000</i>
Clinton WTP Plant Piping <i>Utility - Water</i>	UT2094CIP	1				3,650,000 <i>3,650,000</i>		3,650,000 <i>3,650,000</i>
2020 Kaw WTP Improvement Program <i>Utility - Water</i>	UT2095CIP	1				430,000 <i>430,000</i>		430,000 <i>430,000</i>
2020 Kaw WTP Infrastructure Rehab <i>Utility - Water</i>	UT2096CIP	1				4,160,000 <i>4,160,000</i>		4,160,000 <i>4,160,000</i>
2020 Watermain Replacement/Relocation Program <i>Utility - Water</i>	UT2097CIP	1				4,540,000 <i>4,540,000</i>		4,540,000 <i>4,540,000</i>
Kanwaka Pressure Zone <i>Utility - Water</i>	UT2098CIP	1				850,000 <i>850,000</i>	7,940,000 <i>7,940,000</i>	8,790,000 <i>8,790,000</i>
Clinton Storage Tanks Maintenance/ Coatings <i>Utility - Water</i>	UT2099CIP	1				2,440,000 <i>2,440,000</i>		2,440,000 <i>2,440,000</i>
2021 Sewer Main Relocations for Road Projects <i>Utility - Wastewater</i>	UT2189CIP	1					440,000 <i>440,000</i>	440,000 <i>440,000</i>
2021 WW Failed Infrastructure Contingency <i>Utility - Wastewater</i>	UT2190CIP	1					440,000 <i>440,000</i>	440,000 <i>440,000</i>
2021 Pump Station Annual Improvements <i>Utility - Wastewater</i>	UT2191CIP	1					160,000 <i>160,000</i>	160,000 <i>160,000</i>
2021 WWTP Annual Improvements (2 PLANTS) <i>Utility - Wastewater</i>	UT2192CIP	1					870,000 <i>870,000</i>	870,000 <i>870,000</i>
2021 Clay Pipe/Manhole Rehabilitation <i>Utility - Wastewater</i>	UT2193CIP	1					5,070,000 <i>5,070,000</i>	5,070,000 <i>5,070,000</i>
2021 Rapid I/I Reduction Program <i>Utility - Wastewater</i>	UT2194CIP	1					3,190,000 <i>3,190,000</i>	3,190,000 <i>3,190,000</i>

Department	Project#	Priority	2017	2018	2019	2020	2021	Total
Lower Yankee Tank Capacity <i>Utility - Wastewater</i>	UT2195CIP	1					2,850,000	2,850,000
							2,850,000	2,850,000
KR-5C 12" Relief Sewer <i>Utility - Wastewater</i>	UT2196CIP	1					1,140,000	1,140,000
							1,140,000	1,140,000
2021 Clinton WTP Improvement Program <i>Utility - Water</i>	UT2197CIP	1					450,000	450,000
							450,000	450,000
2021 Kaw WTP Improvement Program <i>Utility - Water</i>	UT2198CIP	1					450,000	450,000
							450,000	450,000
2021 Watermain Replacement/Relocation Program <i>Utility - Water</i>	UT2199CIP	1					4,720,000	4,720,000
							4,720,000	4,720,000
Utilities Total			42,236,620	27,140,000	29,630,000	27,650,000	28,170,000	154,826,620
GRAND TOTAL			107,534,720	70,012,840	57,645,397	48,583,175	44,676,678	328,452,810

Memorandum
City of Lawrence
Douglas County
Planning & Development Services

TO: Planning Commission

FROM: Mary Miller, Planner

Date: July 15, 2016

RE: Item No. 2: Z-16-00147 – GPI to IG, 7.7 acres located at 711 E 23rd Street

Attachments:

Attachment A: Traffic Impact Study

Attachment B: June Planning Commission Staff Report

Staff Recommendation:

Staff recommends the approval of the rezoning request from GPI (General Public and Institutional Use) to IG (General Industrial), for approximately 7.7 acres located at 711 E 23rd Street based on the findings listed in the June staff report and the additional information provided by the First Student Management and the Traffic Impact Study.

Background

The Planning Commission held a public hearing on the rezoning request for 711 E 23rd Street/K10 Highway at their June meeting and deferred action to the July meeting. The Commission directed the applicant to provide a Traffic Impact Study evaluating the impact of the proposed use *Fleet Storage* on the adjacent street network and nearby residential neighborhood. Two members of the public spoke at the Commission meeting with concerns regarding the safety of the proposed location's access to E 23rd Street/K10 and the possible impact on nearby residential properties. The Commission also directed staff to expand the notification area to include the residential properties near Learnard Street. The notification area was expanded from 200 ft to 1,000 ft from the subject property boundaries to include this area.

The applicant provided a Traffic Impact Study which is included with this memo as Attachment A. The following is a discussion of the study and the recommendations.

TRAFFIC IMPACT STUDY JULY 8, 2016

The study notes that the site is bordered on the north by two streets: E 23rd Street/K10 Highway, an arterial, and N Perimeter Road, a local street. The study identifies a portion of N Perimeter Road as a private street along the north side of Haskell Indian National University.

The study noted that the transportation provider, First Student, has a fleet of 88 buses of which 78 are in routine service with 10 buses being kept as spares when buses need maintenance. Approximately one-half the fleet are 40 ft long and the other are the smaller buses. First Student has a policy which restricts bus movements across E 23rd Street/K10. At the current location buses may only enter and exit the site via a westbound right-turn.

The traffic study looked at several scenarios (Page 3-4) but concluded by recommending Scenario # 1. (Figure 1) Scenario # 1 would use the access on to E 23rd Street/K10 Highway and continue the policy of restricting bus movements to right-in and right-out movements. This prevents the buses from making left turns across E 23rd Street/ K10 Highway. The study states that "...*sight distance is sufficient for all vehicles, passenger cars and buses, to make all turning movements from a stopped condition at this access point to/from 23rd Street.*" The City Engineer agreed that Scenario #1 would be the best option.

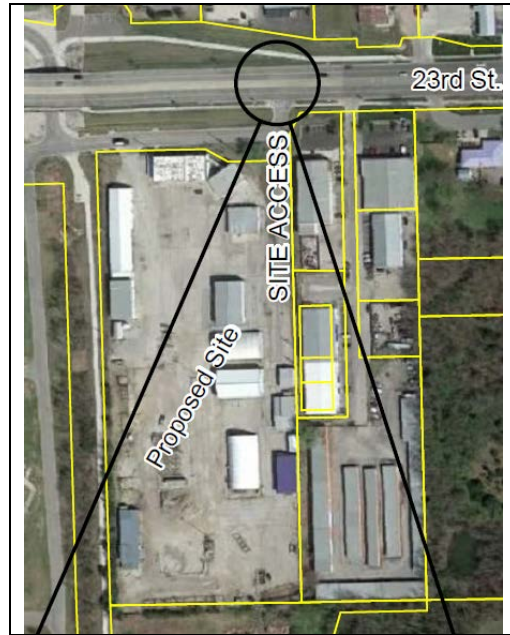


Figure 1. Scenario # 1 site access.

The TIS study recommends a scenario that provides a safe ingress/egress for the school buses and addresses the concerns of the Barker Neighborhood regarding bus traffic through the residential neighborhood.

BUS SYSTEM ROUTING:

The following information was provided by Wayne Zachary, Branch Manager First Student regarding their routing policy for the school buses:

"Our egress/ingress plan for the proposed facility would have our buses exit the facility onto 23rd Street Eastbound. From there buses to the South, West, and Northwest areas would utilize Haskell Avenue to either the new SLT or 31st Street. Buses going to the North or central parts of town would utilize Haskell Avenue to the North and utilize 19th, 15th, or 11th Streets. Bus drivers would be instructed as part of our return policy to utilize these routes for return to the lot.

We do not plan on any left turns onto or off of 23rd Street entering or exiting the lot. Louisiana Street and Massachusetts Street would both be alternative routes for return to the lot.

There is the possibility of some buses being routed for pickups, primarily for Special Needs students in the Barker Neighborhood areas. There may also be right turns off of 23rd Street onto Learnard to access the frontage road. However, any use of Learnard or Barker Avenue will be limited and minimal."

CONCLUSION

The Traffic Impact Study recommends a scenario in which there would be no buses making left turns across E 23rd Street; all buses would make right turns when exiting and entering the facility. The First Student management indicated that neither Learnard Avenue nor Barker Avenue would be utilized as a route to and from the facility but they may be used for pick-ups in the neighborhood. Based on this information, and the additional review that will occur with the site planning of the property, the rezoning of the property from GPI to IG should have minimal impact on the neighborhood.

PLANNING COMMISSION REPORT
Regular Agenda – Public Hearing Item

PC Staff Report
06/20/16

ITEM NO. 2: GPI TO IG 7.7 ACRES; 711 E 23RD STREET (MKM)

Z-16-00147: Consider a request to rezone approximately 7.7 acres located at 711 E 23rd Street from GPI (General Public and Institutional Uses) District to IG (General Industrial) District. Submitted by Lawrence Public Schools USD #497 and BG Consultants, Inc. for Douglas County, the property owner of record.

STAFF RECOMMENDATION: Staff recommends approval of the rezoning request for approximately 7.7 acres from GPI (General Public and Institutional Uses) District to IG (General Industrial) District and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report.

REASON FOR REQUEST

Applicant's Response:

"Lawrence Public Schools is in the process of purchasing this property from the County. They would like to have the option of storing fleet vehicles on the site. That use requires IG zoning. The new County shops were built in the IG zoning."

KEY POINTS

- The previous uses, the DG County Public Works Facility and the Household Hazardous Waste Facility, were installed on the site when the property was zoned M-2 (General Industrial) District. The property was rezoned to the GPI (General Public and Institutional Uses) District with the adoption of the 2006 Development Code.
- The School District plans to use this site for their Operations and Facilities and as a parking area for the school busses. The parking of the school busses is classified in the Development Code as *Fleet Storage*. *Fleet Storage* is not an allowed use in the GPI District but is permitted in commercial and industrial districts. The overall nature of the previous use and the proposed use, Lawrence Public Schools Facilities and Operations Headquarters, are industrial in nature; therefore, industrial zoning is appropriate.
- The property is not located within the boundaries of a registered neighborhood association but was included in the planning boundary for the Burroughs Creek Corridor Plan.

OTHER ACTION REQUIRED

- City Commission approval of rezoning request and adoption/publication of ordinance.
- Submittal and approval of a site plan application for any proposed site improvements.
- Submittal of construction plans to Development Services for processing of building permits. Building Permit must be obtained prior to construction activity.

PUBLIC COMMENT

The owner of the adjacent property to the east inquired about the future use and provided an email to Planning noting that the School District would be a good neighbor and he had no concerns with the rezoning which would allow them to park school busses on their property.

Project Summary

The property at 711 E 23rd Street contains approximately 7.7 acres and was previously used by Douglas County as their Public Works Facility and the Household Hazardous Waste Facility. The Public Works Facility and the Household Hazardous Waste Collection Facility were recently relocated and the property has been unused since that time. The School District Facilities and Operations would be relocated to the subject site from the current location at 146 Maine Street. (Figure 1) The current site has access on W 2nd Street, which is classified as a Local Street. The proposed site has access on E 23rd Street / K10 Hwy, a Principal Arterial. The use is very similar to the former use; however, the school district also



Figure 1. Current USD # 497 Facilities and Operation site.

plans on moving the bus system to the property. This use, *Fleet Storage*, is not permitted in the GPI District; therefore, rezoning to an industrial district has been requested.

REVIEW & DECISION-MAKING CRITERIA

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response:

"The request is in compliance with the Comprehensive Plan."

Recommendations in *Horizon 2020* are discussed below, with staff comments in red.

The property is located within the boundaries of the Burroughs Creek Corridor. The Comprehensive Plan recommends that future development of this area be in accordance with the Burroughs Creek Corridor Plan. (Page 7-4, *Horizon 2020*)

The recommendations of the Burroughs Creek Corridor Plan will be discussed in Section 4 of this report.

Goal 1: Development in Established Industrial and Employment-Related Areas:

Encourage the retention, redevelopment and expansion of established industrial and employment-related areas.

Policy 1.1: Retain Established Development and Encourage New Development in Existing Industrial and Employment-Related Areas (Page 7-11, *Horizon 2020*)

The proposed rezoning will accommodate the reuse of the facility which is located within an Industrial and Employment-Related Area, (Figures 2a and 2b)

Staff Finding – The proposed rezoning and reuse of the facility is compliant with recommendations in *Horizon 2020*.

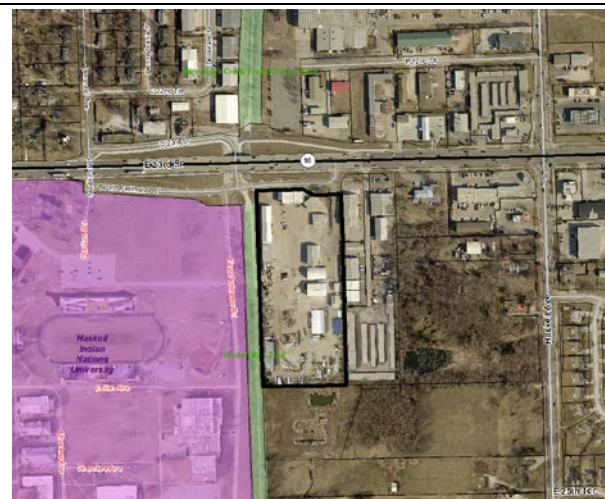
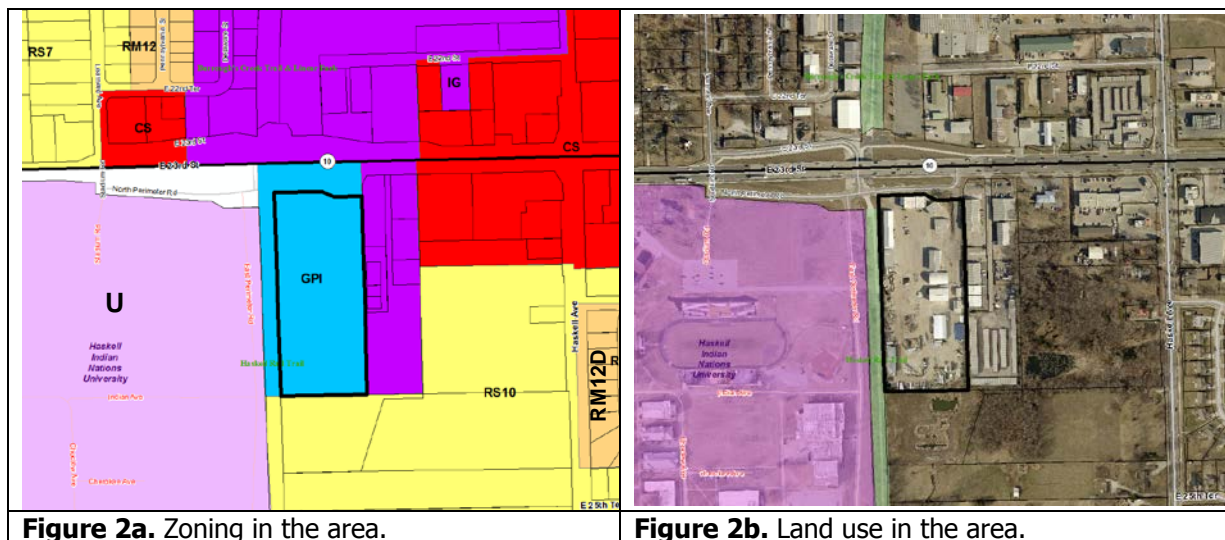
2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING ANY OVERLAY ZONING

Current Zoning and Land Use: GPI (General Public and Institutional Uses) District; former Douglas County Public Works Facility and Household Hazardous Waste Facility, uses included: *Exterior Storage, Light and Heavy Equipment Repair, Recycling Processing Facility*, and Accessory Office.

Surrounding Zoning and Land Use:

- To the north:
IG (General Industrial) District; *Construction Sales and Services*
- To the west:
U (University-Haskell Indian Nations University) District; University
- To the east:
IG (General Industrial) District; *Manufacturing and Production, Limited; Mini-Warehouse*
- To the south:
RS10 (Single-Dwelling Residential) District; property is owned by City of Lawrence and designed to filter stormwater runoff from the facility before it discharges into the wetlands to the south.

(Figure 2)



Staff Finding – The area contains a mix of zonings and land uses. The subject property is part of the industrial Burroughs Creek Corridor and is surrounded on the east and north by property that is zoned and used for industrial land uses, on the west by Burroughs Creek Trail, a linear park and beyond that, the Haskell Indian Nations University. The property to the south is zoned residential, but is used as stormwater management for the subject

property. The proposed zoning is compatible with the existing zoning and land uses in the area.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response:

"The property fronts on K-10 highway and has IG zoning on the north and east sides. Haskell University is located to the west along with the Burroughs Rail Trail. The south portion of the property is adjacent to an undeveloped area that is currently zoned RS10."

The term 'neighborhood' in this case refers to the area surrounding the subject property. For the purpose of this evaluation the neighborhood is bounded generally by Massachusetts Street on the west and Silicon Avenue on the east, E 27th Street on the south and E 20th Street (extended) on the north. (Figure 3)

This area is divided north and south by E 23rd Street / K10 Highway, a principal arterial, and east and west by the Burroughs Creek Trail, a multi-use path for pedestrians and bicyclists. Haskell Indian Nations University is a principal feature of the area.

Land Uses listed with the Douglas County Appraiser's Office are mapped in Figure 4. The principal land use west of Learnard Street is residential. Residential uses to the east of Learnard Avenue are set back from E 23rd Street/K10 Highway. Industrial uses border the Burroughs Creek Trail and a mix of industrial and commercial uses border East 23rd Street/K10 Highway, east of Learnard.

Staff Finding – This is a mixed use neighborhood which has the Haskell Indian Nations University as a key feature. Residential, industrial, and commercial uses are the other predominate uses in the area. Residential uses are located primarily in the west and southeast portion of the neighborhood and commercial uses are concentrated along 23rd Street/ K10 Highway east of the corridor. Industrial uses are located along the Burroughs Creek Corridor and are interspersed with the commercial uses. Rezoning to the IG District will maintain the industrial land use of the property and maintain the character of the neighborhood.

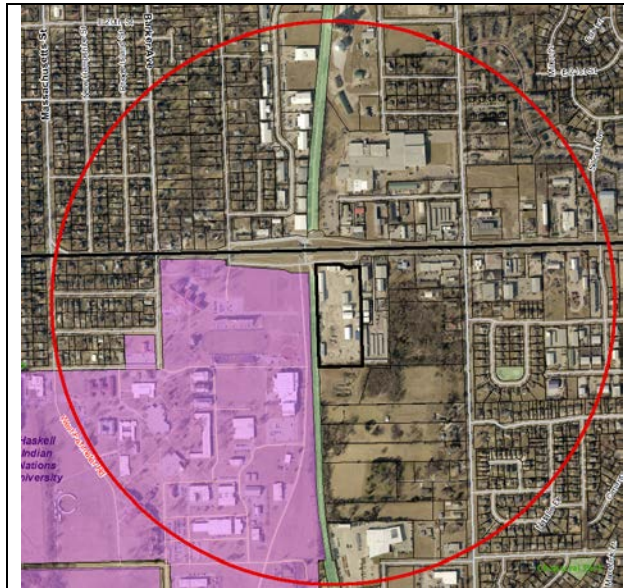


Figure 3. Neighborhood Area

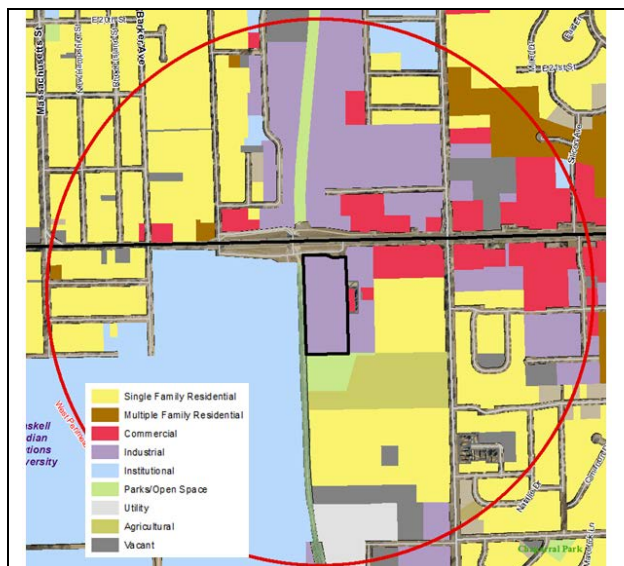


Figure 4. Land Uses in the area, per the DG County Appraisers records.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

The property is located within the boundaries of the Burroughs Creek Corridor Plan. This plan was requested by the Old East Lawrence, Barker, and Brook Creek Neighborhood Associations. The plan notes that the neighborhoods would like to emphasize residential infill and neighborhood friendly redevelopment of industrial and commercial areas that are presently underutilized. A large part of the redevelopment they would like to see is the transition of the railroad right-of-way to a linear park and recreation trail. The plan does not provide specific land use recommendations for properties in the planning area but recommends that when redevelopment of industrially zoned areas is proposed, these areas should be scrutinized for neighborhood compatibility. (Page 14, *Burroughs Creek Corridor Plan*) Any redevelopment of the site will require site plan approval and the plans will be reviewed for compatibility with nearby uses and the character of the neighborhood. The *Burrough Creek Corridor Plan* listed properties that were recommended for rezoning to residential, commercial, or office uses. The subject property was not included in that list.

Staff Finding – The rezoning will allow for the reuse of the former Douglas County Public Works Facility site. The *Burroughs Creek Corridor Plan* stressed neighborhood compatibility with the redevelopment of industrial sites. As recommended by the plan, any site plan submitted for changes to the site will include a review for neighborhood compatibility and may include additional screening or buffering if necessary.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response:

"The existing zoning does not permit the future owner from utilizing the space to provide fleet parking for buses."

The property is well suited to many of the uses of an industrial nature which are permitted in the GPI District, based on its current development. While the individual uses included in the Public Works Facility were allowed in the GPI District, the intent of the District is to accommodate institutional uses. Rezoning to the IG District will allow a broader spectrum of industrial uses on the property and will remove the institutional uses such as *College, Cultural Center, Lodge, School, Community Mental Health Facility, Health Care Office, and Outpatient Care Facility* that the property, as developed, is not suited for.

Staff Finding – The property has been developed for uses of an industrial nature. Many of these uses are permitted in the current GPI Zoning. The property, as developed, is not appropriate for the various institutional and community facility uses that are permitted in the GPI District; but is well suited for the uses which are permitted in the IG District.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response:

"Approximately 2 years."

Staff Finding – The Douglas County Public Works Facility and the Household Hazardous Waste Facility were relocated in 2014. Most of the equipment and materials were removed by August, 2015.

7. EXTENT TO WHICH REMOVAL OF RESTRICTIONS WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicants Response:

"The rezoning will not detrimentally affect nearby properties. The proposed use will be similar to the former use and the rezoning will allow the transfer of property to USD 497 which will keep the property from sitting vacant."

The removal of restrictions that would occur with this rezoning will allow the property to be used in a similar fashion as before with the additional use of *Fleet Storage*. The addition of this use will allow the school bus system to be relocated to the site. The addition of the bus system will increase the traffic load on this portion of E 23rd Street / K10 Highway at certain times of the day. The busses currently access E 23rd Street/K10 Highway near the intersection with Harper Street. When the facility is site planned, any impact the additional traffic would have at this location will be evaluated through the review of a Traffic Impact Study. If the traffic was found to be significant enough to have a negative impact, mitigation measures, if necessary, would be determined based on the study. This review at the site plan stage will insure the additional traffic at this location does not have a negative impact on nearby properties.

The site is screened from Haskell Indian Nations University to the west by the Burroughs Creek Corridor Trail and its bordering vegetation and from properties to the south by the open space parcel that provides stormwater management. (Figure 5) The property to the east is developed in a similar fashion and is compatible with the facility.



Figure 5. Haskell Indian Nations University's athletic field and open space is located to the west of the subject property and other industrial uses are located to the east.

Staff Finding – The removal of the restrictions will allow *Fleet Storage* on the property which could result in additional traffic on this portion of E 23rd Street/K10 Highway. The impact of the additional traffic will be evaluated and mitigated, if necessary, at the site plan review stage through the review of a Traffic Impact Study. The property, as developed, is

compatible with the surrounding properties to the west, south, and east. The removal of restrictions should have no negative impact on nearby properties.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicants Response:

"If the rezoning is approved, the property will be redeveloped to serve as the Lawrence Public Schools Facilities and Operations headquarters. If the rezoning is denied, the property will remain vacant and continue to deteriorate."

If approved, the property would provide one location for the School District Facilities and Operations and their bus system. Facilities and Operations currently operate out of an approximately one acre property at the intersection of W 2nd Street and Maine Street, both local streets. Moving the facility to the proposed location of 7.7 acres will provide more space in a more central location with access to a principal arterial street. In addition, the property currently has shops that were designed for vehicle repair which could be used for servicing the school busses. Approval of the rezoning request would increase the efficiency of the School District operations and provide a centralized location. If the request is denied, the School District would need to maintain the Facilities and Operations and bus storage in their current locations or look for another site to consolidate the facilities.

Staff Finding – Approval of the rezoning request would benefit the public health, safety, and welfare by allowing the reuse of the property for the consolidation of the School District's Facilities and Operations and Bus System. The rezoning would provide a more appropriate location for the Facilities and Operations on a Principal Arterial rather than a Local Street and would result in efficiencies due to the shared use of the property for the Facilities and Operations and Bus System.

PROFESSIONAL STAFF RECOMMENDATION

This staff report reviews the proposed rezoning request for its compliance with the Comprehensive Plan, adopted plans for the area, the Golden Factors, and compatibility with surrounding development.

Staff recommends approval of the rezoning request for approximately 7.7 acres from GPI (General Public and Institutional Use) District to IG (General Industrial) District and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report.

TRAFFIC IMPACT STUDY

for

USD #497 Warehouse and Bus Site



Prepared by: Jason Hoskinson, PE, PTOE

BG Project No. 16-1200L
July 8, 2016

Introduction: This Traffic Impact Study (TIS) has been prepared in accordance with the City of Lawrence Ordinance 7650 as required by the City's Land Development Code. The purpose of the TIS is to identify the basic land use and transportation characteristics of the proposed USD #497 Lawrence Public Schools use of the site at 711 E. 23rd Street.

Item #1: Proposed Development: The proposed site for the USD #497 Warehouse and Bus Site is a property currently owned by Douglas County. The site had been used by the Douglas County Public Works Department for vehicle storage, maintenance and refueling. Douglas County and USD #497 have been discussing the feasibility of exchanging ownership of the property. The USD #497 intends to use the site for similar purposes, including warehouse, maintenance and bus fleet storage. A site plan is currently unavailable as this site is in the process of being rezoned.

Item #2: Horizon 2020 Land Use: The Lawrence/Douglas County Comprehensive Plan identifies the future land use of this site as Office Research and/or Industrial/Warehouse/Distribution (Map 3-2, Horizon 2020). The proposed use is consistent with Comprehensive Plan for this location.

Item #3: Street Functional Classification: The site is bordered by two streets on the north. 23rd Street (K-10) is classified as an arterial street and N. Perimeter Road is classified as a local street (frontage road). A portion of this road is a private street along the north side of Haskell Indian Nations University. T2040 classifies 23rd Street (K-10) as the nearest arterial street to the proposed site.

Item #4: Allowable Access: Two existing access points of access will be retained to the N. Perimeter Road fronting this site. The east end of N. Perimeter Road accesses 23rd Street (K-10) at the east property line of the site.

Item #5: Adjacent Street Characteristics: N. Perimeter Road is the only public street adjacent to the site with an unposted speed limit, implying a statutory speed limit of 30 mph. 23rd Street also borders the north side of the site, serving as the nearest adjacent arterial street. The posted speed limit of 23rd Street is 45 mph and according to the latest traffic data from KDOT, the estimated Annual Average Daily Traffic (AADT) is 30,000 vehicles per day. The AADT may decrease slightly with opening of the South Lawrence Trafficway in several months.

N. Perimeter Road currently accesses 23rd Street as a STOP controlled, T-intersection. 23rd Street is a 5-lane urban street, providing a center two-way-left-turn (TWLTL) for westbound ingress traffic generated by the site. N. Perimeter Road is a 2-lane urban street allowing for 1-lane of travel in each direction along the north side of the property.

Sight distance was measured at the east intersection of 23rd Street and N. Perimeter Road. This is also the approximate location where the Site Access will access the frontage road and will be the primary point of access to 23rd Street for site generated traffic. Sight distance was found to be sufficient. For passenger cars making a left-turn from a stopped condition, a minimum time gap of 8.0 seconds is required. The available time gap in normal traffic flow was consistently measured at approximately 10.0

seconds to the west and well in excess of 10.0 seconds to the east. For bus traffic in particular, only right-turning egress movements are anticipated as will be discussed in further detail below. A minimum time gap for a bus to egress the site making a right turn from a stopped condition is 9.2 seconds. The available time gap for passenger cars of 10.0 seconds exceeds the required time. Furthermore, the actual available time gap for bus and truck traffic with a driver sitting at a higher elevation above the roadway would be greater than the recorded values for passenger cars.

Item #6: Proposed Access: The site currently has two access points to N. Perimeter Road, leading to one primary access point to 23rd Street at the east property line of the site. This area was recently reconstructed as a part of KDOT's replacement of the 23rd Street bridge immediately west of the site. With exception of 23rd Street and its frontage road, no other public streets offering access opportunities are adjacent to the site. Given the geometric and vertical constraints between 23rd Street and the N. Perimeter Road, relocation of the existing intersection to the west is not feasible.

Item #7: Trip Generation: Detailed plans for use of the site have not been developed. With exception of the bus storage/staging, the anticipated use of the site and resulting trips will likely be very similar to the previous use by the Douglas County Public Works Department. For the purposes of this study, the trips generated by bus traffic and their circulation plan options will be discussed in further detail.

Traffic Type and Departure/Arrival Characteristics: First Student serves as the transportation provider for the USD #497. They currently have a fleet of 88 buses. 78 buses are in routine service and 10 buses are spares used only as needed or when buses are taken out of service for maintenance. Approximately one-half of the fleet are 40' long buses and the other one-half of the fleet are smaller buses. For the purpose of this analysis, 80 buses are assumed to be in service on a typical weekday with the type of bus used being split 50% / 50% between long / small buses.

A summary of the peak hours associated with the bus storage of this site is as follows:

- AM Peak Hour (typical weekday)
 - 6:00 am – 7:00 am: cars entering site and buses exiting site.
 - 8:00 am – 9:00 am: buses entering site and cars exiting site.
- PM Peak Hour (typical weekday)
 - 2:00 pm – 3:00 pm: cars entering site and buses exiting site.
 - 4:00 pm – 5:00 pm: buses entering site and cars exiting site.

Site Circulation Plan: First Student currently uses a site on the north side of 23rd Street, approximately 1 mile east of the proposed 711 E. 23rd Street site. The company policy restricts the bus drivers' movements such that they may only enter the site via a westbound right-turn and they may only egress the site via southbound right-turn. No drivers are allowed to make left-turns into or out of the site while driving a school bus.

Several scenarios were explored regarding the access in/out of the proposed site as summarized below.

- Scenario #1: Existing Site Access
 - No turn restrictions for passenger cars in/out of the site.
 - School buses restricted to right-in/right-out only.
- Scenario #2: Existing Site Access
 - No turn restrictions for passenger cars in/out of the site.
 - School buses restricted to right-in and/or left-in.
 - School buses restricted to right-out only.
- Scenario #3: Alternate Site Access using frontage roads
 - No turn restrictions for passenger cars in/out of the site.
 - Consider allowing school buses making right-in movements to use the intersection of 23rd and N. Perimeter Road/Learnard.
 - Consider allowing school buses needing to go westbound on 23rd Street to use the frontage roads to cross 23rd Street to get to the intersection of 23rd and Learnard to make a right-turn movement.

Scenario #1: In Scenario #1, employees will arrive between 6:00 am and 6:30 am in passenger vehicles and then drive the buses out of the site between 6:30 am and 7:00 am. The buses will remain on their routes until mid-morning when all students are dropped off at the various USD #497 schools, returning to the site between 8:00 am and 9:00 pm.

The PM peak hours of the site will operate in a similar fashion with the peak demand of employees arriving to the site between 2:00 pm and 2:30 pm followed by buses exiting the site between 2:30 pm and 3:00 pm. Upon completion of their routes, the buses will return between 4:00 pm and 4:30 pm followed by employees leaving the site to go home between 4:30 pm and 5:00 pm.

For Scenario #1, First Student would be required to continue their policy of restricting bus movements to right-out egress and right-in ingress. Exhibit #1 shows the estimated volume and pattern of traffic movements. As previously noted, sight distance is sufficient for all vehicles, passenger cars and buses, to make all turning movements from a stopped condition at this access point to/from 23rd Street.

Scenario #2: Scenario #2 is identical to Scenario #1 with the exception of allowing left-ingress movements to the site from 23rd Street. First Student's policy will allow a bus to make a left-turn across a street such as 23rd Street if a dedicated left-turn lane is provided. A TWLTL currently exists to the east of the Site Access and the median is painted with yellow gore markings to the west of the Site Access.

Exhibit #2 shows the estimated volume and pattern of traffic movements. Again, sight distance is sufficient for all vehicles, passenger cars and buses, to make all turning movements from a stopped condition at this access point to/from 23rd Street in Scenario #2.

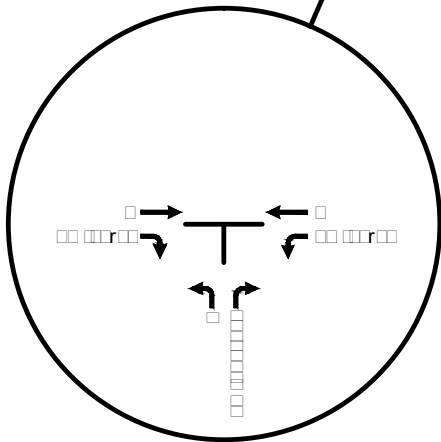
If First Student desires to pursue Scenario #2, the travel pattern policy would have to be modified to allow the buses to make a left-turn from the TWLTL at the Site Access. Restriping the west end of the TWLTL to provide a dedicated left-turn lane into the proposed site is not feasible due to existing access points on the north side of 23rd Street beginning 150 feet east of the proposed Site Access.

Scenario #3: Scenario #3 explores several options for ingress/egress utilizing the existing frontage roads. One option considered under this scenario would allow buses to utilize this intersection for right-turns in and out of the site. The available sight distance is somewhat greater at this location, measured at approximately 12 seconds to the west as compared to 10 seconds at the proposed Site Access, and excellent to the east for passenger cars. However, a portion of the south frontage road, known as N. Perimeter Road, is a private street and an agreement would likely need to be made between First Student and the US Government to utilize this street across the frontage of Haskell Indian Nations University for bus traffic. The buses would also be required to cross the existing hike/bike trail at grade located at the west property line of the proposed site.

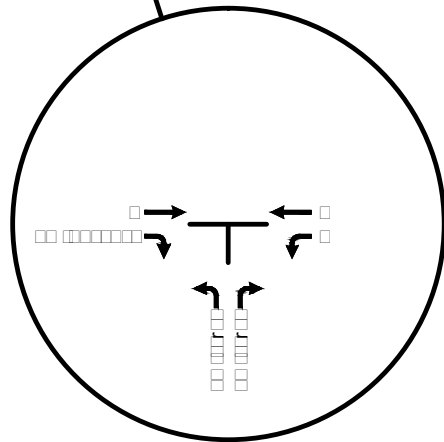
Another option considered in Scenario #3 would allow bus traffic ultimately wanting to go westbound on 23rd Street to utilize the frontage roads to cross under 23rd Street, ultimately accessing Learnard Avenue and using Learnard Avenue to access westbound 23rd Street via right-turn. However, the intersections of the North Frontage Road/Learnard Avenue and 23rd Street/Learnard Avenue are in such close proximity to each other that a bus would be unable to make the necessary turning movements without blocking Learnard Avenue traffic and potentially causing northbound Learnard Avenue traffic queues to spill back onto 23rd Street.

Exhibit #3 highlights the bus travel pattern options explored in Scenario #3. Due to the geometric limitations of the existing roadway and the relatively minor increase in available sight distance, we do not recommend pursuing any of the options considered in Scenario #3.

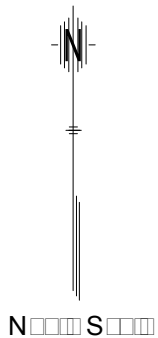
Summary: The proposed reuse of the existing site at 711 E. 23rd Street is anticipated to have similar impacts on the existing public street system as was experienced by the past user (Douglas County Public Works). Based on the analysis summarized in this report, we recommend First Student and the USD #497 pursue implementation of Scenario #1 and continue the policy of requiring right-in and right-out only movements for bus traffic.

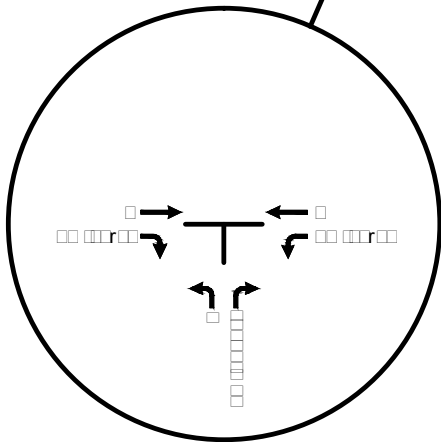


AM P...H...r
PM P...H...r

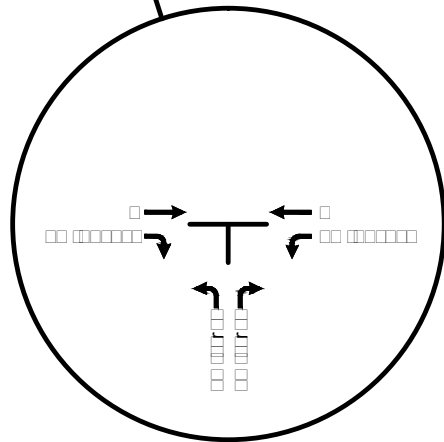


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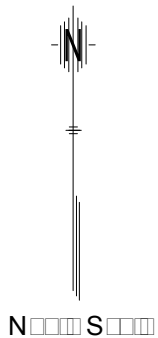


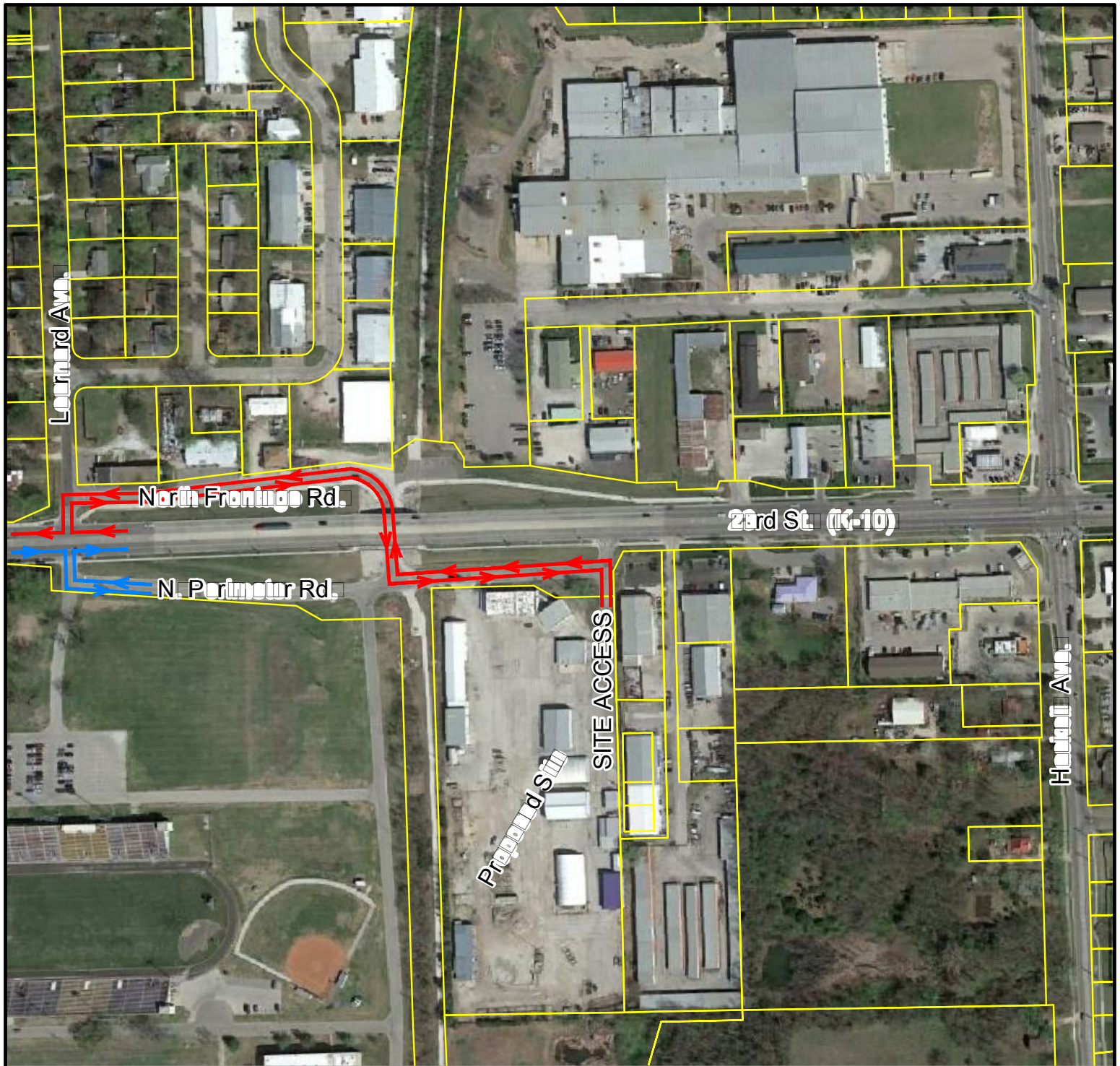


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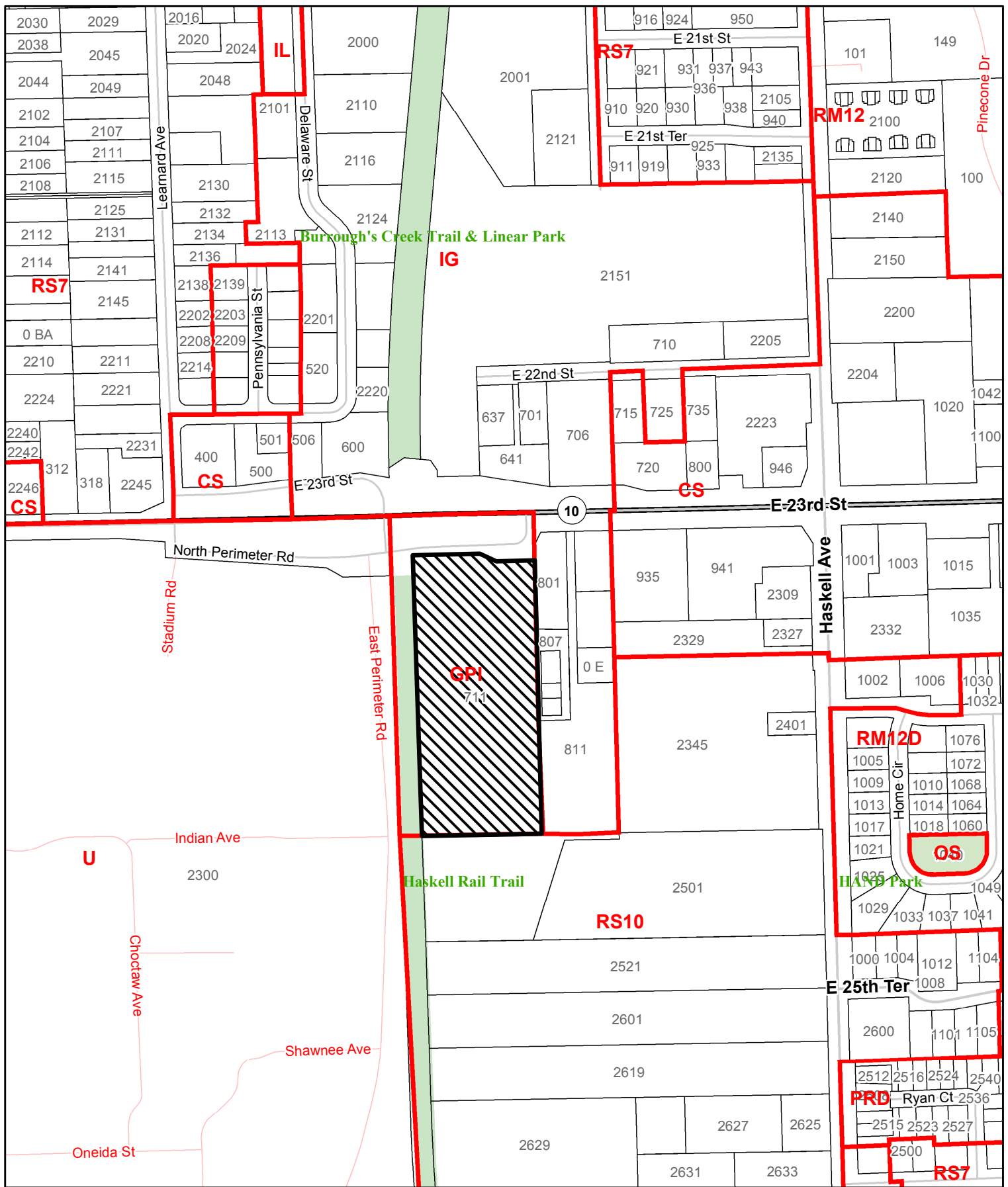


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N S



**Z-16-00147: Rezone Approximately 7.7 acres from GPI District to IG District
Located at 711 E. 23rd Street**

Lawrence-Douglas County Planning Office
June 2016

 Subject Property



June 20, 2016

Concerns with existing PC Staff Report Z-16-00147:

1. Re: Criterion 2 - "Zoning and Use of Nearby Property"

Barker Neighborhood is absent from the description of surrounding zoning and land use to the north and west but should be included.

2. Re: Criterion 3 - "Character of the Neighborhood"

Figures 3 and 4 illustrate that nearly $\frac{1}{4}$ of the nearby property is residential, a wedge from Mass to 19th Street. This proportion should not be excluded from the text description.

3. Re: Criterion 7 - "Extent to Which Removal of Restrictions Will Detrimentially Affect Nearby Properties"

Staff finding states that "The property, as developed, is compatible with the surrounding properties to the west, south, and east. The removal of restrictions should have no negative impact on nearby properties."

4. Re: Criterion 4 - "Plans for Area or Neighborhood..."

Neighborhood compatibility recommendations in the Burroughs Creek Corridor Plan cannot be satisfied without consideration of residential properties to the north.

5. Re: Project Summary :

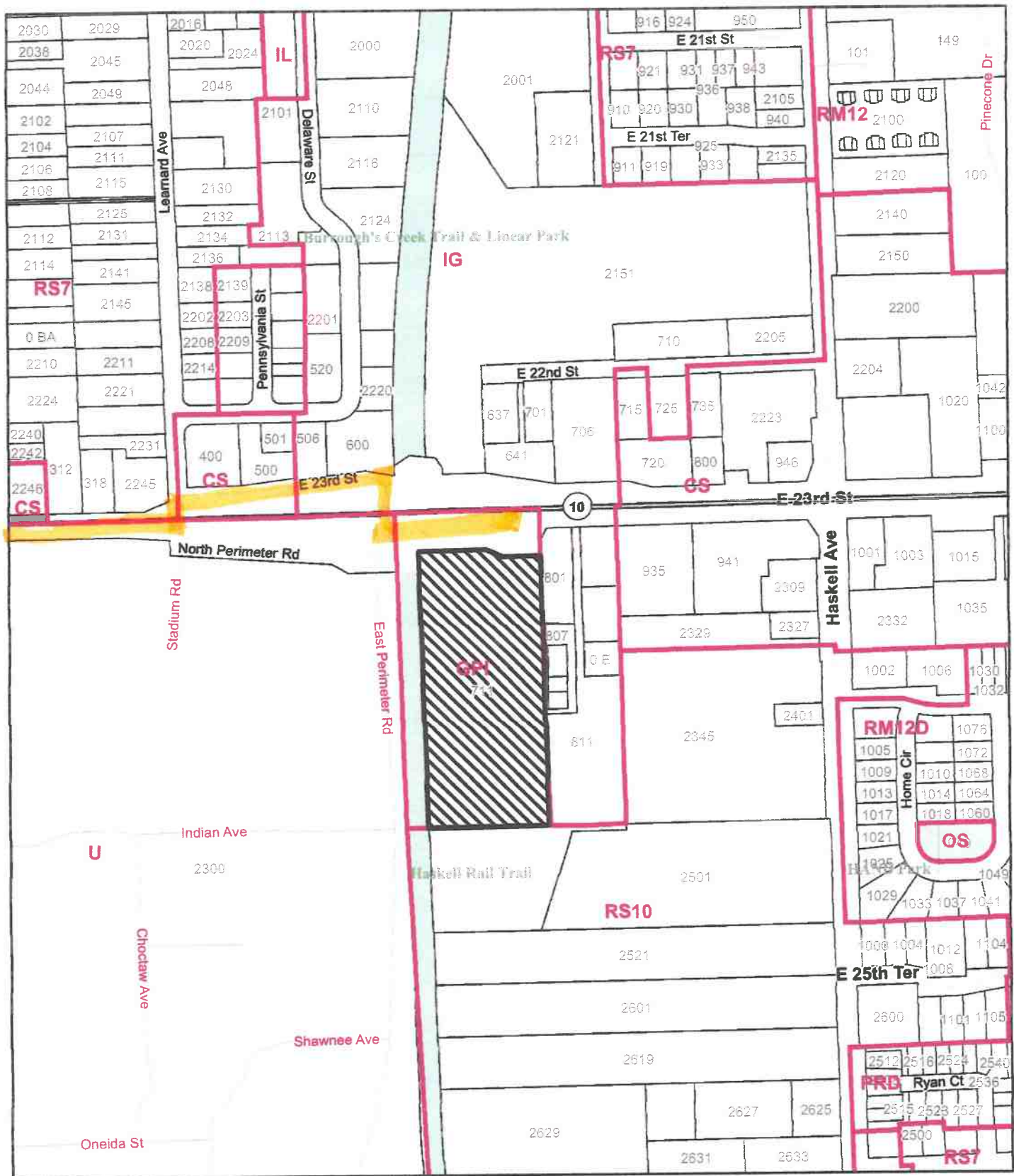
The only route westbound without making a left across traffic on East 23rd street from the property at 711 E 23rd runs below 23rd Street on perimeter road and then on to Learnard Ave and then onto 23rd Street.

I would appreciate any follow up you could provide regarding these concerns. Thank you for your consideration.

Respectfully,



Marsha Heeb
2134 Learnard Ave



**Z-16-00147: Rezone Approximately 7.7 acres from GPI District to IG District
Located at 711 E. 23rd Street**

Lawrence-Douglas County Planning Office
June 2016

 Subject Property



PUBLIC COMMENT

The owner of the adjacent property to the east inquired about the future use and provided an email to Planning noting that the School District would be a good neighbor and he had no concerns with the rezoning which would allow them to park school busses on their property.

Project Summary

The property at 711 E 23rd Street contains approximately 7.7 acres and was previously used by Douglas County as their Public Works Facility and the Household Hazardous Waste Facility. The Public Works Facility and the Household Hazardous Waste Collection Facility were recently relocated and the property has been unused since that time. The School District Facilities and Operations would be relocated to the subject site from the current location at 146 Maine Street. (Figure 1) The current site has access on W 2nd Street, which is classified as a Local Street. The proposed site has access on E 23rd Street / K10 Hwy, a Principal Arterial. The use is very similar to the former use; however, the school district also plans on moving the bus system to the property. This use, *Fleet Storage*, is not permitted in the GPI District; therefore, rezoning to an industrial district has been requested.



Figure 1. Current USD # 497 Facilities and Operation site.

REVIEW & DECISION-MAKING CRITERIA

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response:

"The request is in compliance with the Comprehensive Plan."

Recommendations in *Horizon 2020* are discussed below, with staff comments in red.

The property is located within the boundaries of the Burroughs Creek Corridor. The Comprehensive Plan recommends that future development of this area be in accordance with the Burroughs Creek Corridor Plan. (Page 7-4, *Horizon 2020*)

The recommendations of the Burroughs Creek Corridor Plan will be discussed in Section 4 of this report.

Goal 1: Development in Established Industrial and Employment-Related Areas:

Encourage the retention, redevelopment and expansion of established industrial and employment-related areas.

Policy 1.1: Retain Established Development and Encourage New Development in Existing Industrial and Employment-Related Areas (Page 7-11, *Horizon 2020*)

The proposed rezoning will accommodate the reuse of the facility which is located within an Industrial and Employment-Related Area, (Figures 2a and 2b)

Staff Finding – The proposed rezoning and reuse of the facility is compliant with recommendations in *Horizon 2020*.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING ANY OVERLAY ZONING

Current Zoning and Land Use: GPI (General Public and Institutional Uses) District; former Douglas County Public Works Facility and Household Hazardous Waste Facility, uses included: *Exterior Storage, Light and Heavy Equipment Repair, Recycling Processing Facility, and Accessory Office.*

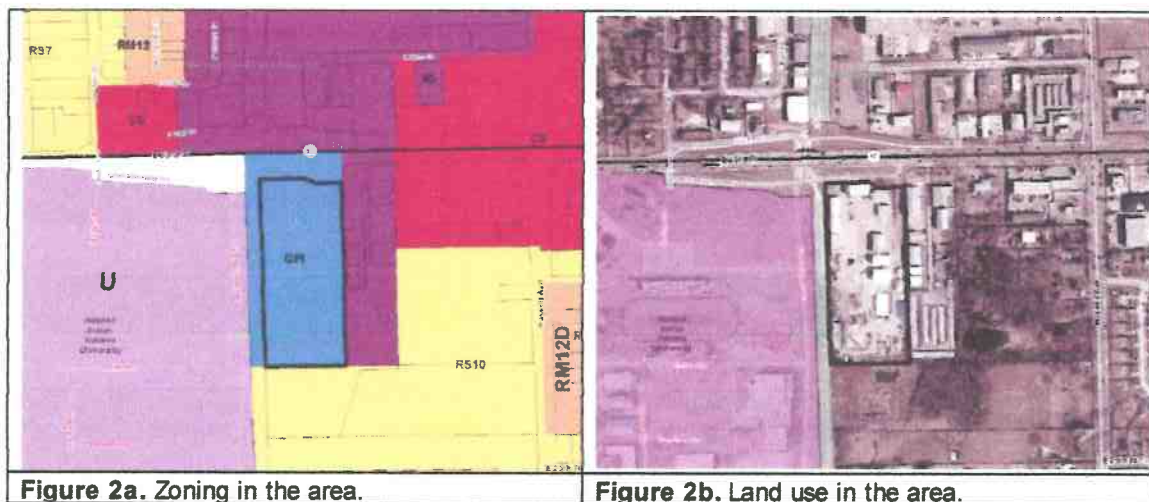
Surrounding Zoning and Land Use: To the north:
IG (General Industrial) District; *Construction Sales and Services*

To the west:
U (University-Haskell Indian Nations University) District; University

To the east:
IG (General Industrial) District; *Manufacturing and Production, Limited; Mini-Warehouse*

To the south:
RS10 (Single-Dwelling Residential) District; property is owned by City of Lawrence and designed to filter stormwater runoff from the facility before it discharges into the wetlands to the south.

(Figure 2)



Staff Finding – The area contains a mix of zonings and land uses. The subject property is part of the industrial Burroughs Creek Corridor and is surrounded on the east and north by property that is zoned and used for industrial land uses, on the west by Burroughs Creek Trail, a linear park and beyond that, the Haskell Indian Nations University. The property to the south is zoned residential, but is used as stormwater management for the subject

property. The proposed zoning is compatible with the existing zoning and land uses in the area.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response:

"The property fronts on K-10 highway and has IG zoning on the north and east sides. Haskell University is located to the west along with the Burroughs Rail Trail. The south portion of the property is adjacent to an undeveloped area that is currently zoned RS10."

The term 'neighborhood' in this case refers to the area surrounding the subject property. For the purpose of this evaluation the neighborhood is bounded generally by Massachusetts Street on the west and Silicon Avenue on the east, E 27th Street on the south and E 20th Street (extended) on the north. (Figure 3)

This area is divided north and south by E 23rd Street / K10 Highway, a principal arterial, and east and west by the Burroughs Creek Trail, a multi-use path for pedestrians and bicyclists. Haskell Indian Nations University is a principal feature of the area.

Land Uses listed with the Douglas County Appraiser's Office are mapped in Figure 4. The principal land use west of Learnard Street is residential. Residential uses to the east of Learnard Avenue are set back from E 23rd Street/K10 Highway. Industrial uses border the Burroughs Creek Trail and a mix of industrial and commercial uses border East 23rd Street/K10 Highway, east of Learnard.

Staff Finding – This is a mixed use neighborhood which has the Haskell Indian Nations University as a key feature. Residential, industrial, and commercial uses are the other predominate uses in the area. Residential uses are located primarily in the west and southeast portion of the neighborhood and commercial uses are concentrated along 23rd Street/ K10 Highway east of the corridor. Industrial uses are located along the Burroughs Creek Corridor and are interspersed with the commercial uses. Rezoning to the IG District will maintain the industrial land use of the property and maintain the character of the neighborhood.



Figure 3. Neighborhood Area



Figure 4. Land Uses in the area, per the DG County Appraisers records.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

The property is located within the boundaries of the Burroughs Creek Corridor Plan. This plan was requested by the Old East Lawrence, Barker, and Brook Creek Neighborhood Associations. The plan notes that the neighborhoods would like to emphasize residential infill and neighborhood friendly redevelopment of industrial and commercial areas that are presently underutilized. A large part of the redevelopment they would like to see is the transition of the railroad right-of-way to a linear park and recreation trail. The plan does not provide specific land use recommendations for properties in the planning area but recommends that when redevelopment of industrially zoned areas is proposed, these areas should be scrutinized for neighborhood compatibility. (Page 14, *Burroughs Creek Corridor Plan*) Any redevelopment of the site will require site plan approval and the plans will be reviewed for compatibility with nearby uses and the character of the neighborhood. The *Burrough Creek Corridor Plan* listed properties that were recommended for rezoning to residential, commercial, or office uses. The subject property was not included in that list.

Staff Finding – The rezoning will allow for the reuse of the former Douglas County Public Works Facility site. The *Burroughs Creek Corridor Plan* stressed neighborhood compatibility with the redevelopment of industrial sites. As recommended by the plan, any site plan submitted for changes to the site will include a review for neighborhood compatibility and may include additional screening or buffering if necessary.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response:

"The existing zoning does not permit the future owner from utilizing the space to provide fleet parking for buses."

The property is well suited to many of the uses of an industrial nature which are permitted in the GPI District, based on its current development. While the individual uses included in the Public Works Facility were allowed in the GPI District, the intent of the District is to accommodate institutional uses. Rezoning to the IG District will allow a broader spectrum of industrial uses on the property and will remove the institutional uses such as *College, Cultural Center, Lodge, School, Community Mental Health Facility, Health Care Office, and Outpatient Care Facility* that the property, as developed, is not suited for.

Staff Finding – The property has been developed for uses of an industrial nature. Many of these uses are permitted in the current GPI Zoning. The property, as developed, is not appropriate for the various institutional and community facility uses that are permitted in the GPI District; but is well suited for the uses which are permitted in the IG District.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response:

"Approximately 2 years."

Staff Finding – The Douglas County Public Works Facility and the Household Hazardous Waste Facility were relocated in 2014. Most of the equipment and materials were removed by August, 2015.

7. EXTENT TO WHICH REMOVAL OF RESTRICTIONS WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicants Response:

"The rezoning will not detrimentally affect nearby properties. The proposed use will be similar to the former use and the rezoning will allow the transfer of property to USD 497 which will keep the property from sitting vacant."

The removal of restrictions that would occur with this rezoning will allow the property to be used in a similar fashion as before with the additional use of *Fleet Storage*. The addition of this use will allow the school bus system to be relocated to the site. The addition of the bus system will increase the traffic load on this portion of E 23rd Street / K10 Highway at certain times of the day. The busses currently access E 23rd Street/K10 Highway near the intersection with Harper Street. When the facility is site planned, any impact the additional traffic would have at this location will be evaluated through the review of a Traffic Impact Study. If the traffic was found to be significant enough to have a negative impact, mitigation measures, if necessary, would be determined based on the study. This review at the site plan stage will insure the additional traffic at this location does not have a negative impact on nearby properties.

The site is screened from Haskell Indian Nations University to the west by the Burroughs Creek Corridor Trail and its bordering vegetation and from properties to the south by the open space parcel that provides stormwater management. (Figure 5) The property to the east is developed in a similar fashion and is compatible with the facility.



Figure 5. Haskell Indian Nations University's athletic field and open space is located to the west of the subject property and other industrial uses are located to the east.

Staff Finding – The removal of the restrictions will allow *Fleet Storage* on the property which could result in additional traffic on this portion of E 23rd Street/K10 Highway. The impact of the additional traffic will be evaluated and mitigated, if necessary, at the site plan review stage through the review of a Traffic Impact Study. The property, as developed, is

compatible with the surrounding properties to the west, south, and east. The removal of restrictions should have no negative impact on nearby properties.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicants Response:

"If the rezoning is approved, the property will be redeveloped to serve as the Lawrence Public Schools Facilities and Operations headquarters. If the rezoning is denied, the property will remain vacant and continue to deteriorate."

If approved, the property would provide one location for the School District Facilities and Operations and their bus system. Facilities and Operations currently operate out of an approximately one acre property at the intersection of W 2nd Street and Maine Street, both local streets. Moving the facility to the proposed location of 7.7 acres will provide more space in a more central location with access to a principal arterial street. In addition, the property currently has shops that were designed for vehicle repair which could be used for servicing the school busses. Approval of the rezoning request would increase the efficiency of the School District operations and provide a centralized location. If the request is denied, the School District would need to maintain the Facilities and Operations and bus storage in their current locations or look for another site to consolidate the facilities.

Staff Finding – Approval of the rezoning request would benefit the public health, safety, and welfare by allowing the reuse of the property for the consolidation of the School District's Facilities and Operations and Bus System. The rezoning would provide a more appropriate location for the Facilities and Operations on a Principal Arterial rather than a Local Street and would result in efficiencies due to the shared use of the property for the Facilities and Operations and Bus System.

PROFESSIONAL STAFF RECOMMENDATION

This staff report reviews the proposed rezoning request for its compliance with the Comprehensive Plan, adopted plans for the area, the Golden Factors, and compatibility with surrounding development.

Staff recommends approval of the rezoning request for approximately 7.7 acres from GPI (General Public and Institutional Use) District to IG (General Industrial) District and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report.

July 21, 2016

Dear Lawrence Planning Commission:

I would like to register my concern about the proposed rezoning of 711 E 23rd to allow relocation of the School District 497 bus fleet and the associated Traffic Impact Study . I have lived at 2134 Learnard Ave for 20 years and use the area under discussion to access 23rd Street.

1. The area of 23rd St. between Mass and Haskell is listed as one of the Top 5 High Volume Roadway Segments in Lawrence by Transportation 2040. Staging all of 497's buses from this location at peak traffic times would be difficult at best and dangerous at worst.
2. There is no direct access from 711 E 23rd to 23rd Street. The driveway doesn't open onto 23rd. It merges with Perimeter Road and proceeds to a stop sign on 23rd St.
3. The buses will share this stop sign with traffic from the north side of 23rd. That traffic crosses under the bridge on Perimeter Road to get on to 23rd going east. This includes the residential neighborhood north of 23rd St. and new businesses on North Perimeter Road since Public Works moved out (a contractor, an appliance store, a chemical manufacturer and an unoccupied building currently under development). This is scenario 1, the one recommended by the current traffic study.
4. Buses cannot make a right hand turn directly into the right lane of 23rd St. Their turn will be wide and require more than one lane. The opening onto 23rd street is narrow and the bridge lanes themselves were narrowed by the bridge project in 2013 in order to add the central turning lane. This is a change from when Public Works utilized the space.
5. Traffic from 23rd and Haskell backs up west past the proposed access onto 23rd Street. When going east from that point during the school year morning commute, I routinely wait for someone to let me in. This is further complicated by a short sight horizon that makes judging oncoming 45 mph traffic difficult.
6. There is a sidewalk on 23rd that will cross in front of the buses as they enter 23rd Street. This is different from Burroughs Creek Trail, this is up on the arterial and goes over the bridge. It was added in the 2013 project too.
7. Public Works had fewer vehicles than the bus fleet and they did not all move at the same time. By comparison, the ingress/egress pattern of the buses and employee vehicles, as described in the traffic study, is much more intense: 160 vehicles from 6am to 7am, again from 8am to 9am, again from 2pm to 3pm and again from 4 pm to 5pm. The traffic study admits in Item 7 that "with exception of the bus storage/staging, the anticipated use of the site and resulting trips will likely be very similar to the previous use." Providing bus maintenance at this site would be similar to its previous use. Rezoning and moving all of USD 497's buses in and out four times a day during rush hour is not.
8. The current traffic study assumes that the Southwest Lawrence Traffic Way "may decrease Annual Average Daily Traffic slightly", however, a 2011 article from the Property and Environment Research Center (reflecting what is a consensus among traffic planners) finds that studies show "road expansion increases traffic by increasing demand." We must not count on SWLTW to make this rezoning feasible.

9. The intersection may need to be reconfigured in order to accommodate the increased traffic. Considering the current state of city finances, the recent outlay of significant funds on the bridge\23rd widening project, and the cost of potential traffic mitigation measures, the rationale for this proposal is further put into question.

Use as the maintenance site for 497's buses seems in keeping with previous use by Public Works. Rezoning to allow storage and staging does not make sense when considered in context of the above. My understanding is that the current arrangement is working well and has been for many years. If one of the main purposes of this proposal is a land exchange to facilitate other projects, then perhaps other options are available that will not increase congestion, require additional costs, and compromise public safety.

Thank you for your consideration.

Respectfully,

Marsha Heeb

PLANNING COMMISSION REPORT
Regular Agenda - Public Hearing Item

PC Staff Report
07/25/2016

ITEM NO. 3 IBP TO IL; 2.11 acres; 1300 Research Park Drive, Lot 3, Block 3 (BJP)

Z-16-00215: Consider a request to rezone approximately 2.11 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, located at 1300 Research Park Drive, Lot 3, Block 3. Submitted by Wallace Engineering, for Lydia L. Neu and Robert M. Neu, property owners of record.

STAFF RECOMMENDATION: Staff recommends approval of the request to rezone approximately 2.11 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, with use restrictions, and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report subject to the following condition:

1. Permitted uses are limited to those listed below:

- a. *Work/Live Unit*
- b. *Cemetery*
- c. *College/University*
- d. *Day Care Center*
- e. *Event Center, Small*
- f. *Event Center, Large*
- g. *Postal & Parcel Service*
- h. *Public Safety*
- i. *Funeral and Interment*
- j. *Social Service Agency*
- k. *Health Care Office, Health Care Clinic*
- l. *Active Recreation*
- m. *Participant Sports & Recreation, Indoor*
- n. *Participant Sports & Recreation, Outdoor*
- o. *Passive Recreation*
- p. *Nature Preserve/Undeveloped*
- q. *Kennel, provided that the use be accessory to the Veterinary use*
- r. *Veterinary*
- s. *Restaurant, Quality*
- t. *Administrative and Professional*
- u. *Financial, Insurance & Real Estate*
- v. *Payday Advance, Car Title Loan Business*
- w. *Office, Other*
- x. *Parking Facility, Commercial*
- y. *Building Maintenance*
- z. *Business Equipment*
- aa. *Business Support*
- bb. *Construction Sales and Service*
- cc. *Maker Space, Limited*
- dd. *Maker Space, Intensive*
- ee. *Manufacturing & Production, Ltd.*

- ff. *Manufacturing & Production, Tech.*
- gg. *Research Service*
- hh. *Wholesale Storage & Distribution, Light*
- ii. *Agriculture, Crop*
- jj. *Broadcasting Tower*
- kk. *Communications Service Establishment*
- ll. *Recycling, Small Collection*

Reason for Request: *IBP (current zoning) may not permit the proposed land use (restoration) for the subject site, although this proposed use does generally conform with the established land use pattern in the surrounding IBP-zoned area. Thus, IL zoning [with restrictions] will facilitate the proposed restoration use and similar future uses upon the subject site, while preserving/maintaining the pattern and character within the neighborhood.*

KEY POINTS

- Proposal accommodates the development of a restoration business, which is classified as *Construction Sales and Service* in Section 20-1718 of the Land Development Code. *Construction Sales and Service* is permitted in the following districts:
 - CC (Community Commercial)
 - CR (Regional Commercial)
 - CS (Strip Commercial)
 - IL (Limited Industrial)
 - IG (General Commercial)
- The subject property is located within Oread West Research Park which is predominantly zoned IBP District.
- The rezoning proposal includes restrictions to the uses permitted in the IL District as a means to facilitate compatibility with the industrial/business park setting. Of the proposed permitted uses all are also allowed in the IBP District, with the exception of the *Work/Live Unit, Kennel, Building Maintenance*, and *Construction Sales and Service* uses.

ASSOCIATED CASES

- There are no active cases associated with this application at this time.

OTHER ACTION REQUIRED

- City Commission approval of rezoning request and adoption/publication of ordinance.
- Submittal and approval of a Major Site Plan application.
- Submittal of construction plans to Development Services for processing of building permit, which is required prior to construction activity.

PLANS AND STUDIES REQUIRED

- *Traffic Study* – Not required for rezoning
- *Downstream Sanitary Sewer Analysis* – not required for rezoning
- *Drainage Study* – Not required for rezoning
- *Retail Market Study* – Not applicable to residential request

ATTACHMENTS

Attachment A: Use Table

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

Staff had one meeting with two adjacent property owners and a member of the Alvamar Development Corporation to discuss the proposed rezoning and receive feedback regarding the potential implications of the proposed use. The comments generally addressed exterior site appearance and compatibility with the surrounding area. The discussion addressed the on-site storage of company vehicles and damaged goods. The meeting participants requested that those items be stored indoors. Staff relayed that compliance with screening and design standards

would be reviewed during the site plan process.

Project Summary:

The proposed request is for the rezoning of approximately 2.11 acres located in the Oread West Research Park from the IBP District to the IL District. This rezoning would accommodate the relocation and potential expansion of an existing restoration business to this undeveloped site. The Planning Director determined that a restoration business is classified as a *Construction Sales and Service* use in Section 20-1718 of the Land Development Code. This use is not permitted in the IBP District.

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response: *The request maintains the land use pattern present in the "Industrial/Business/Research Park" (pp. 7-3, 7-9); provides/promotes "employment opportunities" (p. 7-9); is compatible with "new technology-related industries" by providing support services, when needed, to the immediate and surrounding areas (p. 7-10).*

Recommendations in *Horizon 2020* are discussed below, with staff comments in red.

Industrial and Employment-Related Land Use (Chapter 7, *Horizon 2020*)

Policy 1.2: Ensure Compatibility of Development

"Encourage best management practices for site planning and design that include, but are not limited to, building placement and orientation, vehicular and pedestrian circulation patterns, open space, landscaping, lighting, stormwater management, and interfacing with adjacent neighborhoods and development, and appropriate accommodation of the design to the site's natural features."

The compatibility of the development will be evaluated during the development process, which will include a site plan application and review. Staff will review the submitted site plan for compliance with the standards of the Land Development Code, which will include compatibility with the arrangement of buildings, off-street parking, lighting, landscaping, pedestrian walkways and sidewalks, ingress and egress, and stormwater drainage. A submitted site plan will also be reviewed with the Community Design Manual to ensure that the Industrial Design Standards are met. The review will ensure that, by meeting the Development Code standards, the development is compatible with the surrounding area.

Policy 1.3: Concentrate Industrial and Employment-Related Development

"Maintain an appropriate supply of industrially zoned land so that site choices are available and infrastructure expansion can occur in an efficient and orderly manner."

The rezoning request maintains the industrially zoned land in the Oread West Research Park. In addition, the restrictions associated with the rezoning align the proposed IL zoning to the surrounding IBP zoning of the research park. The uses permitted in the IBP District would be maintained with this rezoning, and the uses included in IL District would be business oriented, rather than consumer oriented.

Staff Finding – Rezoning of the subject property to the IL District would be in conformance with *Goal 1: Development in Established Industrial and Employment Related Area of Horizon 2020*. The proposed rezoning would conform with the Comprehensive Plan.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING OVERLAY ZONING

Current Zoning and Land Use: IBP (Industrial/Business Park) District; undeveloped.

Surrounding Zoning and Land Use: IBP (Industrial/Business Park) District in all directions; *Day Care Center* to the northwest, *Manufacturing and Production, Limited* and *Office* to the north, *Health Care Office, Health Care Clinic* to the east and southeast, undeveloped to the south and west.

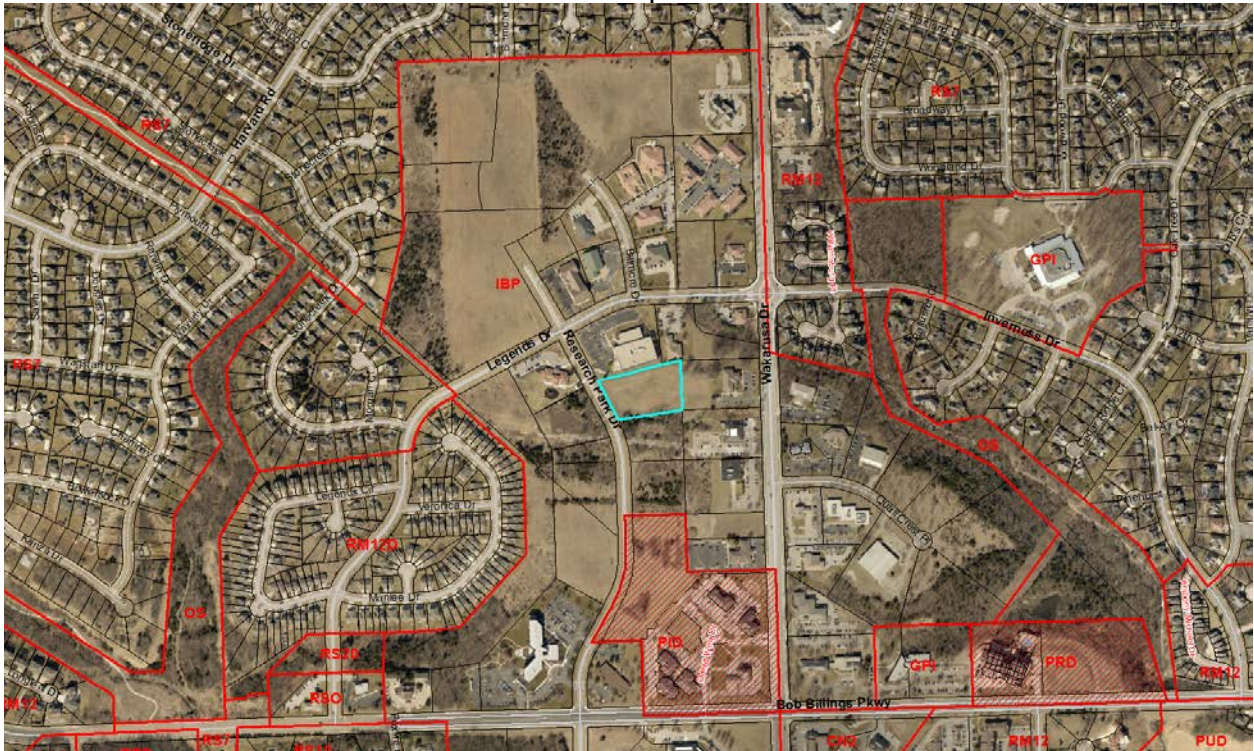


Figure 1: Surrounding zoning and land use of the subject property.

Staff Finding – The subject property is located in the Oread West Research Park. The surrounding properties are zoned IBP District and the subject property is adjacent to industrial and business type land uses, as well as vacant parcels. The proposed rezoning, with the use restrictions, is compatible with the existing zoning and land uses in the area.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response: *Business within a quarter-mile radius of the subject site include: dance school; gymnastics venue; tutoring service; plumbing contractor; dentist; financial planner/investment office; software company; Montessori school; two physical therapy offices and a university research office. Approx. 65% developed.*

The subject property is located in the Oread West Research Park and is surrounded by existing industrial and business type uses, which include a day care center to the northwest, Microtech Computers, Inc. and an office building to the north, Neu Physical Therapy Center to the east, and TherapyWorks to the southeast. There are also undeveloped parcels located to the south and southwest of the property.

Staff Finding – The subject property is located in the IBP District, which is intended to provide space for low-impact employment and manufacturing uses. The proposed rezoning would permit the development of a *Construction Sales and Service* use that is not permitted in the IBP District. The rezoning proposes restrictions to the uses permitted in the IL District as a means to align the zoning of the subject property to the surrounding area. Limiting the uses in the IL District would result in a project that is compatible with the character of the area.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

Staff Finding – The subject property is located within the boundaries of the West Lawrence Neighborhood Association, which does not have any adopted area or sector plan.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response: *The property is not necessarily unsuitable for uses currently allowed, however, a slight-to-moderate expansion of uses on the subject lot will promote continued development within an otherwise very slowly developing area.*

The property is currently zoned IBP which permits certain low-impact employment and manufacturing uses in a planned industrial/business parking setting. Below is an analysis of the new uses that would be permitted and the uses that would be restricted with the approval of the rezoning.

NEW USES THAT WOULD BE PERMITTED

A Use Table is provided in Attachment A that shows the uses permitted in the two districts. The rezoning proposes restricting many of the uses normally permitted in the IL District that are not permitted in the IBP District as a means to align the subject property to the surrounding area zoned IBP. Four uses permitted in the IL District (not allowed in the IBP District) are proposed to not be restricted. These uses include *Construction Sales and Service*, *Work/Live Unit*, *Kennel*, and *Building Maintenance*.

Construction Sales and Service

The Development Code defines *Construction Sales and Service* as:

"Construction activities and incidental storage on lots other than construction sites. Also includes landscape contractors and landscape maintenance businesses and the retail or wholesale sale, from the premises, of materials used in the construction of buildings or other structures including the retail sale of paint, fixtures, and hardware, but excludes those uses classified as "Automotive" and/or "Heavy Equipment" use types. Typical uses include home improvement or building material stores, tool and equipment rental or sales, **building contracting/construction offices with shops and/or outside storage yards.**" (Section 20-1717, emphasis added)

The rezoning would accommodate the relocation and expansion of an existing business, Rainbow International. This business provides restoration services to residential and business properties located in Topeka, Lawrence, and surrounding communities. This business is most like the contractor use described in the *Construction Sales and Service* definition above.

The Use Table of the Development Code indicates that the *Exterior Storage* use is permitted in the IBP and IL Districts as an accessory use subject to the standards in Section 20-538. To align the subject property with the surrounding development and the existing business park aesthetics, the *Exterior Storage* use would be restricted as a principal use; however, it would be permitted as incidental, or accessory, to the principal use. If incidental storage were a component of future development, the location and screening would be evaluated through the site plan review process.

Work/Live Unit

The use is defined in the Code as:

"A space within a building that consists of a dwelling unit which is accessory to a nonresidential use and has direct internal access to the nonresidential use." (Section 20-1734(8))

The applicant proposes not to restrict this use based on trends seen in modern-day research and industrial applications, and the perceived limited affects this use would have on nearby properties. Any proposal for a *Work/Live Unit* use would require submittal of a site plan application for administrative review and approval. Staff would review compatibility with the surrounding area and site plan compliance at that time.

Kennel

The use is defined in the Code as:

"Kennel services for dogs, cats, and small animals, including day care and overnight care. Typical uses include boarding kennels and dog training centers." (Section 20-1710(2))

The *Kennel* use supplements the *Veterinary* use, which is a use permitted in the IBP and IL Districts. The *Kennel* use would allow veterinary clinics to board their patients during the day or overnight. While the this use could also be associated with doggie day care facilities and dog training centers, the rezoning proposes restricting the *Kennel* use to activities that supplement the *Veterinary* use. Outdoor day care activities would be excluded from this rezoning.

Building Maintenance

The Code defines this use as:

"Provision of maintenance and custodial services to commercial and industrial establishments. Typical uses include janitorial, landscape maintenance and window cleaning services. Also includes exterminator services for residential, commercial or industrial applications." (Section 20-1712)

It is proposed that this use not be restricted given its supplemental nature with the *Construction Sales and Service* use. Permitting this use may provide an opportunity for the property to be developed with uses that complement each other.

USES THAT WOULD BE RESTRICTED

Below is a list of the uses that are allowed in the IBP District that are not allowed in the IL District. If the rezoning is approved, the following uses would no longer be an option for development.

- *Cultural Center Library*
- *Extended Care Facility, General* (permitted with approval of a Special Use Permit)
- *Accessory Restaurant* (permitted as an accessory use)
- *Private Dining Establishment*

The uses normally permitted in the IL District that are proposed to be restricted are listed below. Uses shown in blue are permitted with the approval of a Special Use Permit.

- | | | |
|---------------------------------------|-------------------------------------|---|
| • Mobile Home | • Personal Improvement Services | • Light Equipment Sales/Rental |
| • Detention Facilities | • Repair Service, Consumer | • RV and Boat Storage |
| • Lodge, Fraternal & Civic Assembly | • Retail Sales, General | • Industrial, General |
| • Campus or Community Institution | • Hotel, Motel, Extended Stay | • Laundry Service |
| • Neighborhood Institution | • Vehicle Sales & Service; Cleaning | • Scrap and Salvage Operation |
| • Livestock Sale | • Fleet Storage | • Wholesale Storage & Distribution; Heavy |
| • Animal Services; Sales and Grooming | • Gas and Fuel Sales | • Mini-Warehouse |
| • Fast Order Food | • Heavy Equipment Repair | • Agricultural Sales |
| • Fast Order Food, Drive-in | • Heavy Equipment Sales/Rental | • Recycling; Large Collection |
| • Food and Beverage | • Inoperable Vehicles Storage | • Recycling; Processing Center |
| • Mixed Media Store | | |
| • Personal Convenience Services | • Light Equipment Repair | |

Staff Finding – The subject property is suited for the addition of the *Construction Sale and Services* use, as well as the other uses discussed above that would differ from the uses permitted in the IBP District.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response: *22 to 23 years.*

Staff Finding – The property was annexed in 1988 and has been vacant since at least that time.

7. EXTENT TO WHICH APPROVING THE REZONING WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicant's Response: *No detrimental effects are anticipated as a result of this approval.*

Staff met with nearby property owners on June 23, 2016 to discuss the project in more detail. Feedback that was received during the meeting included concerns on the number of company vehicles that would be parked on-site, and the potential for damaged goods to be stored outside.

The applicant noted that Rainbow International presently operates two service vehicles and those vehicles are stored indoors. The intent of the new development would also be for the vehicles to be parked indoors. Regarding the storage of damaged goods, the applicant noted that damaged materials are not brought back to the facility. Instead, the materials are typically disposed of at the project site.

Concerns voiced by the property owners will also be addressed during the development process via the site plan application review. The process will include review of the site for compliance with the standards of the Development Code. In particular, landscape screening standards for perimeter parking lot and bufferyard requirements will be reviewed for compliance. The site plan will also

note areas that will be used for incidental storage and show how the areas will be screened from view.

The site plan review process will also include public notice in the form of written notice mailed to property owners within 200 feet of the subject property and sign posting on the property. The notification will provide an opportunity for feedback and input to be given on the proposed site layout and building design.

Staff Finding – The request to rezone the property from IBP District to IL District includes proposed restrictions on the uses permitted in the IL District, as discussed in Factor 6 above. The recommended conditions set on these uses should minimize negative impacts to nearby properties. The rezoning, with the proposed restrictions on permitted uses, should have positive impact on the nearby properties through the development of a property that has been vacant for multiple decades.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicant's Response: *Gain to the public will be the addition of land-uses to support the existing business park and surrounding neighborhoods, an expansion of the employment base, and a reduction of undeveloped, unmaintained property within the park. Denial of this application will perpetuate non-development of the subject site and effectively force an appropriate land-use to seek a less suitable location.*

Evaluation of this criterion includes weighing the benefits to the public versus the benefit of the owners of the subject property. Benefits are measured based on anticipated impacts of the rezoning request on the public health, safety, and welfare.

If the rezoning request were denied, the property would remain vacant as an IBP property, which has been in that state since the property was annexed in 1988.

If the rezoning were approved, a change in the uses permitted on the property would occur. However, use restrictions are proposed with the rezoning to align the subject property with the IBP District. Approval of the request will facilitate development and investment in existing property.

Staff Finding – There is no harm to the public resulting from approval of this request.

9. PROFESSIONAL STAFF RECOMMENDATION

Staff recommends approval of the request to rezone approximately 2.11 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, with use restrictions and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report subject to the following condition:

1. Permitted uses are limited to those listed below:

- a. *Work/Live Unit*
- b. *Cemetery*
- c. *College/University*
- d. *Day Care Center*
- e. *Event Center, Small*

- f. *Event Center, Large*
- g. *Postal & Parcel Service*
- h. *Public Safety*
- i. *Funeral and Interment*
- j. *Social Service Agency*
- k. *Health Care Office, Health Care Clinic*
- l. *Active Recreation*
- m. *Participant Sports & Recreation, Indoor*
- n. *Participant Sports & Recreation, Outdoor*
- o. *Passive Recreation*
- p. *Nature Preserve/Undeveloped*
- q. *Kennel, provided that the use be accessory to the Veterinary use*
- r. *Veterinary*
- s. *Restaurant, Quality*
- t. *Administrative and Professional*
- u. *Financial, Insurance & Real Estate*
- v. *Payday Advance, Car Title Loan Business*
- w. *Office, Other*
- x. *Parking Facility, Commercial*
- y. *Building Maintenance*
- z. *Business Equipment*
- aa. *Business Support*
- bb. *Construction Sales and Service*
- cc. *Maker Space, Limited*
- dd. *Maker Space, Intensive*
- ee. *Manufacturing & Production, Ltd.*
- ff. *Manufacturing & Production, Tech.*
- gg. *Research Service*
- hh. *Wholesale Storage & Distribution, Light*
- ii. *Agriculture, Crop*
- jj. *Broadcasting Tower*
- kk. *Communications Service Establishment*
- ll. *Recycling, Small Collection*

TABLE 1. USE TABLE FOR IBP and IL DISTRICT
Uses proposed to be restricted in the IL District are shown with a strikethrough.

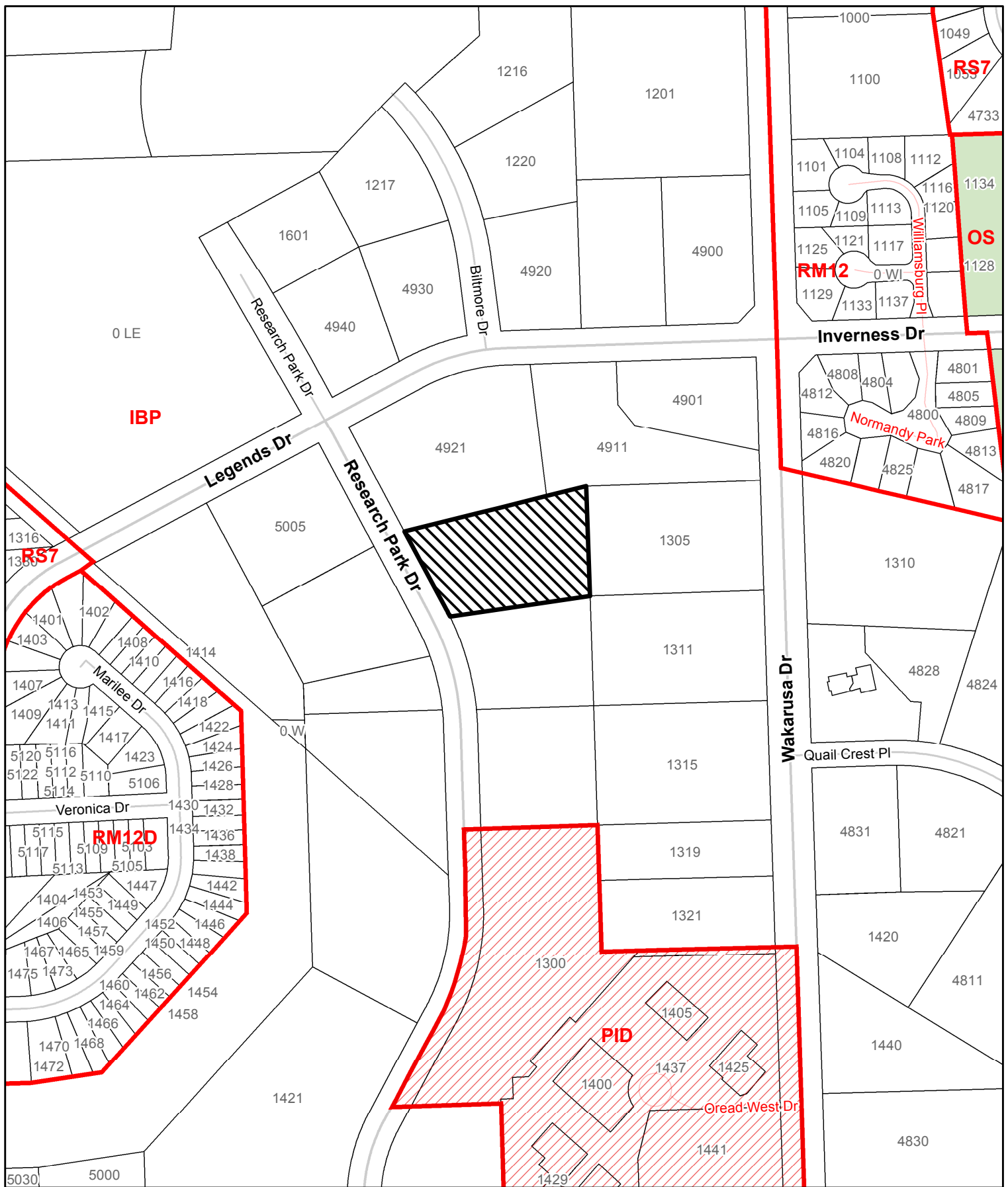
<i>A = Accessory</i> <i>P = Permitted</i> <i>S = Special Use</i> <i>* = Standard Applies</i> <i>- = Use not allowed</i>	IBP	IL	STDS
Household Living			
Accessory Dwelling Unit	-	-	
Attached Dwelling	-	-	
Cluster Dwelling	-	-	
Detached Dwelling	-	-	
Duplex	-	-	
Manufactured Home	-	-	
Manufactured Home, Residential-Design	-	-	
Mobile Home	-	P	
Mobile Home Park	-	-	
Multi-Dwelling Structure	-	-	
Non-Ground Floor Dwelling	-	-	
Work/Live Unit	-	P*	517/541
Zero Lot Line Dwelling	-	-	
Home Occupation, Type A or B	-	-	
Group Living			
Assisted Living	-	-	
Congregate Living	-	-	
Dormitory	-	-	
Fraternity or Sorority House	-	-	
Group Home, General [11 or more]	-	-	
Group Home, Limited [10 or fewer]	-	-	
Community Facilities			
Cemeteries	P*	P*	505
College/University	P	P	
Cultural Center/ Library	P	-	
Day Care Center	P*	P*	507
Day Care Home, Class A	-	-	
Day Care Home, Class B	-	-	
Detention Facilities	-	S	
Event Center, Small	P	P	
Event Center, Large	P	P	
Lodge, Fraternal & Civic Assembly	-	P*	512
Postal Service	P	P	
Public Safety	P	P	
School	-	-	
Funeral and Interment	P*	P*	505
Temporary Shelter	S*	S*/A	544/522
Social Service Agency	P	P	
Community Meal Program	S	S/A*	522
Utilities, Minor	P*/S*	P*/S*	530
Utilities and Service, Major	S	S	

Medical Facilities			
Community Mental Health Facility	–	–	
Extended Care Facility, General	S	–	
Extended Care Facility, Limited	–	–	
Health Care Office, Health Care Clinic	P	P	
Hospital	–	–	
Outpatient Care Facility	–	–	
Recreational Facilities			
Active Recreation	P	P	
Entertainment & Spectator Sports, General	–	–	
Entertainment & Spectator Sports, Limited	–	–	
Participant Sports & Recreation, Indoor	P	P	
Participant Sports & Recreation, Outdoor	P	P	
Passive Recreation	P	P	
Nature Preserve/Undeveloped	P	P	
Private Recreation	–	–	
Religious Assembly			
Campus or Community Institution	–	P*	522
Neighborhood Institution	–	P*	522
Animal Services			
Kennel	–	A**	
Livestock Sale	–	P	
Sales and Grooming	–	P	
Veterinary	P	P	
Eating & Drinking Establishment			
Accessory Bar	A*	A*	509
Accessory Restaurant	A	–	
Bar or Lounge	–	–	
Brewpub	–	–	
Fast Order Food	–	P*	511/509
Fast Order Food, Drive-in	–	P	
Nightclub	–	–	
Private Dining Establishments	P*	–	539
Restaurant, Quality	P*	P*	524
Office			
Administrative and Professional	P	P	
Financial, Insurance & Real Estate	P	P	
Payday Advance, Car Title Loan Business	P	P	
Other	P	P	
Parking Facilities			
Accessory	A*	A*	535
Commercial	P	P	
Retail Sales & Services			
Building Maintenance	–	P	
Business Equipment	P	P	
Business Support	P	P	
Construction Sales and Service	–	P	
Food and Beverage	–	P*	511
Mixed Media Store	–	P*	516/528

Personal Convenience	-	P	
Personal Improvement	-	P	
Repair Service, Consumer	-	P*	523
Retail Sales, General	-	P*	525
Retail Establishment, Large	-	-	
Retail Establishment, Medium	-	-	
Retail Establishment, Specialty	-	-	
Sexually Oriented Businesses			
Sexually Oriented Media Store	-	-	
Physical Sexually Oriented Business	-	-	
Sex Shop	-	-	
Sexually Oriented Theater	-	-	
Transient Accommodations			
Bed and Breakfast	-	-	
Campground	-	-	
Hotel, Motel, Extended Stay	-	P	
Vehicle Sales & Services			
Cleaning (e.g., car wash)	-	P	
Fleet Storage	-	P	
Gas and Fuel Sales	-	P	
Truck Stop	-	-	
Heavy Equipment Repair	-	P	
Heavy Equipment Sales/Rental	-	P	
Inoperable Vehicles Storage	-	P	
Light Equipment Repair	-	P	
Light Equipment Sales/Rental	-	P	
RV and Boats Storage	-	P	
Industrial Facilities			
Explosive Storage	-	-	
Industrial, General	-	P	
Industrial, Intensive	-	-	
Laundry Service	-	P	
Maker Space, Limited	P	P	
Maker Space, Intensive	P	P	
Manufacturing & Production, Limited	P	P	
Manufacturing & Production, Tech	P	P	
Research Service	P	P	
Scrap and Salvage Operation	-	S*	527
Wholesale Storage & Distribution			
Exterior Storage	A*	A*	538
Heavy	-	S	
Light	P	P	
Mini-Warehouse	-	P	
Adaptive Reuse			
Designated Historic Property	S*	S*	501
Greek Housing Unit	-	-	
Agriculture			
Agricultural Sales	-	P	
Agriculture, Animal	-	-	
Agriculture, Crop	P	P	

Communication Facilities			
Amateur and Receive-Only Antennas	A*	A*	536
Broadcasting Tower	P	P	
Communications Service Establishment	P	P	
Telecommunications Antenna	A*	A*	529
Telecommunications Tower	S*	S*	529
Satellite Dish	A*	A*	536
Mining			
Mining	-	-	
Recycling			
Large Collection	-	P	
Small Collection	P	P	
Processing Center	-	S	

** Kennel Use permitted as Accessory to the *Veterinary* use



**Z-16-00215: Rezone Approximately 2.11 acres from IBP District to IL District
Located South of Legends Drive on East side of Research Park Drive**

Lawrence-Douglas County Planning Office
July 2016

 Subject Property



PLANNING COMMISSION REPORT
Regular Agenda – Public Hearing Item

PC Staff Report
07/25/16

ITEM NO. 4 SPECIAL USE PERMIT; SEEDS FROM ITALY; 1501 LEARNARD AVE (MKM)

SUP-16-00217: Consider a Special Use Permit for Seeds From Italy, located at 1501 Learnard Avenue. The applicant proposes to renovate an existing building on the property for office and warehouse use supporting their mail-order garden seed business. Submitted by Lynn Byczynski, for Sunrise Green LLC, property owner of record.

STAFF RECOMMENDATION: Planning Staff recommends approval of a Special Use Permit for Seeds From Italy a *Wholesale Storage and Distribution, Limited* use to be located at 1501 Learnard Avenue as Phase 1 of the Sunrise Green Project, and forwarding the item to the City Commission with a recommendation of approval subject to the following conditions:

1. Provision of a site plan performance agreement.
2. Provision of shop drawings for the new manhole construction/connection and the inspection fee to the City Utilities Division for approval prior to the release of the Special Use Permit to Development Services for processing of a building permit.
3. New manhole constructed and connected to service lines per utilities approval prior to release of Certificate of Occupancy.
4. Prior to the release of the Special Use Permit for issuance of a building permit the applicant shall provide a revised drawing to include the following note:
 - a. *"Alternative Compliance from the Bufferyard Requirement on the south property line was approved to allow the building itself to serve as a buffer. Alternative Compliance was approved based on site constraints, primarily the lack of area to the south of Building C to install a landscaped bufferyard, and the fact that the building serves as an effective screen for the facility and operations to the north."*

Reason for Request: *"We are seeking a Special Use Permit to convert a garage on the property of the former Sunrise Garden Center into an office and storage area for our mail-order garden seed business. The property was recently rezoned to allow the use, provided we receive a Special Use Permit."*

KEY POINTS

- The Special Use Permit application was submitted for Phase 1 of the Sunrise Green project, the conversion of an existing garage, Building C on the attached site plan, into a mail-order seed distribution business. This is one of the less intense uses proposed as part of the project.
- The subject property is not platted. Platting will be required prior to more significant development, but the Planning Director determined that the seed mail-order company and the educational/growing component, proposed in Building A on the attached site plan, could occur prior to platting due to their low intensity and the fact that they will reuse existing buildings on the site. This will accommodate the project by allowing the low intensity changes to occur without platting; while, preserving the platting requirement for the more significant site improvements, such as a building addition or new construction.

- Public Improvements will be required with the platting of the property to extend a sanitary sewer main to serve the facility. A new manhole connection will be required with this phase. The City Utilities Division will require shop drawings of the connection and payment of inspection fees prior to the release of the Special Use Permit plans.
- In 2015, the property was rezoned to the IL District, conditioned to restrict permitted uses, to accommodate the proposed local food project.

ASSOCIATED CASE

- Z-15-00427; Rezoning of the subject property from RS7 to IL, with conditions. Approved by the City Commission on December 8, 2015 with the adoption of Ordinance No. 9181.

OTHER ACTION REQUIRED

- City Commission approval of Special Use Permit and adoption of related ordinance.
- Publication of Special Use Permit ordinance.
- Submittal and approval of shop drawings and inspection fees for new manhole, prior to the release of the SUP plans to Development Services for processing with building permits.
- Building permits shall be obtained from Development Services Division prior to the commencement of development.
- Manhole shall be constructed and connected to service lines per the City Department of Utilities approval prior to the release of Certificate of Occupancy for Building C.

PLANS AND STUDIES REQUIRED

- *Traffic Study* – The Traffic Study requirement has been waived as the City Engineer determined that proposed development will not generate traffic impacts sufficient to justify the preparation of a Traffic Impact Study.
- *Downstream Sanitary Sewer Analysis* – Fixture count analysis provided as the Downstream Sanitary Sewer Analysis was accepted by the City Utilities Engineer.
- *Drainage Study* – A drainage study is not required for this project because there is no change in the property surface summary.

ATTACHMENTS

Attachment A: Special Use Permit Site Plan

Attachment B: Rezoning Ordinance

Attachment C: Public Communication

PUBLIC COMMENT

- Hampton letter, expressing support for the application.

GENERAL INFORMATION

Current Zoning and Land Use:

IL (Limited Industrial, with conditions); vacant nursery buildings with *Crop Agriculture*.

Surrounding Zoning and Land Use:

To the north:

RS5 (Single-Dwelling Residential) District with minimum lot area of 5,000 sq ft; *Detached Dwellings*

To the northwest:

GPI (General Public and Institutional Use) District; *School*

To the west, south, and east:

RS7 (Single-Dwelling Residential) District with minimum lot area of 7,000 sq ft; *Detached Dwellings*
(Figure 1)

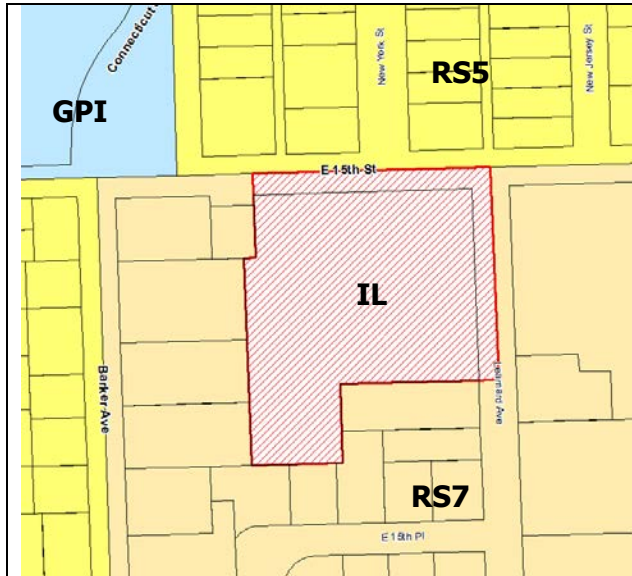


Figure 1a. Zoning of surrounding area. Subject property outlined.



Figure 1b. Land use of surrounding area.

SITE SUMMARY	Existing	Proposed	Change
Land Use:	<i>Crop Agriculture;</i>	<i>Wholesale Storage and Distribution, Limited</i>	Change in use
Land Area:	127,665 sq ft	127,665 sq ft	---
Building Area:	39,332 sq ft	39,332 sq ft	---
Pavement Area:	3,980 sq ft	3,980 sq ft	---
Impervious Area:	43,312 sq ft (33.9%)	43,312 sq ft (33.9%)	---
Pervious Area:	84,353 sq ft	84,353 sq ft	---

SUMMARY OF SPECIAL USE

This application proposes the conversion of a 1,520 sq ft building (formerly a garage) into office and warehouse space for Seeds from Italy, a mail-order seed distribution company. (Figure 2) The applicant indicated that there would be one to two semi-truck deliveries per year and that product would be distributed through the mail. No customers are expected at the site. They currently operate this business as a Rural Home Occupation in the unincorporated portion of the County, but have decided to move it into the city limits. The improvements to the building will include the addition of a restroom. There will be no other physical changes to the site to accommodate this use.



Figure 2. Building C, proposed mail order distribution/seed warehouse.

A *Wholesales Storage and Distribution, Limited* use is typically permitted in the IL District with site plan approval, but conditions placed on the IL Zoning in this location allow the use only when approved with a Special Use Permit to insure compatibility with adjacent residential uses. Section 20-1306(a), of the Development Code provides the following information on the review procedures for a Special Use:

"The process entails a public review and evaluation of the use's operating characteristic and site development features and is intended to ensure that proposed Special Uses will not have a significant adverse impact on surrounding uses or on the community at large."

SITE PLAN REVIEW

Review and Decision-Making Criteria (20-1306(i), Development Code)

1. WHETHER THE PROPOSED USE COMPLIES WITH ALL APPLICABLE PROVISIONS OF THIS DEVELOPMENT CODE

Applicant's Response:

"Yes, the current zoning allows for the office and warehouse use."

The proposed use, a mail order seed distribution business, is classified in the Development Code as a *Wholesale Storage and Distribution, Limited* use. The subject property was recently rezoned to the IL zoning district with conditions limiting the permitted uses on the property. The conditions allow the *Wholesale Storage and Distribution, Limited* use when approved as a Special Use. Standards that apply to this development include density and dimensional requirements in Article 6, parking in Article 9, landscaping in Article 10 and general development standards in Article 11. The following is a review of the change being proposed with this project, the renovations to Building C, for compliance with the Development Code.

DENSITY AND DIMENSIONAL STANDARDS

The property is not currently platted. The Planning Director determined that the proposed use and other low-scale uses could occur on the site without requiring platting; however, uses that were more intense or that required significant development on the site would trigger the platting requirement. The IL District requires a minimum lot area of 20,000 sq ft and a minimum width of 100 ft. The subject property exceeds these requirements with an area of approximately 2.93 acres (127,630 sq ft) and a width of 300 ft.

IL District Density and Dimensional Standards		
STANDARD		
REQUIRED		
PROVIDED		
Lot Area		
20,000 sq ft		
127,630 sq ft		
Lot Width		
100 ft		
300 ft		
Maximum Impervious Lot Coverage	75%	33.9%
Setback from Street	50 ft	83 ft
Setback from Adjacent Lots	20 ft	20 ft with exception of the eastern row of hoopouses, the concrete mulch bins and Building C, the proposed warehouse/office building.*

*The property at 1501 Learnard Avenue was developed with greenhouses and a retail store in 1926, prior to annexation into the City and prior to the adoption of zoning regulations in the County. The Douglas County Appraiser's records note that the garage was constructed in 1998. No

information is provided regarding the date the hoop houses were installed. Additional research would be necessary when development is proposed for the portion of the property containing the hoop houses or the platting of the property to determine if the hoop houses are nonconforming structures or if a variance would be required for them to remain in this location. The building permit that was issued for the garage, Building C on the plan, indicates City approval of the location and it will be considered as a nonconforming structure, as defined in Section 20-1503 of the Development Code. A nonconforming structure is one that was legally installed, but that no longer complies with the City Code. Building C can be used and can be remodeled or extended as long as the changes do not increase the degree of the nonconformity, which in this case would be increased encroachment into the setback.

PARKING

Per the Off-Street Parking Schedule C (Section 20-904 of the Development Code), a *Wholesale Storage and Distribution, Limited* use with less than 20,000 sq ft of building area requires 1 parking space per 1,000 sq ft of building area and 1 space per vehicle used in the business. The applicant indicated they would have one vehicle for the business. The building is 1,520 sq ft in area; therefore, 3 parking spaces are required for this use. One van accessible ADA parking space is required for parking lots with 25 spaces or less. The required parking is provided on the plan.

LANDSCAPING / BUFFERYARD

Street trees will be required with the platting and development of the property. The changes being proposed with this plan, the change in the use of Building C and the use of a portion of the parking, are interior to the site and won't require street trees at this time. The subject property is zoned IL (Limited Industrial) and is adjacent to property in the RS7 (Single-Dwelling Residential) District. It is across the street from property in the RS5 (Single-Dwelling Residential) District. A Type 3 landscaping buffer area, minimum of 15 ft wide, is required when these two zoning districts are adjacent each other.

North Property Line The existing greenhouse and retail building are set back approximately 80 ft from E 15th Street. (Figure 3) No bufferyard is required along E 15th Street with this phase of the project, as it will occur behind the greenhouses on the south side of the property and will not be visible from the street. The north bufferyard will be evaluated with the plan review when development of the facilities along E 15th Street is proposed.

West Property Line An access drive and hoop houses are located near the west property line. (Figure 3) Adequate space is provided along the west property line for a bufferyard and there is existing vegetation in this area. The current project is located to the east of the hoop houses



Figure 3. Bufferyards for the property. Area with inadequate space for a bufferyard is shown with a solid red line. The bufferyard required with this project is marked with yellow dashes.

and is not visible from the west property line; therefore a bufferyard along the west of the property is not required with this project. The west bufferyard will be evaluated with the plan review when development of the facilities along the west side of the property is proposed.

East Property Line. The building is separated from the east property line and Learnard Avenue by concrete mulch bins that are not a part of this project. Existing trees and shrubs provide screening along Learnard Avenue. (Figure 4) The existing vegetation effectively buffers the facility from Learnard Avenue. This bufferyard will be reevaluated with the plan review when development of the facilities along the east side of the property is proposed.

South Property Line As Building C is located near the south property line there is inadequate space between the building and the property line to install a landscaped bufferyard. (Figure 3) Fencing is often used in areas where there is inadequate space for a landscaped bufferyard; however, the rear wall of the building screens the development from the property to the south, in this location. (Figure 4)

Alternative Compliance has been approved for the Type 3 Bufferyard that is required along the south property line south of the subject building to allow the rear of the structure to serve as a screening wall. Alternative Compliance was approved based on site constraints, primarily the lack of area to the south of Building C to install a landscaped bufferyard, and the fact that the building serves as an effective screen for the facility and operations to the north. This should be noted on the plan.



Figure 4. Screening from existing vegetation along the east side of the property, Learnard Avenue right-of-way.



Figure 5. Rear of Building C, facing adjacent property to the south.

SITE COVERAGE.

The IL District permits a maximum of 75% of the site to be covered with impervious materials. Approximately 35% of the site is currently impervious.

LIGHTING

No exterior lighting is being proposed with this project. The plan notes that the addition of any exterior lighting will require the submittal of a photometric plan to the Planning Office for approval prior to the installation of any exterior lighting.

Staff Finding – This use complies with the applicable provisions of the Development Code as an allowed use in the IL District subject to a Special Use Permit and as conditioned.

2. WHETHER THE PROPOSED USE IS COMPATIBLE WITH ADJACENT USES IN TERMS OF SCALE, SITE DESIGN, AND OPERATING CHARACTERISTICS, INCLUDING HOURS OF OPERATION, TRAFFIC GENERATION, LIGHTING, NOISE, ODOR, DUST AND OTHER EXTERNAL IMPACTS

Applicant's Response:

"Yes. Seeds from Italy is a very small and quiet business. We expect to have a maximum of four people working in the building at any given time, so there will be minimal traffic. We will operate only during regular business hours, 8 a.m. to 6 p.m. We will be renovating an existing building, so there will be no additional structures."

The facility will be screened from nearby residential uses as noted in the section above. No exterior lighting is being proposed with this project. The applicant indicated there would be, at most, two semi-truck deliveries per year. Product will be distributed through the mail, so no customers are expected at the site. The business will operate from 8 am to 6 pm; therefore, there will be no night-time activity associated with this use. This is a very small scale, low intensity wholesale storage and distribution use. The applicants are currently operating the business from their home in rural Douglas County as a Rural Home Occupation.

Staff Finding – The proposed use is a very small scale, low intensity wholesale storage and distribution use that will operate during daytime hours. The business will be located in a building that is oriented toward the interior of the site. The proposed use is compatible with the adjacent land uses based on the small scale and low intensity of the proposed use and its orientation to the interior of the site.

3. WHETHER THE PROPOSED USE WILL CAUSE SUBSTANTIAL DIMINUTION IN VALUE OF OTHER PROPERTY IN THE NEIGHBORHOOD IN WHICH IT IS TO BE LOCATED

Applicant's Response:

"No, in fact we believe that bringing life back to the former garden center will be good for the neighborhood. As neighbors ourselves, we find that the vacant site is forlorn looking. We look forward to being part of a flourishing center for food production and related endeavors. Although our building is just a small part of the overall site, we plan for it to be an asset to Sunrise and to the Barker Neighborhood. We will invest in an attractive renovation of the run-down garage and we will help to maintain the landscaping and display gardens around the site."

One of the principal factors for rezoning the property to the IL District, was the rehabilitation of the garden center. The garden center has been in place since 1926, and predates many of the residences in the area. The garden center is a unique feature of area and rezoning was seen as a tool to accommodate the redevelopment of a Local Food oriented project while maintaining the character of the facility. As the applicant noted, this development is one of the early stages of that development.

Staff Finding – The project is one of the first phases for the re-use and rehabilitation of the nursery/garden center property with a local food oriented project. There is no evidence to support a finding that the proposed use would cause a diminution of other property values in the area.

4. WHETHER PUBLIC SAFETY, TRANSPORTATION AND UTILITY FACILITIES AND SERVICES WILL BE AVAILABLE TO SERVE THE SUBJECT PROPERTY WHILE MAINTAINING SUFFICIENT LEVELS OF SERVICE FOR EXISTING DEVELOPMENT

Staff Finding – Safety, transportation and utility facilities are available to serve the subject property.

5. WHETHER ADEQUATE ASSURANCES OF CONTINUING MAINTENANCE HAVE BEEN PROVIDED

Staff Finding – The site plan will function as the enforcement document to assure that the maintenance and use of the property is consistent with the approval.

6. WHETHER THE USE WILL CAUSE SIGNIFICANT ADVERSE IMPACTS ON THE NATURAL ENVIRONMENT

Applicant's Response:

"No. Our proposed use will have no effect at all on the natural environment. In fact, the entire thrust of the project is to support organic and sustainable gardening and farming. As long-time organic farmers ourselves, we do not plan to make any changes to the landscape, gardens, buildings, or other features that would harm the natural environment."

The proposed use should have no adverse impact on the natural environment. The facility will reuse and rehabilitate a vacant building. Minimal deliveries will be made to the site and the business will use the mail to distribute their product.

Staff Finding – The proposed use, a mail order seed distribution business, should have no adverse impact on the natural environment.

7. WHETHER IT IS APPROPRIATE TO PLACE A TIME LIMIT ON THE PERIOD OF TIME THE PROPOSED USE IS TO BE ALLOWED BY SPECIAL USE PEMRIT AND, IF SO, WHAT THAT TIME PERIOD SHOULD BE

Time limits are established on Special Use Permits to permit a periodic review to determine if the use remains compliant with the area. As mentioned earlier in the report, the use will have very few deliveries and very little impact on adjacent properties. The property is located in a developed residential area and has historically been used for a garden center/nursery which included truck deliveries, employees and customers on the site. The use, as approved, should remain compatible with the area. Any changes to the use would require review and approval. Significant changes to the site or a proposed change to the use would require a revised SUP and a public hearing while minor changes to the site would require approval of a revised site plan.

Staff Finding – The Special Use Permit would allow a small scale low-intensity use on a property that has historically been used as a nursery/retail garden center in the residential area. The Special Use Permit approval process for any significant change to the site or an intensification of use should insure compatibility with the neighborhood. It would not be appropriate to place a time limit on the Special Use Permit.

CONCLUSION

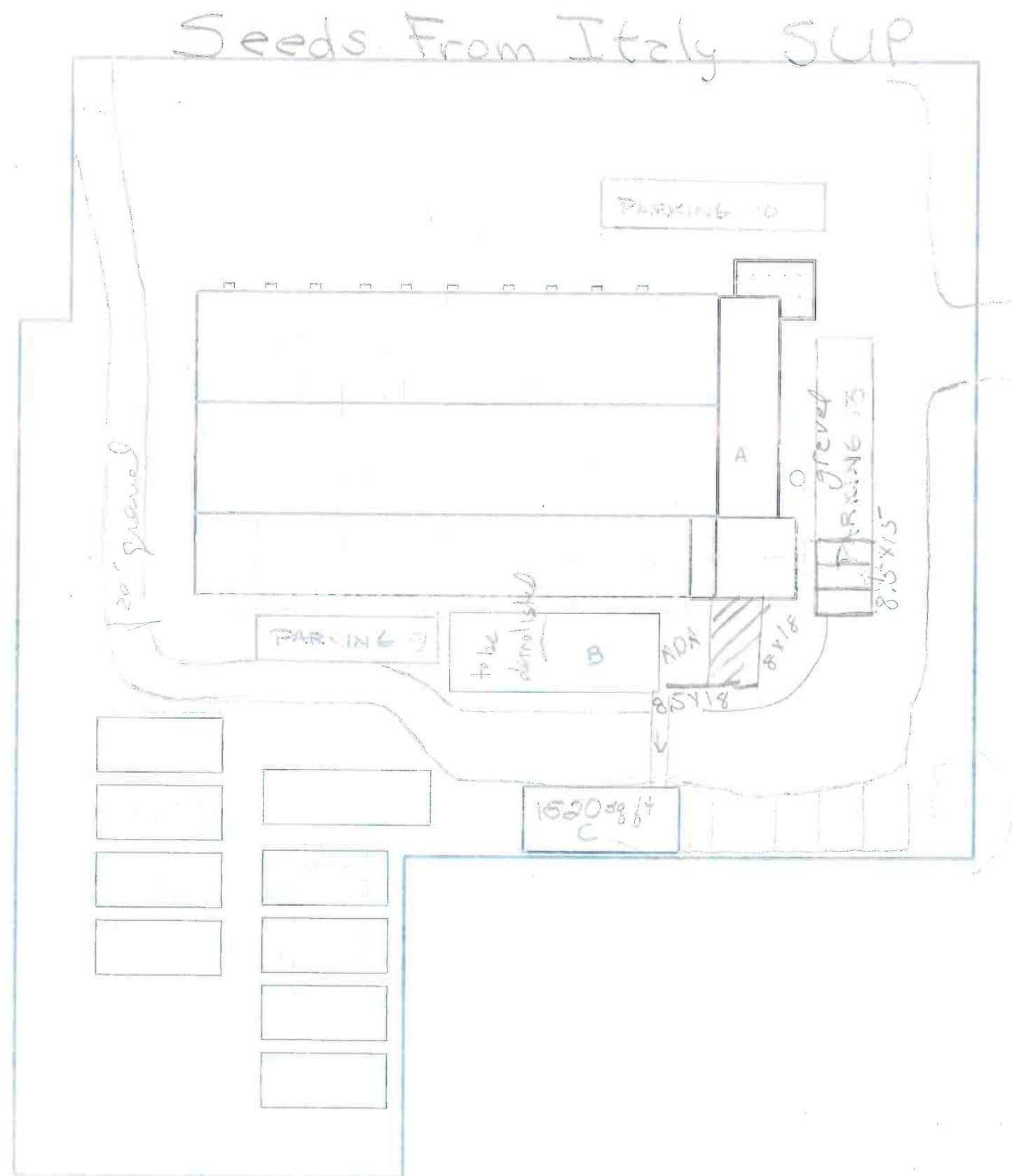
Based on the findings in this report, and as conditioned, staff recommends approval of the proposed Special Use Permit.



Property Address:
1501 LEARNARD AVE, LAWRENCE, KANSAS

Plate/Record Id: U10952

Legal Description
6-13-20 BEG AT PT 231 FT E OF NW COR NE
1/4 TH S 462 FT 2 INCHES E 146 FT N 125 FT 1
INCH E 250 FT N 337 FT 1 INCH W 396 FT TO PT
BEG



SUNRISE GARDEN CENTER PROJECT
1501 LEARNARD AVE, LAWRENCE, KANSAS

REVISIONS

Permit Documents

ISSUE DATE APRIL 12, 2016

SCALE 1" = 50'-0"

PROJECT NO

DRAWN BY

TITLE

EXISTING SITE PLAN

SHEET NO.

A 0.0

SUNRISE GARDEN CENTER PROJECT PHASE 1
Seeds from Italy warehouse (Building C)
SUP-16-00217

General Notes

Owner: Girasole Inc. dba Seeds from Italy, 966 E 800th Rd., Lawrence KS 66047

Zoning: IL, with conditions

Existing Use: Vacant nursery / greenhouse

Proposed Use: Wholesale Storage and Distribution, Limited

Site Area: 2.93 acres

Building C will be converted to an office and warehouse for Seeds from Italy, an importer of garden seeds that are sold online and by catalog to gardeners as well as wholesale to various retailers across the U.S.

Semi-truck delivery will be approximately 1 to 2 trucks per year. Distribution of product occurs through the U.S. Postal Service.

No exterior lighting is proposed with this project. The addition of exterior lighting in the future will require Planning approval of a photometric plan prior to installation of the lighting.

Heating and air conditioning will be added to the building. The mechanical equipment outside will be on the west side of the building and shall be fully screened from view of adjacent properties and from adjacent street rights-of-way per Section 20-1006 of the Development Code. This screening can be in the form of landscape plantings or architectural treatment that is similar to that of the principal building.

PARKING REQUIREMENT

Wholesale Storage and Distribution, Limited

Requirement: 1 space per vehicle used in the business PLUS 1 space per 1,000 square feet.

Required: 1 business vehicle and 1,520 sq. ft. building: 3 parking spaces required / 3 provided

ADA PARKING: 1 van accessible ADA space required / 1 provided

Bicycle parking: none required / none provided

SITE SURFACE SUMMARY

Existing and Proposed Summary

TOTAL BUILDINGS =39,332 S.F.

TOTAL PAVEMENT =3,980 S.F.

TOTAL IMPERVIOUS =43,312 S.F.

TOTAL PERVIOUS =84,353 S.F.

TOTAL PROPERTY AREA = 127,665 S.F.

ORDINANCE NO. 9181

AN ORDINANCE OF THE CITY OF LAWRENCE, KANSAS, REZONING APPROXIMATELY 2.96 ACRES FROM RS7 (SINGLE-DWELLING RESIDENTIAL) DISTRICT TO IL (LIMITED INDUSTRIAL) DISTRICT AND AMENDING THE CITY'S "OFFICIAL ZONING DISTRICT MAP," INCORPORATED BY REFERENCE INTO THE CITY CODE AT CHAPTER 20, ARTICLE 1, SECTION 20-108 OF THE CODE OF THE CITY OF LAWRENCE, KANSAS, 2015 EDITION, AND AMENDMENTS THERETO.

BE IT ORDAINED BY THE GOVERNING BODY OF THE CITY OF LAWRENCE, KANSAS:

SECTION 1. The base zoning district classification for the following legally described real property, situated in the City of Lawrence, Douglas County, Kansas, to-wit:

A TRACT OF LAND IN THE NORTHEAST QUARTER OF SECTION 6, TOWNSHIP 13 SOUTH, RANGE 20 EAST OF THE 6TH PRINCIPAL MERIDIAN IN THE CITY OF LAWRENCE, DOUGLAS COUNTY, KANSAS, DESCRIBED AS FOLLOWS:

COMMENCING AT A POINT 231 FEET EAST FROM THE NORTHWEST CORNER OF THE NORTHEAST QUARTER OF SAID SECTION 6; THENCE SOUTH 442 FEET, 2 INCHES; THENCE EAST 146 FEET; THENCE NORTH 125.1 FEET; THENCE EAST 250 FEET; THENCE NORTH 317 FEET; THENCE WEST 396 FEET TO THE POINT OF BEGINNING, LESS AND EXCEPT ANY PORTION THEREOF LYING IN LOT 1, PENCE ADDITION, AN ADDITION TO THE CITY OF LAWRENCE, DOUGLAS COUNTY, KANSAS.

is hereby changed from RS7 (Single-Dwelling Residential) District to IL (Limited Industrial) District, as such district is defined and prescribed in Chapter 20 of the Code of the City of Lawrence, Kansas, 2015 Edition, and amendments thereto.

SECTION 2. The rezoning granted in Section 1, *supra*, in addition to being subject to the general conditions established in Chapter 20 of the Code of the City of Lawrence, Kansas, 2015 Edition, as amended, is also subject to the following special conditions:

- (a) Permitted uses are limited to those listed below:
 - (i) Crop Agriculture;
 - (ii) Social Service Agency;
 - (iii) Health Care Office/Health Care Clinic, provided that the gross floor area shall not exceed 3,000 square feet;
 - (iv) Administrative and Professional Office, provided that the gross floor area shall not exceed 3,000 square feet;
 - (v) Personal Improvement, provided that the gross floor area shall not exceed 3,000 square feet;

- (vi) General Retail Sales, provided that the gross floor area shall not exceed 3,000 square feet;
 - (vii) Fast Order Food, provided that the gross floor area shall not exceed 3,000 square feet;
 - (viii) Limited Manufacturing and Production, provided that it is approved through the issuance of a Special Use Permit;
 - (ix) Light Wholesale Storage and Distribution, provided that it is approved through the issuance of a Special Use Permit;
 - (x) Neighborhood Religious Institution; and
 - (xi) Satellite Dish, accessory.
- (b) All site plans submitted for standard or major development projects shall require approval by the City Commission.

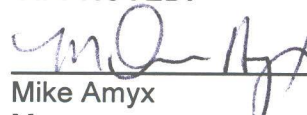
SECTION 3. The "Official Zoning District Map," which is adopted and incorporated into the City Code by reference at City of Lawrence, Kan., Code § 20-108 (Jan. 1, 2015), is hereby amended by showing and reflecting thereon the new zoning district classification for the subject property as described in more detail in Section 1, *supra*.

SECTION 4. If any section, sentence, clause, or phrase of this ordinance is found to be unconstitutional or is otherwise held invalid by any court of competent jurisdiction, it shall not affect the validity of any remaining parts of this ordinance.

SECTION 5. This ordinance shall be in full force and effect from and after its passage and publication as provided by law.

PASSED by the Governing Body of the City of Lawrence, Kansas, this 8th day of December, 2015.

APPROVED:




Mike Amyx
Mayor

ATTEST:

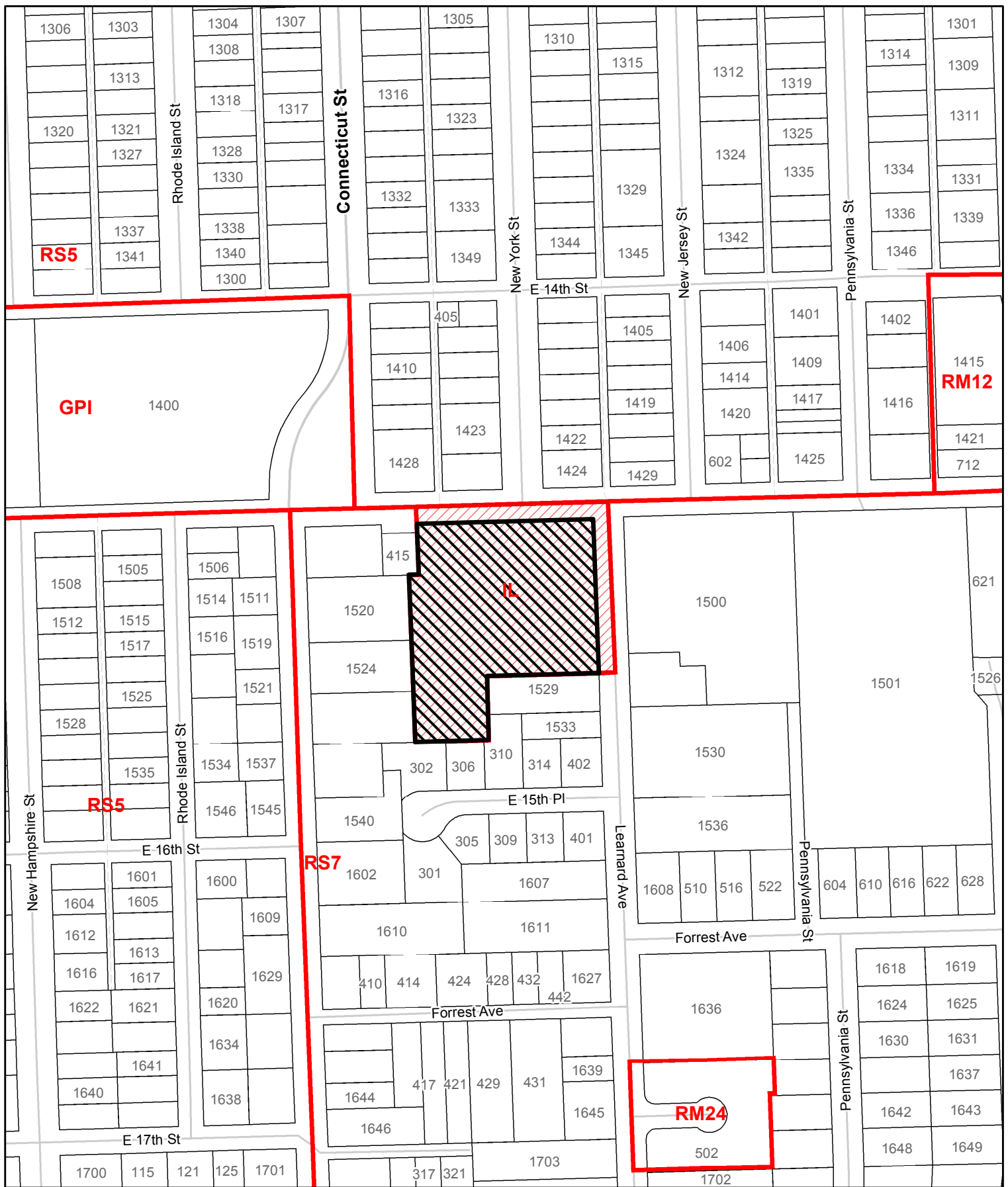


Brandon McGuire
Acting City Clerk

APPROVED AS TO FORM:




Toni R. Wheeler
City Attorney



SUP-16-00217: Special Use Permit for Seeds from Italy Located at 1501 Learnard Avenue

Lawrence-Douglas County Planning Office
July 2016

 Subject Property





July 12, 2016

Lawrence-Douglas County Planning Commission
6 E. 6th St.
Lawrence, Kansas 66044

Dear Planning Commissioners,

I am writing in support of Seeds From Italy's Special Use Permit request for 1501 Learnard Ave. Sunrise Project will share the site with Seeds From Italy and greatly values their partnership. The company's owners, Lynn Byczynski and Dan Nagengast, have built a very successful business and contribute great value to the community through the distribution of high quality Italian seeds.

The Special Use Permit would allow the company to renovate one of the buildings at the Sunrise site to be used for seed storage and office space. Their footprint would be very minimal, would not change the intensity of use, and would have less traffic impact than the previous occupant.

Thank you for considering Seeds From Italy's SUP request. Sunrise Project supports the Commission voting in favor of this request.

Sincerely,

Emily Hampton
Executive Director, Sunrise Project

From: [Matthew Stephens](#)
To: [Mary Miller](#)
Subject: Special Use Permit 1501 Learnard
Date: Friday, July 22, 2016 3:58:32 PM

Hello Mary,

I wanted to voice my support for the approval of the special use permit that Seeds from Italy is seeking for the garden center at 1501 Learnard Avenue. I live 3 houses down at 1536 Learnard and I am in full support of them renovating the old garage at the site to use for their business and I have zero concerns with their plan. As a resident who lives in close proximity to the site, I hope that the planning commission will approve this special use permit.

Thank you,

Matthew Stephens

July 19, 2016

Lawrence-Douglas County Planning Commission
6 E. 6th St.
Lawrence, Kansas 66044

Dear Planning Commissioners,

I wanted to send a note to express my support of the Special Use Permit request by Seeds from Italy to convert a garage on the property of the former Sunrise Garden Center into an office and storage area for their garden seed business.

As a regular customer and former employee of the business, I can attest to the high quality with which they run their business, as well as the respectful attitude that they maintain in all of their interactions. As a business owner who will grow in the greenhouse on the Sunrise site, I'm excited to have them as a neighbor, and I see their business as a great fit with the envisioned use of the property.

This business will be a positive addition to the neighborhood, as well as to all of Lawrence, and I urge the Commission to vote in favor of this request.

Sincerely,
Kevin Prather
Lawrence Organics, LLC

PLANNING COMMISSION REPORT
Regular Agenda - Public Hearing Item

PC Staff Report
07/25/2016

**ITEM NO. 5A RM12D (MULTI-DWELLING RESIDENTIAL) DISTRICT TO RS5
(SINGLE-DWELLING RESIDENTIAL) DISTRICT; 13.268 ACRES (SLD)**

Z-16-00219: Consider a request to rezone approximately 13.268 acres from RM12D (Multi-Dwelling Residential) District to RS5 (Single-Dwelling Residential) District, located at 751 W 29th Terrace. Submitted by Landplan Engineering, for Savannah Holdings, LC, property owner of record.

STAFF RECOMMENDATION: Staff recommends approval of the request to rezone approximately 13.268 Acres, from RM12D (Multi-Dwelling Residential) District to RS5 (Single-Dwelling Residential) District based on the findings presented in the staff report and forwarding it to the City Commission with a recommendation for approval.

Reason for Request: *This rezoning request facilitates the development of single-family detached homes on the subject property.*

KEY POINTS

- Planned residential development.
- Property includes small areas encumbered by regulatory floodplain.
- Proposed request modifies the housing type from duplex to detached housing.
- This application should be considered concurrently with the preliminary plat.

ASSOCIATED CASES/OTHER ACTION REQUIRED

- A-13-00437; Annexation of 14.53 Acres, Ord. No. 8953
- Z-13-00348; A to RM12D and RM12D-FP, Ord. No. 8954
- Z-16-00220; RM12D-FP to RS5-FP
- PP-16-00221; Naismith Creek Addition

PLANS AND STUDIES REQUIRED

- *Traffic Study* – Not required for rezoning
- *Downstream Sanitary Sewer Analysis* – not required for rezoning
- *Drainage Study* – Not required for rezoning
- *Retail Market Study* – Not applicable to residential request

ATTACHMENTS

1. Area Map
2. Neighborhood Map
3. Land Use Map
4. Existing RS5 Zoning District Map

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

- None

Project Summary:

This request would amend the zoning district affecting only the base zoning of the existing RM12D. This application was submitted concurrently with a preliminary plat for the Naismith Creek Addition.

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response: *This request conforms to the future land use recommendations provided in the Revised Southern Development Plan which, as a specific area plan, is incorporated into Chapter 14 of Horizon 2020. That plan specifically recommends for this site residential land use with a gross density of no more than 8 dwelling units per acre. As depicted in the preliminary plat, which is concurrently submitted with this rezoning request, the proposed subdivision for this site features a gross density of 4.5 units per acre. The Revised Southern Development Plan further specifies that the site development with a residential character similar to the neighborhood to the north including single-family, duplex, triplex and townhome structures. This request for RS5 will facilitate the development of such residential housing types. This request also conforms to multiple strategies for residential development listed in Chapter 5 of Horizon 2020 such as providing a mixture of housing types, styles and economic levels for new and infill residential developments, as well as providing appropriate transition zones between low-density residential uses and non-residential land uses.*

The original annexation and zoning of the area in 2013 included an identification of the boundary of the floodplain and resulted in certain additional areas needing to be included in the Floodplain Overlay District. Much of the area annexed that was encumbered by the regulatory floodplain was transferred to the City of Lawrence and expanded the boundary of Naismith Valley Park. Only a small area of the floodplain remains within the proposed development area and was required to be zoned with the Floodplain Overlay designation (Z-16-00220). This request impacts only the portion of the subject property that is not encumbered by the floodplain within the development area.

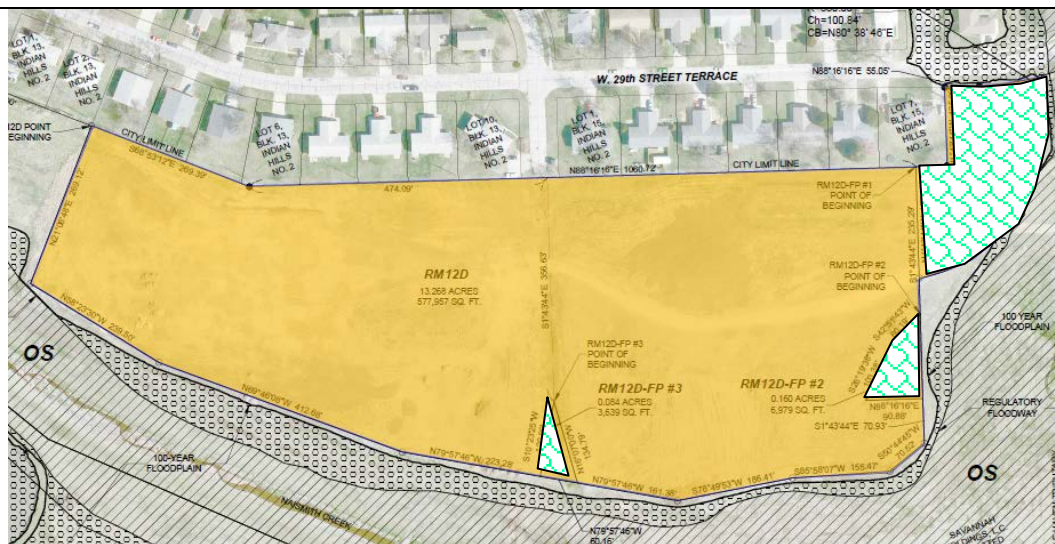


Figure 1: Areas with Floodplain Overlay

FP overlay highlighted with green patterned shape. Beige color represents the base zoning district unencumbered by floodplain.

The zoning was approved in 2013 for duplex development that included a concept plan for the area that extended the street network to the south.

This area is identified as suitable for future medium density residential development reflecting an overall density of 7 to 15 dwelling units per acre. Density was further defined by the approved ordinance to not exceed 8 dwelling units per acre. Regardless of the housing type, attached or detached, the maximum density for the area is unchanged by this request.

Residential Goal 3: Neighborhood Conservation:

The character and appearance of existing low-density residential neighborhoods should be protected and improvements made where necessary to maintain the values of properties and enhance the quality of life.

Policy 3.3 Encourage compatible Infill Development

a. Encourage redevelopment and infill as a means of providing a variety of compatible housing types within neighborhoods.

f. Maintain the physical form and pattern of existing, established neighborhoods to the extent possible.

Residential Goal 5: Create a Functional and Aesthetic Living Environment

Create and maintain neighborhoods that are aesthetically pleasing and functionally efficient and practical.

These goals and policies speak to the intent to integrate new infill development with the surrounding neighborhood pattern. These design policies are implemented through the subdivision design standards. This application meets the recommendations for medium density development while accommodating detached residential lots and providing additional housing choices within the neighborhood.

Staff Finding – The proposed rezoning request conforms with *Horizon 2020* policies related to medium density residential development in existing neighborhoods.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING OVERLAY ZONING

The following zoning and land use summary is provided for the surrounding area extending south to 31st Street. The request is to modify the base district and retain the Floodplain Overlay District designation.

Current Zoning and Land Use: Z-16-00220 applies to portions of development that are encumbered by the regulatory floodplain within the boundary of the development.	RM12D (Multi-Dwelling Residential) District and RM12D-FP (Multi-Dwelling Residential Floodplain Overlay) District. Undeveloped land.
Surrounding Zoning and Land Use:	RM12 (Multi-Dwelling Residential) District to the north; Existing development includes both duplex and triplex residential units. RS7 (Single-Dwelling Residential) District to the north and northeast east of Belle Haven Drive. Existing uses include a <i>Detached Dwelling</i> and a <i>Religious Assembly</i> use. OS-FP (Open Space Floodplain Overlay) District to the east, west, and south. Existing Naismith Valley Park.

Staff Finding – The proposed request is directly related to the request for the proposed preliminary plat for Naismith Creek Addition. The surrounding area to the south and east is also designated with the Floodplain Overlay District designation and is incorporated into Naismith Valley Park. Residential uses to the north are a mix of detached and attached dwellings. Land uses to the

east include a *Religious Institution* use and a City sanitary sewer pump station adjacent to Louisiana Street.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response: *The subject site is bounded to the north by the Indian Hills Neighborhood, a predominantly residential neighborhood. The properties immediately to the north were developed with duplexes in the late 1960s and are zoned RM12. Development north of W 29th Terrace consists of single-family detached homes and is zoned RS7. Property to the east, south and west is zoned Open Space with floodplain overlay district (OS-FP) and is owned by the City of Lawrence. The property to the east across Belle Haven Tributary is currently being developed by the City as the site of the new Pump Station No. 10. Also to the east is a pair of churches, both zoned RS7. The property to the west and south is part of Naismith Valley Park. Lying further to the south and west is property owned by Menards and the very eastern edge of the South Iowa Commercial Corridor.*

The subject property is located in the south portion of the Indian Hills Neighborhood. The neighborhood includes all of the Naismith Valley Park on the west and is bounded on the south and east by W. 31st Street and Louisiana Street. The Neighborhood extends to the north excluding development north of W. 24th Street west of Alabama and excluding development north of W. 25th Street between Alabama Street and Louisiana Street. A map of the Neighborhood is attached to this report.

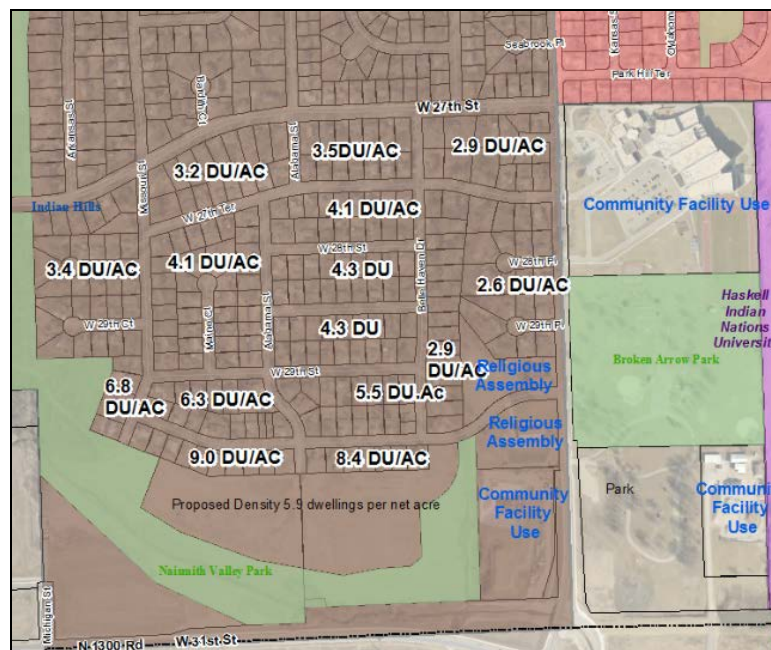


Figure 2: Residential Density south of 27th Street

The neighborhood is residential and includes detached and attached housing. A review of the density between 27th Street and 31st Street shows an increasing residential density at the south end of the subdivision. Housing type includes predominantly detached housing. Duplex, triplex and multi-dwellings are located at both the north and south ends of the neighborhood. The proposed request is consistent with this density pattern and provides a small lot, detached dwelling alternative

Staff Finding – The proposed request is compatible with the existing development pattern.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

This property is included in the revised Southern Development Plan boundary. The property is also located within the Indian Hills Neighborhood. The Revised Southern Development Plan identifies this property as suitable for medium-density residential (7-15 dwelling units per acre). Zoning districts consistent with this land use recommendation include the *Multi-Dwelling Residential Districts* RM12D, RM12 and RM15. Applicable medium-density *Detached Residential Districts* are RS3 and RS5. The proposed request is consistent with the planned development of the area in terms of residential use.

Staff Finding – The proposed request is consistent with the planned development of the area in terms of residential use.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response: *The subject property is currently zoned RM12 which strongly restricts the property to duplex, triplex or some other form of multi-family development. Additionally, the zoning district carries a condition which restricts the gross density to 8 or fewer dwelling units per acre. The current market for duplexes and townhomes in Lawrence is weak while the single-family character of the Indian Hills neighborhood is strong. Those factors make the property ill-suited for the uses to which it is currently restricted. Nearly three years since the property was annexed into the City and zoned RM12D, the applicant prefers to pursue single-family development of this property.*

The key consideration of this application is the change in housing type that is permitted in the RM12D district compared to an RS district. The overall intensity of development is restricted by the physical limitations of the property; floodplain, utility infrastructure, and access. This area is surrounded by and immediately adjacent to public property on the south, east and west. The direct access to public open space (Naismith Park) balances the design of smaller lots having less private open space with proximity and accessibility to public open space.

The previously approved duplex zoning reflected the existing development pattern immediately north. However, the proposed detached residential zoning is situated so that it is rear yard to rear yard orientation consistent with land use trends that change zoning boundaries within neighborhoods.

Either type of development type, duplex or detached housing are suitable options for this property. Since the adoption of 2006 Land Development Code, the RS5 District has grown in acceptance and is used in many of the original neighborhoods and in new developing neighborhoods. This district allows lots as small as 5,000 SF and as narrow as 40'. This district accommodates smaller residential lots with less area required for private maintenance (yards) and is often reflective of a more traditional type of neighborhood development pattern.

The approval of this area was restricted to a maximum density of 8 dwelling units per acre. The proposed request does not exceed that density cap. If the property were developed at the maximum allowed density, 8 dwelling units per acre, the total number of units would be between 106 and 115 residential units. This is an unrealistic capacity for the area and does not account for the requirements for public streets and minimum required yards. These elements are further considered with the preliminary plat.

Staff Finding – The proposed request is suitable for subject property.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response: *The property has been vacant, at least since purchased by the current owner in 1973.*

This property is undeveloped and has been zoned A (Agricultural) since 1966. The property was annexed and rezoned to RM12D in 2013.

Staff Finding – This property is undeveloped and has been zoned A (Agricultural) since 1966 and RM12D since 2013.

7. EXTENT TO WHICH APPROVING THE REZONING WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicant's Response: *This rezoning will not result in any significant detriment to nearby properties. The request will facilitate residential development at a type and scale similar to the existing neighborhood to the north. This request, combined with the concurrent rezoning request for property inside floodplain overlay districts, ensures preservation of the floodway and floodway fringe associated with the nearby Naismith Creek and Belle Haven Tributary.*

The property is currently a vacant field. There are a number of existing utility easements through the property along the north property line with planned improvements to the lines in the area. This will have the effect of disturbing the rear areas of the existing development (along W. 29th Terrace). The applicant is working with the City to coordinate improvements to limit disruption to the area as it develops. Utility improvements in the area will occur regardless of the zoning designation.

The property has been acknowledged through the years as suitable for future infill development. The Revised Southern Development Plan designated the specific use as to medium density residential. The proposed request does not alter that land use recommendation. The area to the south has been incorporated into the City's park inventory and Naismith Park has been extended to the south and east with the 2013 annexation and zoning considerations.

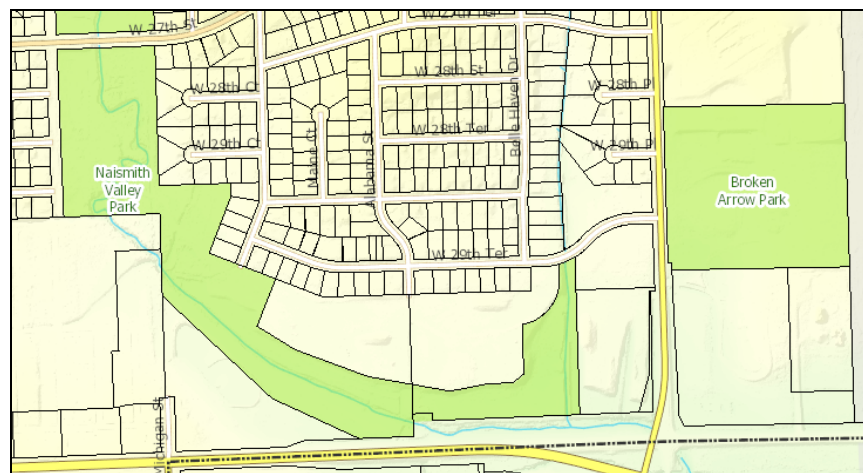


Figure 3: Existing Parks

Staff Finding – Staff concurs with the applicant that the proposed change will not result in detrimental impacts to the adjacent properties.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicant's Response: *Approval of this request facilitates the development of infill and appropriate residential uses adjacent to the existing Indian Hills neighborhood as recommended in Horizon 2020. Denial of this request will perpetuate the vacancy of this land.*

Evaluation of this criterion includes weighing the benefits to the public versus the benefit of the owners of the subject property. Benefits are measured based on anticipated impacts of the rezoning request on the public health, safety, and welfare.

Either the existing or the proposed zoning district will facilitate the infill development of the area within the Indian Hills neighborhood. This application is consistent with a trend to develop detached housing over attached housing. This phenomenon is also occurring in the Langston Heights subdivision in West Lawrence.

Staff Finding – While either the existing or the proposed zoning will meet the public needs as they relate to infill development and extending housing choices within an established neighborhood the proposed zoning is more consistent with the residential development trends of the neighborhood and the community.

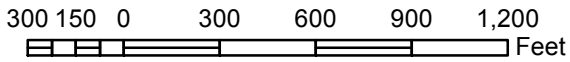
9. PROFESSIONAL STAFF RECOMMENDATION

The RS5 zoning district was initially applied to the older neighborhoods of Pinckney, Old West Lawrence, East Lawrence, Oread, Brook Creek, University Place, Centennial, Breezdale, Sunset Hills and North Lawrence. More recently it has been used in newer Neighborhoods including Fairfield Farms Subdivision east of O'Connell, Langston Heights (part of the West Lawrence Neighborhood) and in the Gateway Neighborhood west of Queens Road north of Overland Drive. See attached Map.

Staff recommends approval of the proposed rezoning.

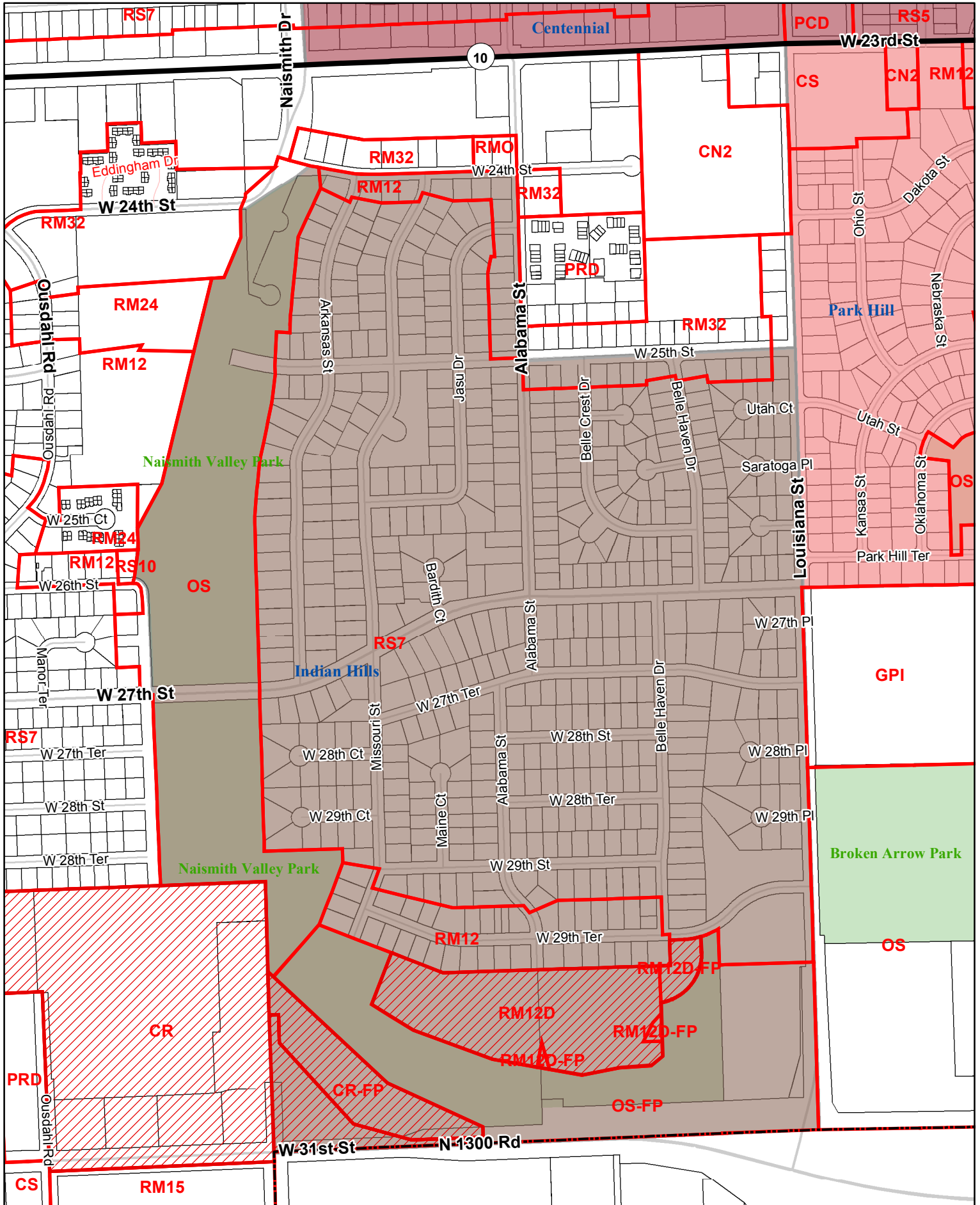
CONCLUSION

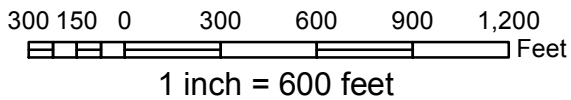
This application is submitted concurrently with a rezoning application for retaining the floodplain overlay district (Z-16-00220) and a preliminary plat (PP-16-00221). The proposed request continues the intent for medium density residential development but alters the housing type from duplex housing to detached housing. The proposed request is consistent with the planned development of the area.



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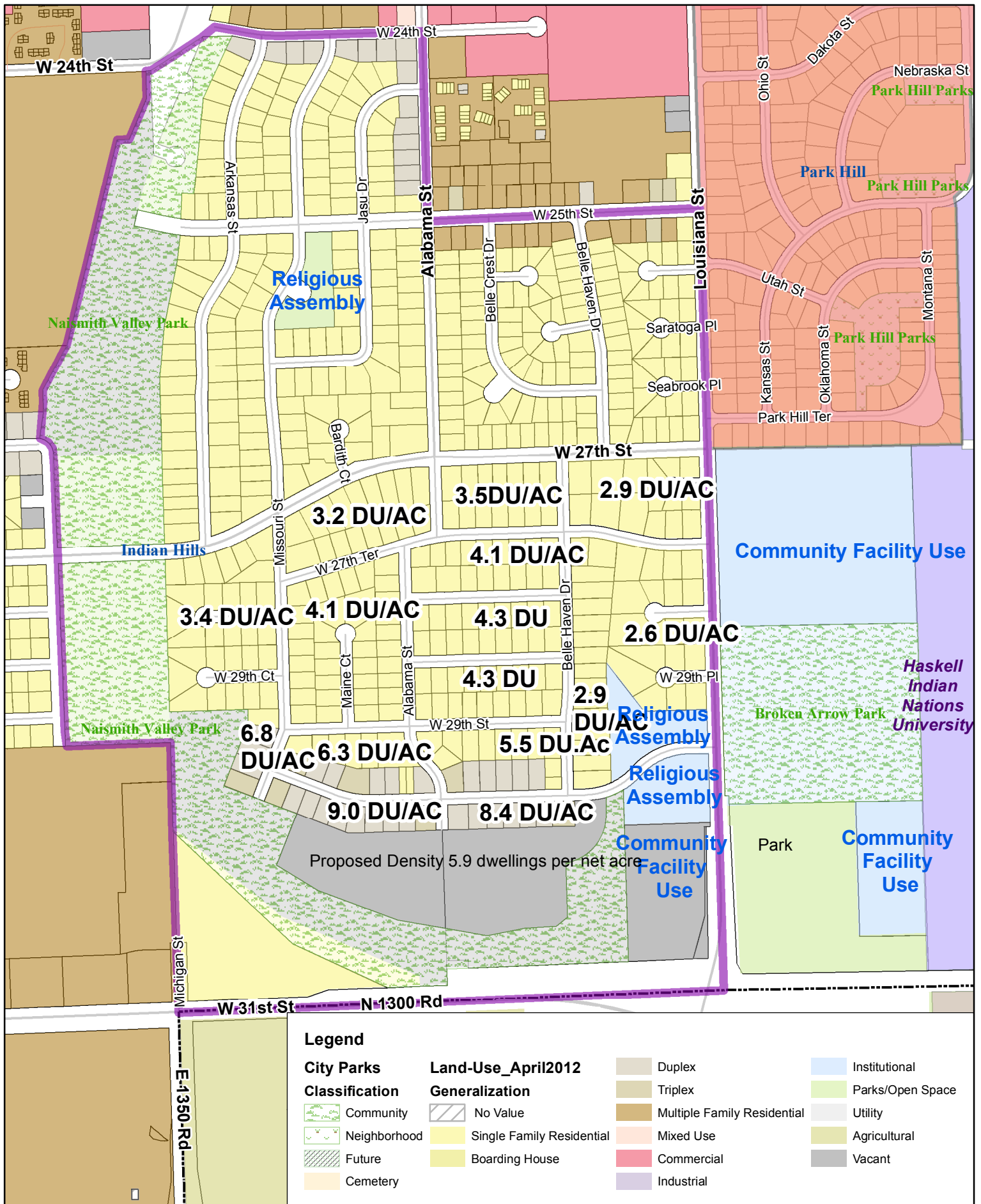




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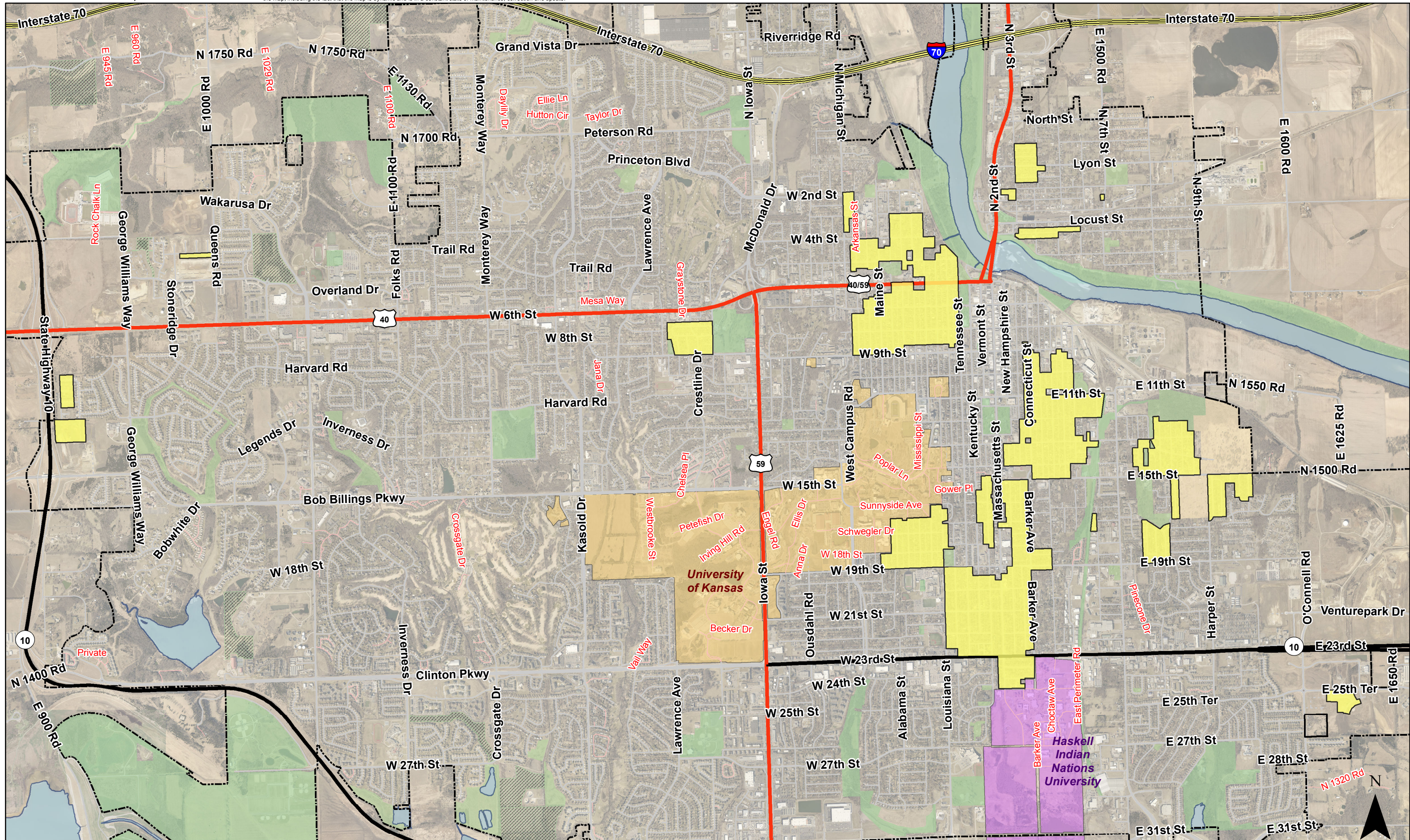
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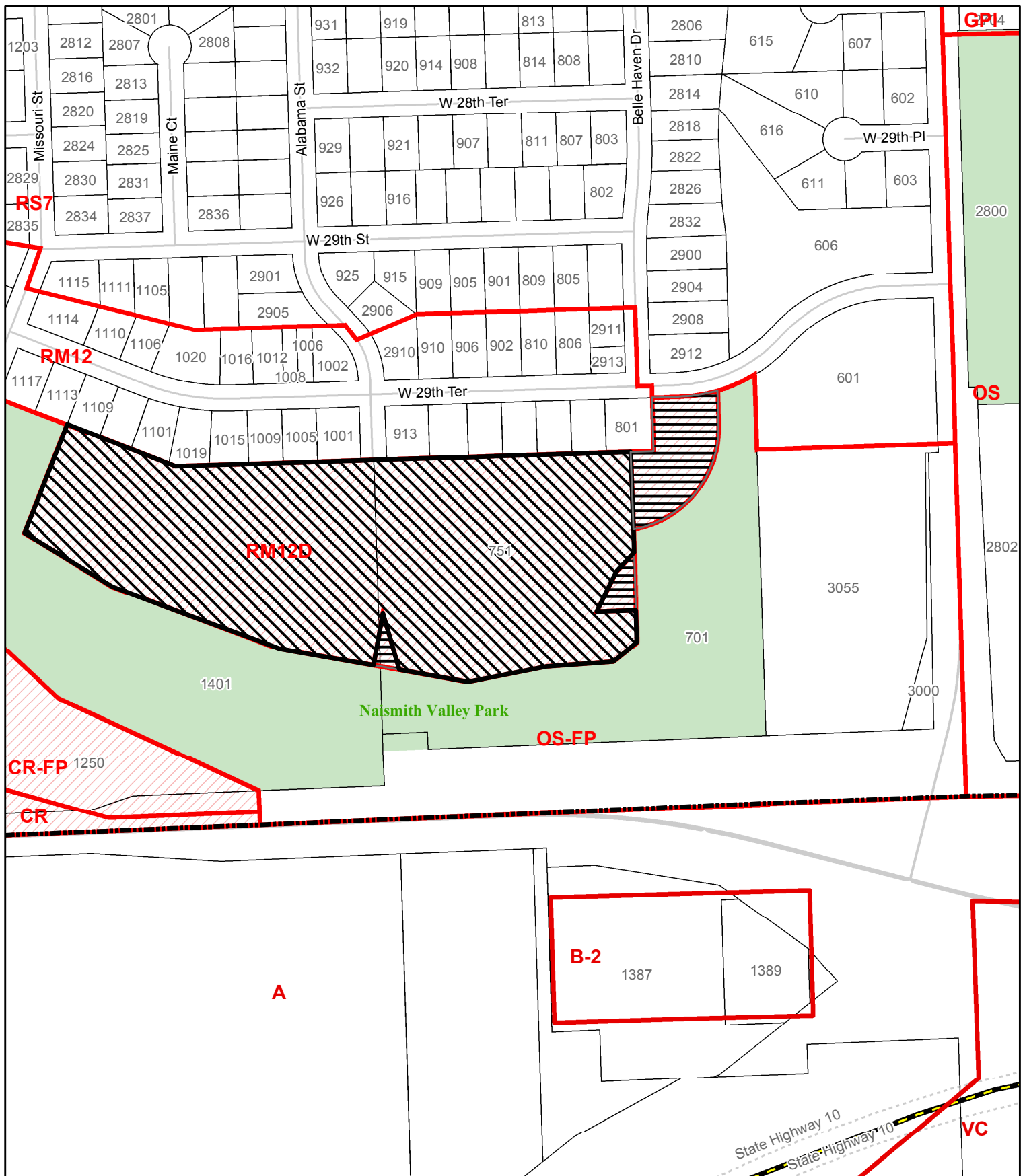
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Existing RS5 Zoning Districts





Z-16-00219: Rezone Approximately 13.268 acres from RM12D District to RS5 District
Z-16-00219: Rezone Approximately 1.262 acres from RM12D-FP District to RS5-FP District
Located at 751 W 29th Terrace



PLANNING COMMISSION REPORT
Regular Agenda - Public Hearing Item

PC Staff Report
07/25/2016

**ITEM NO. 5B RM12D-FP (MULTI-DWELLING RESIDENTIAL FLOODPLAIN OVERLAY)
DISTRICT TO RS5-FP (SINGLE-DWELLING RESIDENTIAL FLOODPLAIN
OVERLAY) DISTRICT; 1.262 ACRES (SLD)**

Z-16-00220: Consider a request to rezone approximately 1.262 acres from RM12D-FP (Multi-Dwelling Residential-Floodplain Overlay) District to RS5-FP (Single-Dwelling Residential-Floodplain Overlay) District, located at 751 W 29th Terrace. Submitted by Landplan Engineering, for Savannah Holdings, LC, property owner of record.

STAFF RECOMMENDATION: Staff recommends approval of the request to **rezone** approximately 1.262 Acres, from RM12D-FP to RS5-FP (Single-Dwelling Residential Floodplain Overlay) District based on the findings presented in the staff report and forwarding it to the City Commission with a recommendation for approval.

Reason for Request: *This rezoning request facilitates the development of single-family detached homes on the subject property. It keeps property currently within a floodplain overlay district in that same overlay district but adjusts the base district from RM12D to RS5 to keep it consistent with the concurrent RS5 rezoning request.*

KEY POINTS

- Planned residential development.
- Property includes small areas encumbered by regulatory floodplain.

ASSOCIATED CASES/OTHER ACTION REQUIRED

- A-13-00437; Annexation of 14.53 Acres, Ord. No. 8953
- Z-13-00348; A to RM12D and RM12D-FP, Ord. No. 8954
- Z-16-00219; RM12D to RS5
- PP-16-00221; Naismith Creek Addition

PLANS AND STUDIES REQUIRED

- *Traffic Study* – Not required for rezoning
- *Downstream Sanitary Sewer Analysis* – not required for rezoning
- *Drainage Study* – Not required for rezoning
- *Retail Market Study* – Not applicable to residential request

ATTACHMENTS

- Area Map
- Floodplain Map

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

-

Project Summary:

This request would amend the zoning district affecting only the base zoning of the existing RM12D-FP. The request would retain the floodplain overlay district designation if approved. This application was submitted concurrently with a rezoning application and preliminary plat for the Naismith Creek Addition.

The summary of the staff report is identical to the summary provided for Z-16-00219 except for references to the floodplain. New or additional text related specifically to the Floodplain Overlay District is included in this application.

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

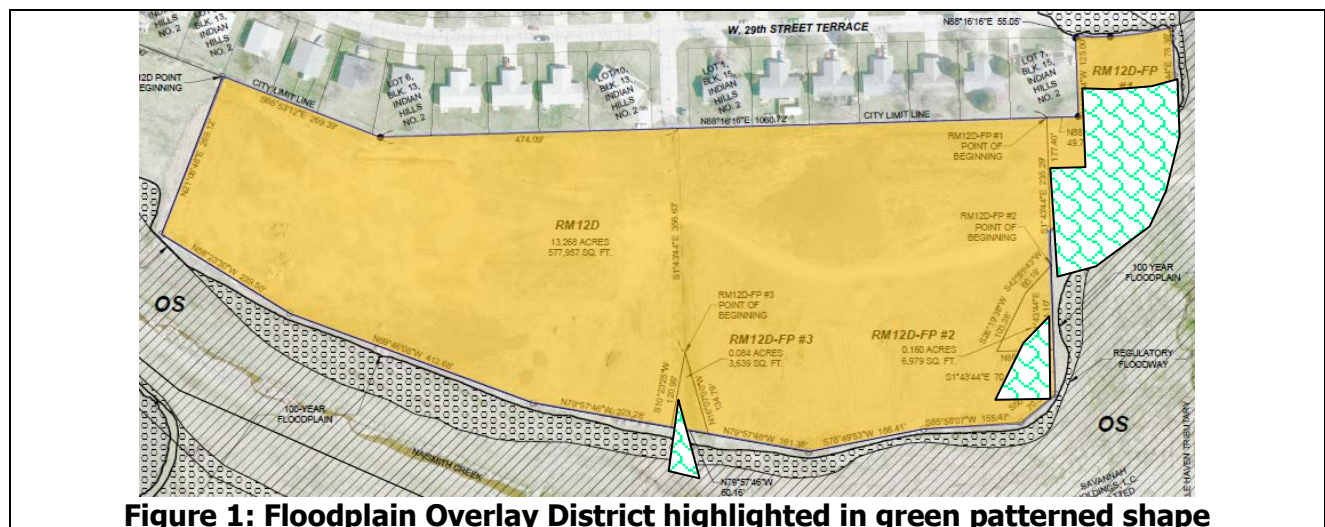
Applicant's Response: *This request conforms to the portion of Chapter 16 of Horizon 2020 which addresses water resource management. Maintaining the existing floodplain overlay district in these small pockets of property ensures that future residential construction will adhere to the applicable floodplain management development standards for property lying less than 2 vertical feet above adjacent FEMA base flood elevations.*

The original annexation and zoning of the area in 2013 included an identification of the boundary of the floodplain and resulted in certain additional areas needing to be included in the Overlay District. Much of the area annexed that was encumbered by the regulatory floodplain was transferred to the City of Lawrence and expanded the boundary of Naismith Valley Park. Only a small area of the floodplain remains within the development area.

Horizon 2020: Chapter 16 Environment:

This chapter of *Horizon 2020* addresses several natural environmental issues including Water Resource Management. The plan states that floodplain areas should be protected. This application includes a Floodplain Overlay District designation that will be applied to specific areas of the subject property as required by the Land Development Code.

The following graphic highlights the portions of the subject property that will carry the dual RS5 and FP Overlay zoning designations.



In addition to the discussion included in staff report Z-16-00219 residential goals address the preservation of the natural environmental features. The proposed application is intended to continue the protections originally established for the area related to the floodplain for newly annexed areas (Residential Policy 5.2).

Staff Finding – The proposed rezoning request conforms with *Horizon 2020* policies related to floodplain development.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING OVERLAY ZONING

The following zoning and land use summary is provided for the entire area included in the Naismith Creek Addition development request. This specific application affects only the area that includes the Floodplain Overlay District to modify the base district and retain the Floodplain Overlay District designation.

Current Zoning and Land Use:	RM12D-FP (Multi-Dwelling Residential Floodplain Overlay) District. Undeveloped land.
Surrounding Zoning and Land Use:	<p>RM12 (Multi-Dwelling Residential) District to the north; Existing development includes both duplex and triplex residential units.</p> <p>RS7 (Single-Dwelling Residential) District to the north and northeast east of Belle Haven Drive. Existing uses include a <i>Detached Dwelling</i> and a <i>Religious Assembly</i> use.</p> <p>OS-FP (Open Space Floodplain Overlay) District to the east, west, and south. Existing Naismith Valley Park.</p>

The subject property includes only a portion of the area included in the related preliminary plat application.

Staff Finding – The proposed request is directly related to the request for RS5 zoning and proposed preliminary plat for Naismith Creek Addition. Approval of the request would facilitate a uniform development of the property. The surrounding area to the south and east is also designated with the Floodplain Overlay District designation and is incorporated into Naismith Valley Park. Residential uses to the north are a mix of detached and attached dwellings.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response: *The subject site is bounded to the north by the Indian Hills Neighborhood, a predominantly residential neighborhood. The properties immediately to the north were developed with duplexes in the late 1960s and are zoned RM12. Development north of W 29th Terrace consists of single-family detached homes and is zoned RS7. Property to the east, south and west is zoned Open Space with floodplain overlay district (OS-FP) and is owned by the City of Lawrence. The property to the east across Belle Haven Tributary is currently being developed by the City as the site of the new Pump Station No. 10. Also to the east is a pair of churches, both zoned RS7. The property to the west and south is part of Naismith Valley Park. Lying further to the south and west is property owned by Menards and the very eastern edge of the South Iowa Commercial Corridor.*

In addition to the character of the neighborhood described in the staff report Z-16-00219 the neighborhood includes area designated as part of the regulatory floodplain. Most of the floodplain is contained within Naismith Valley Park. Small portions of the floodplain extend into the existing and proposed residential development of the neighborhood. Upon annexation certain additional areas were required to be rezoned to also include the floodplain overlay designation. This application seeks to retain that portion of the property that was originally designated as part of the overlay district while the base district is changed from RM12D to RS5.

Staff Finding – The proposed request is compatible with the existing development pattern.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

This property is included in the revised Southern Development Plan boundary. The property is also located within the Indian Hills Neighborhood. There is no adopted plan for the Indian Hills Neighborhood. The Revised Southern Development Plan identifies this property as suitable for medium-density residential (7-15 dwelling units per acre). Zoning districts consistent with this land use recommendation include the *Multi-Dwelling Residential Districts* RM12D, RM12 and RM15. Applicable medium-density *Detached Residential Districts* are RS3 and RS5. The proposed request is consistent with the planned development of the area in terms of residential use.

Staff Finding – The proposed request is consistent with the planned development of the area in terms of residential use.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response: *The subject property is currently zoned RM12D-FP. This rezoning request is only being made to complement a concurrent rezoning request which proposes to change adjacent property from RM12D to RS5.*

The suitability of this application as it relates to the proposed base zoning district, RS5, is discussed in the staff report for Z-16-00219. This application will retain the FP designation for the area within the development as required by the City Code while amending the base district from RM12D to RS5 consistent with the related applications for this property.

Staff Finding – The proposed request is suitable for subject property.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response: *The property has been vacant, at least since purchased by the current owner in 1973.*

This property is undeveloped and has been zoned A (Agricultural) since 1966. The property was annexed and rezoned to RM12D in 2013.

Staff Finding – This property is undeveloped and has been zoned A (Agricultural) since 1966 and RM12D since 2013.

7. EXTENT TO WHICH APPROVING THE REZONING WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicant's Response: *This rezoning will not result in any significant detriment to nearby properties. The request will facilitate residential development that adheres to the City's floodplain management development standards. In doing so, this request helps to ensure preservation of the floodway and floodway fringe associated with the nearby Naismith creek and Belle Haven Tributary.*

Denial of the request would result in some lots having split zoning making development of the lots challenging. The following image highlights the proposed residential lots affected by this zoning (proposed RS5-FP).

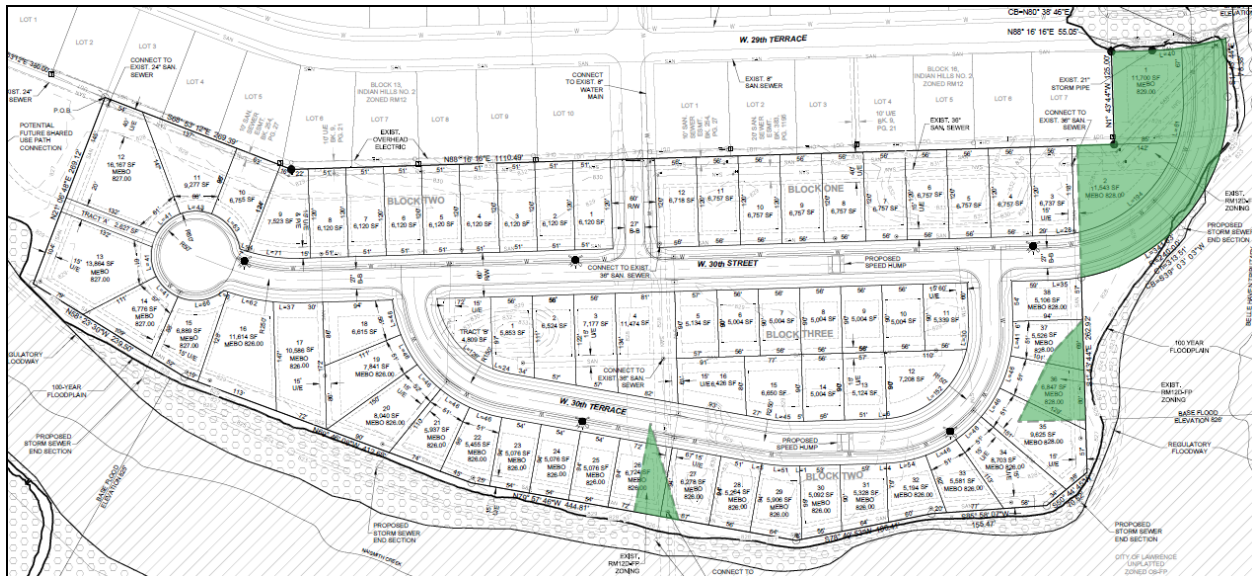


Figure 2: Lots with FP Overlay District Designation

As discussed in Z-16-00219 the base zoning RS5 is compatible with the surrounding residential and open space land uses. The FP (Floodplain Overlay) District designation must be extended to the original property regardless of the base zoning district.

Staff Finding – Staff concurs with the applicant that the proposed change will not result in detrimental impacts to the adjacent properties.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicant's Response: *Approval of this request facilitates the code-compliant residential development in the vicinity of floodplain as recommended in Horizon 2020. Denial of this request will perpetuate the vacancy of this land.*

Evaluation of this criterion includes weighing the benefits to the public versus the benefit of the owners of the subject property. Benefits are measured based on anticipated impacts of the rezoning request on the public health, safety, and welfare.

Either the existing or the proposed zoning district will facilitate the infill development of the area within the Indian Hills neighborhood. This application is consistent with a trend to develop detached housing over attached housing. This phenomenon is also occurring in the Langston Heights subdivision in West Lawrence. The Floodplain Overlay District is a continuation of the development code requirements from the original annexation. The applicant may in the future seek to amend the map boundaries and then rezone the property out of the overlay district based on a future floodplain development permit and related map amendment.

Staff Finding – While either the existing or the proposed zoning will meet the public needs as they relate to infill development and extending housing choices within an established neighborhood the proposed zoning is more consistent with the residential development trends of the neighborhood and the community. The FP (Floodplain Overlay) District zoning provides continuity for applying the Development Code requirements to this development.

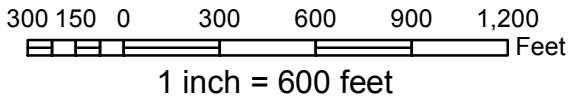
9. PROFESSIONAL STAFF RECOMMENDATION

This application must be considered concurrently with the request for the development area described in staff report Z-16-00219. If one application is approved (or denied) then both should be approved (or denied). This will result in a uniform base zoning district for the entire development and a continuation of the floodplain overlay district zoning for the applicable portion of the development property.

Staff recommends approval of the proposed rezoning.

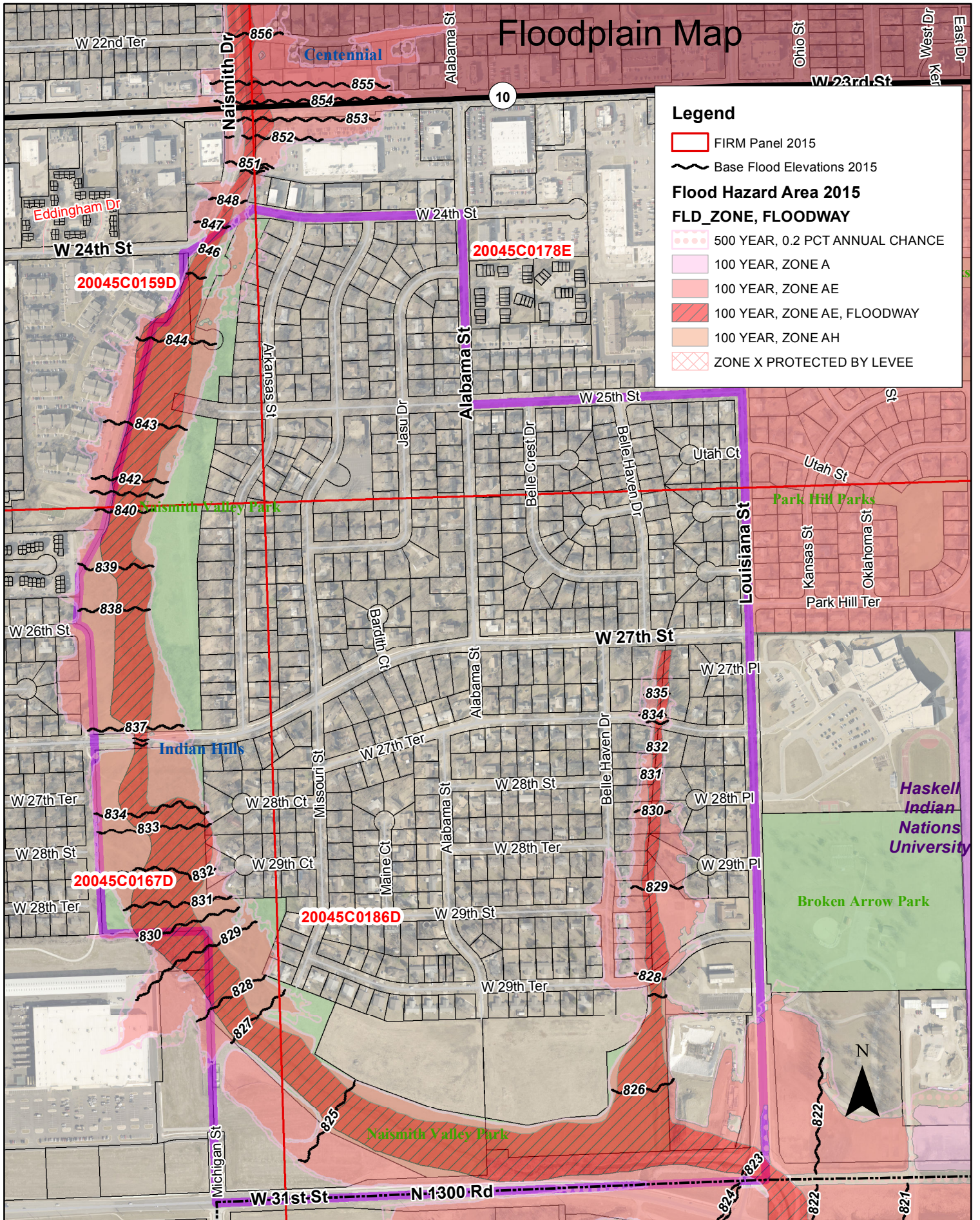
CONCLUSION

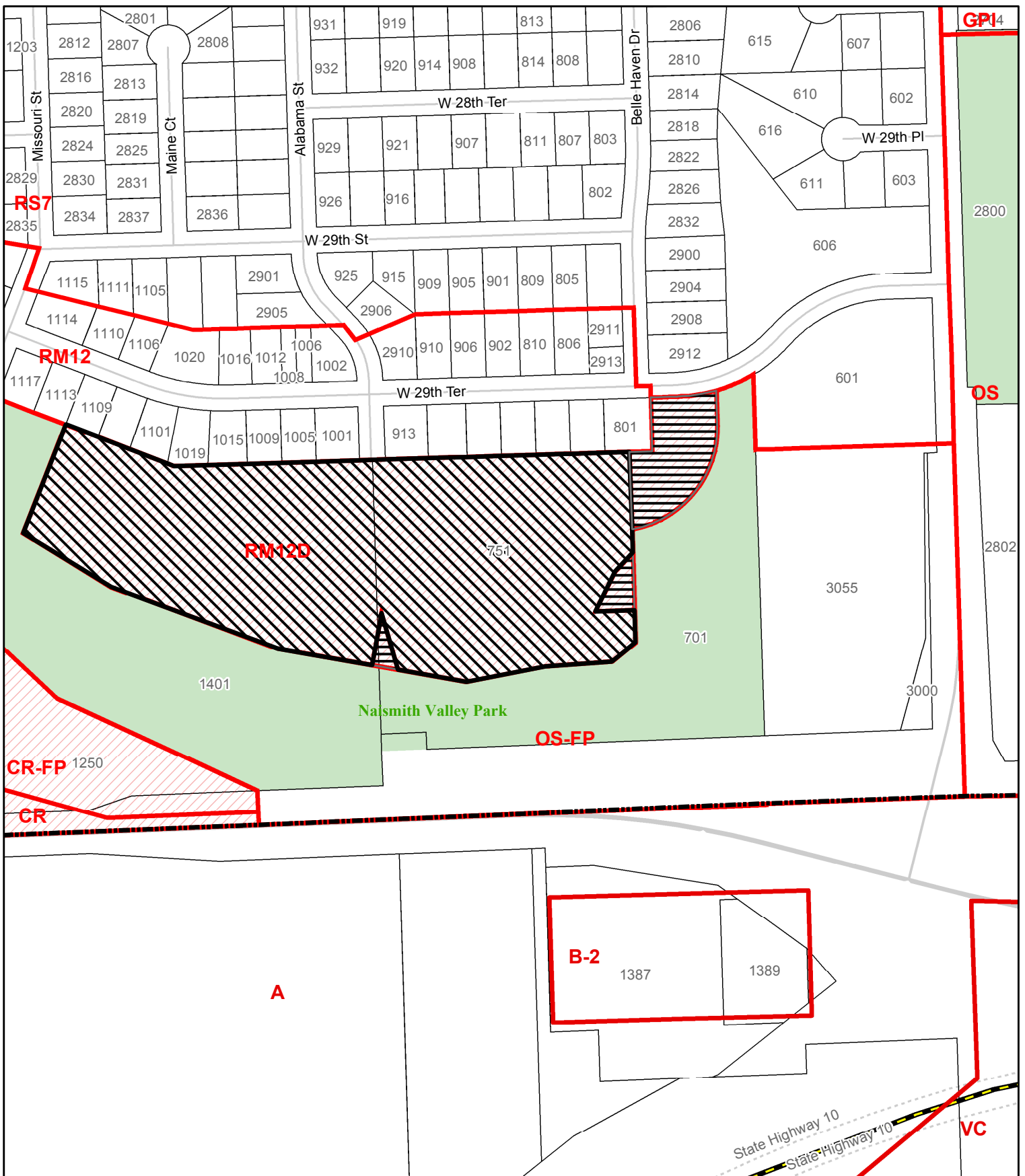
This application is submitted concurrently with a rezoning application for RS5 zoning (Z-16-00219) and a preliminary plat (PP-16-00221). The proposed request continues the intent for medium density residential development but alters the housing type from duplex housing to detached housing and continues the required floodplain overlay zoning designation. The proposed request is consistent with the planned development of the area.



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Date: 7/5/2016





Z-16-00219: Rezone Approximately 13.268 acres from RM12D District to RS5 District
Z-16-00219: Rezone Approximately 1.262 acres from RM12D-FP District to RS5-FP District
Located at 751 W 29th Terrace



**PLANNING COMMISSION REPORT
NON PUBLIC HEARING ITEM**

PC Staff Report
07/25/14

**ITEM NO. 5C PRELIMINARY PLAT FOR NAISMITH CREEK ADDITION; 751 W 29TH
TERR (SLD)**

PP-16-00221: Consider a Preliminary Plat for Naismith Creek Addition, for 66 single-dwelling residential lots, located at 751 W 29th Terrace. The subdivision will take access from Alabama Street and W 29th Terrace east of Belle Haven Drive. Submitted by Landplan Engineering, for Savannah Holdings, LC, property owner of record.

STAFF RECOMMENDATION:

Staff recommends approval of the Preliminary Plat for Naismith Creek Addition subject to the following conditions:

1. Provision of a revised Preliminary Plat with the following notes and changes:
 - a. Differentiate between existing and proposed contours lines (indicate where existing topography is changing) per the approval of the City Stormwater Engineer.
 - b. Show additional traffic calming per the City Traffic Engineer's approval.

Reason for Request: Pre-development requirement.

KEY POINTS

- Platting required as pre-development step.
- Application submitted concurrently with rezoning applications for detached residential development.
- Subdivision includes the creation of new public streets.

SUBDIVISION CITATIONS TO CONSIDER

- This application is being reviewed under the Subdivision Regulations for Lawrence and Unincorporated Douglas County, effective Jan 10, 2012.

ATTACHMENTS

Attachment A: Preliminary Plat
Attachment B: Plot Plan Concept

ASSOCIATED CASES/OTHER ACTION REQUIRED

Associated Cases

- A-13-00437; Annexation of 14.53 Acres, Ord. No. 8953
- Z-13-00348; A to RM12D and RM12D-FP, Ord. No. 8954
- Z-16-00219; RM12D to RS5
- Z-16-00220; RM12D-FP to RS5-FP

Other Action Required:

- Submittal of final plat for administrative approval and recordation.
- City Commission acceptance of dedication of easements and rights-of-way on the Final Plat.

- Submittal and approval of public improvement plans and provision of means of assurance of completion shall be submitted prior to the recording of the Final Plat.
- Submittal and approval of building plans prior to release of building permits for development.

PLANS AND STUDIES REQUIRED

- *Downstream Sanitary Sewer Analysis* – The concurrent rezoning application will reduce the density of the development from RM12D to RS5. The downstream sanitary sewer analysis and cover letter dated May 23, 2016 provided by Landplan Engineering has been reviewed and accepted for this project to satisfy the criteria required for the DSSA as outlined in Administrative Policy 76.
- *Drainage Study* – The Hydrologic & Hydraulic Study dated 5-23-2016 meets the specified requirements and is approved.
- *Traffic Study* – Accepted by Staff.

PUBLIC COMMENT

Public comments regarding preliminary fill activity has been reported to the Planning Office.

Site Summary	
Gross Area:	14.53 Acres
Additional Right-of-Way (acres):	3.86 Acres
Tracts A & B:	.170 Acres
Number of Proposed Lots:	66
Minimum lot size:	5,004 SF
Maximum lot size:	16,167 SF
Average lot size:	6,929 SF
Minimum Lot Area:	5,000 SF per RS5 Zoning District
Tract A:	2,627SF (pedestrian access to Naismith Valley Park)
Tract B:	4,809 SF
Net Area:	10.50 Acres
Density:	Gross Density: 4.5 DU/Acre [66 units/14.53 Acres] Net Density: 6.2 DU/Acre [66 units/10.50 Acres]

GENERAL INFORMATION

Current Zoning and Land Use:

RM12D (Multi-Dwelling Residential) District and RM12D-FP (Multi-Dwelling Residential Floodplain Overlay) District. Undeveloped land.

Surrounding Zoning and Land Use:

RM12 (Multi-Dwelling Residential) District to the north; Existing development includes both duplex and triplex residential units.

RS7 (Single-Dwelling Residential) District to the north and northeast east of Belle Haven Drive. Existing uses include a *Detached Dwelling* and a *Religious Assembly* use.

OS-FP (Open Space Floodplain Overlay) District to the east, west, and south. Existing Naismith Valley Park.

STAFF REVIEW

This property is proposed to be platted as 66-lot residential subdivision for detached residential development.

Zoning and Land Use

This application is submitted concurrently with two rezoning applications to accommodate detached residential development. The property is currently undeveloped and zoned for duplex development. The property is encumbered by regulatory floodplain.

Streets and Access

This proposed preliminary plat extends the street network to the south with proposed street connections to Alabama Street and W. 29th Terrace. The Preliminary Plat shows a proposed traffic calming device along W. 30th Street. Additional traffic calming is needed per the review of the City Traffic Engineer and is reflected as a condition of approval. These improvements will continue to be evaluated with the Final Plat and Public Improvement plans as they are submitted.

An additional non-motorized connection is provided from the end of the 30th Street cul-de-sac to the Naismith Valley Park, via Tract A. Each lot is accommodated with direct access to a public street.

Previous plans for this area included a connection either from the subdivision or around the subdivision to the east and intersecting Louisiana Street to the east. A more detailed review of the required stream crossings has concluded that this connection is not feasible. Access to the public park land is accommodated from the pedestrian connection to the west. Individual lots along the south side of the development will also have immediate access to the park property. There are no improved paths planned for the open space to the south of the development or to the east to connect to Louisiana Street.

Sidewalks

The proposed Subdivision includes new public sidewalks on both sides of streets within the subdivision. The following image highlights the existing and proposed public sidewalks in the immediate area.

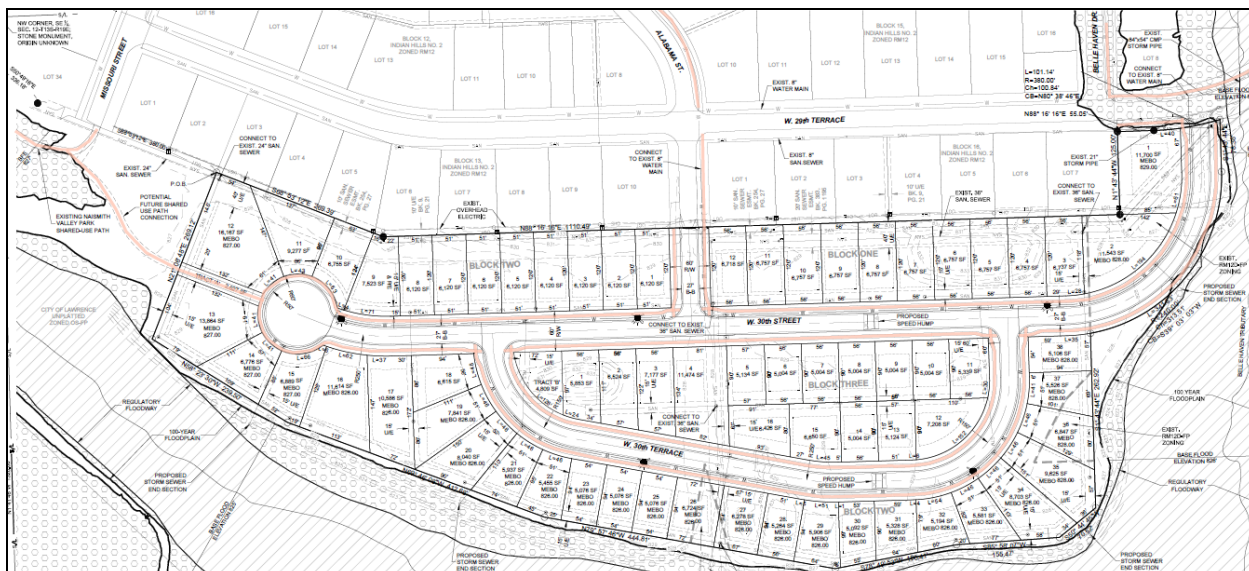


Figure 1: Sidewalk Map

Sidewalk construction and the recreation path connection will be required to be included with the future Public Improvement Plans for this residential development.

Utilities and Infrastructure

The following image shows the existing and proposed easements. Yellow represents new utility easements. Orange represents existing and exclusive easements within the proposed subdivision and on the immediate development to the north. This property is encumbered by existing utility easements that bisect the property north and south and along the north property line.

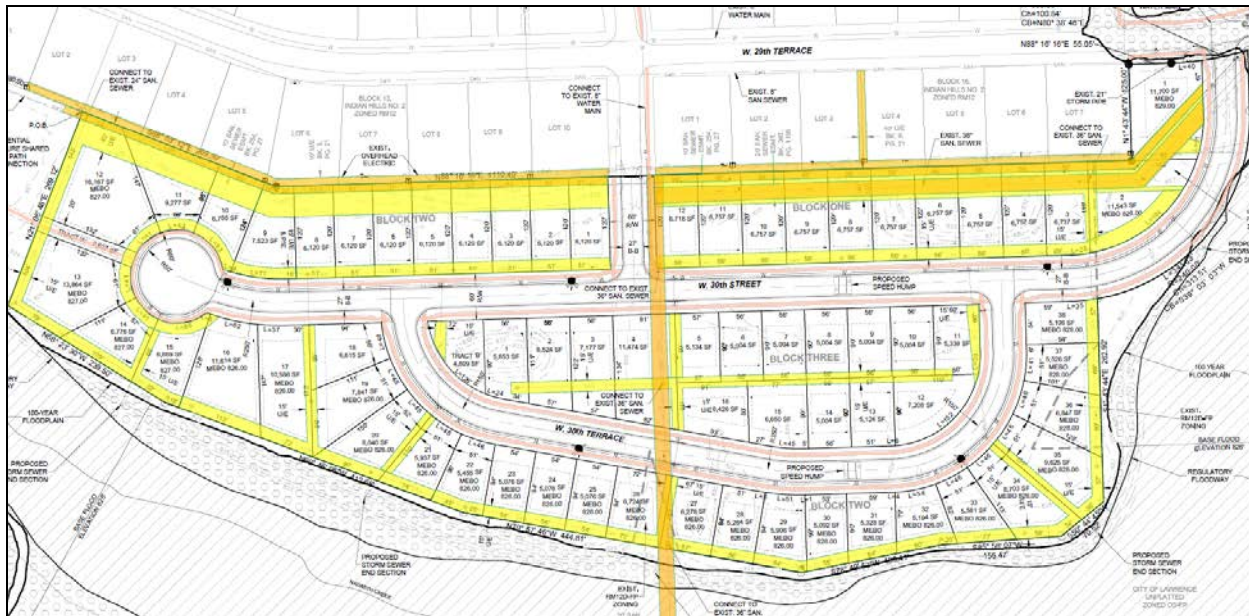


Figure 2: Utility Easement Map

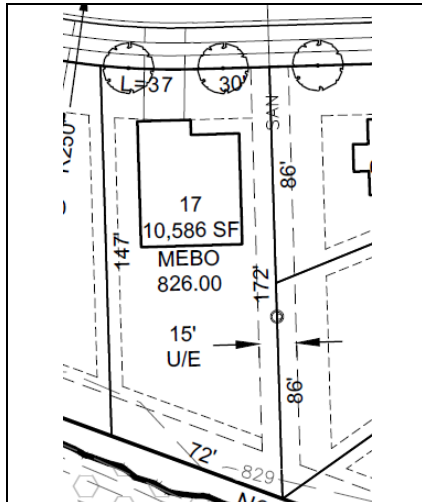
The city is working on the Naismith Valley interceptor sewer that will be installed along the north property line and result in the removal of Pump Station #8. The proposed preliminary plat shows a 40' wide utility easement along the north property line. The improvements have not been designed the easement width is based on a maximum possible improvement. Depending on the final design the easement could potentially be reduced. Staff has made the applicant aware of the design options and will continue to communication with the applicant/developer as design decisions are finalized. There is a possibility that the easement width could be reduced.

City Staff noted that the north portion of the property included existing "sanitary sewer easements." These easements are exclusive and do not accommodate any other utilities. The proposed preliminary plat shows a new 40' utility easement that would overlap the existing exclusive easements. City Utility Staff commented that the proposed 40' utility easement should be for the exclusive use for sanitary sewer. The proposed Preliminary Plat shows this exclusive easement.

A general utility easement located along the north side of W. 30th Street would allow for other utilities in addition to the proposed sanitary sewer along the front of the lots. This orientation typically has the effect of pushing street trees to the back of the utility easement. The proposed plat anticipates that street trees could be located between the sidewalk and the property line, within the street right-of-way. Maintaining adequate separation of trees and utilities will be required during utility construction and tree installation. Additional review of utility easements will be

included in the review of the Final Plat. Staff anticipates minor adjustments as public improvement plans are developed for the proposed subdivision.

City Utility staff has noted that structures must be located a minimum of 15' from the Sanitary Sewer and Water lines. The lot size and utility plan pose some challenge to this design requirement. The proposed subdivision shows typical utility easements centered on lot lines and the applicable front, side and rear setback or building envelop. Utilities located in the front or rear of a lot are typically not endanger of encroachment because the front and rear yard building setbacks are wider.



Block 2, Lot 17 Example:

- 15' utility easement centered on common property line.
- 7.5' easement on each half of lot line.
- Interior side lot line 5' setback.

To meet this standard an additional 7.5' setback would be required.

Assuming construction of sewer line on property line a minimum 15' setback on either side of the property line would be required to ensure minimum separation.

The structure could be located closer to the west property line to provide greater separation from the utility as an option.

As an alternative, the Development Code allows a structure to be setback 3' from an interior lot line when the sum of the two interior side yards equal the total required. In the above example, a 5' side yard setback is required, 5' on either side for a total of 10'. The building could be located on the lot so that it is 3' from the west property line and a minimum of 7' from the east property line. This example provides additional space from the sanitary sewer line while providing required setbacks. A larger building setback could be required along the east property line without changing the width of the easement. However, the plot plan included in this application is for conceptual purposes to demonstrate that each lot is buildable given the lot sizes and unique easements across some lots. These elements will continue to be reviewed with the Final Plat, Public Improvement Plans and building permit applications. The intent of including this discussion is to make public the challenge of building on small lots with extensive utility easements, but demonstrating that these lots can be buildable without variances.

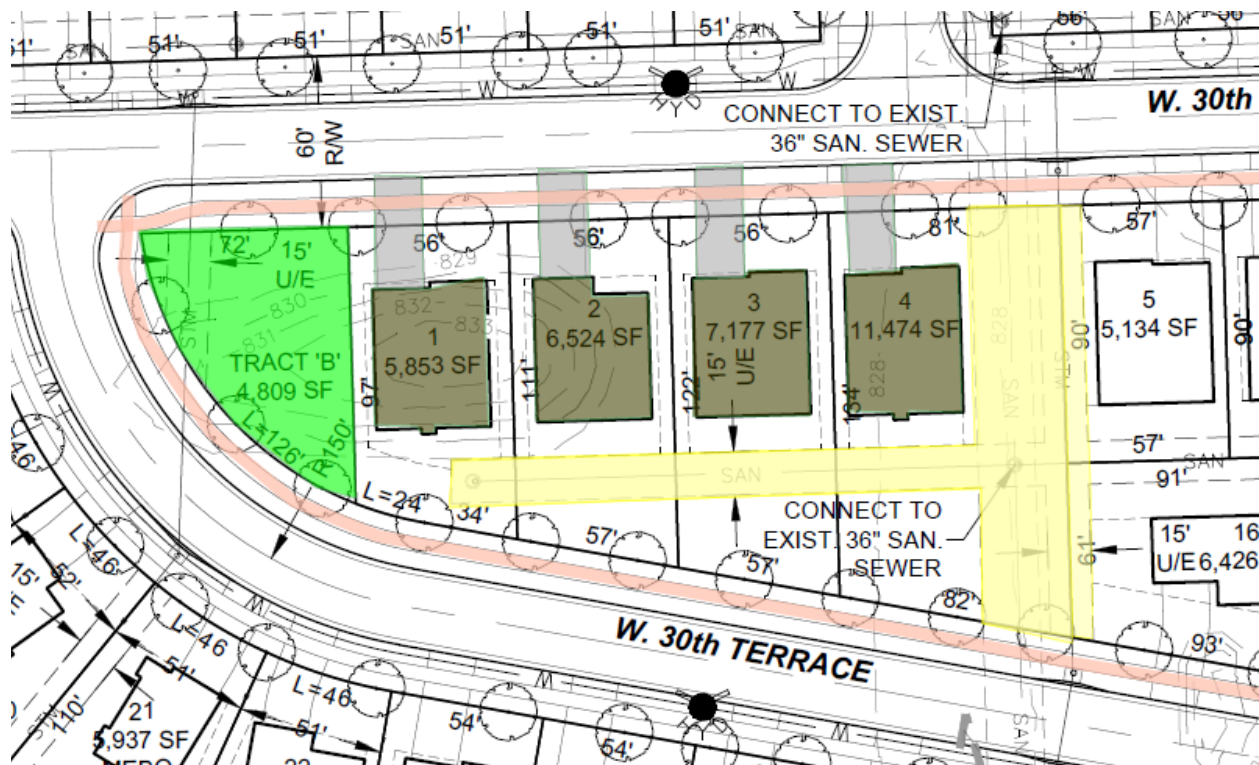
Additional review of specific elements related to utility infrastructure will continue to be evaluated as part of a future Final Plat and public improvement plan application. Items such as the location of manholes will be included in that future review. The intent is to minimize or prohibit conflicts such as manholes in driveways and in accessible locations. This same concern extends to the placement of certain stormwater utility elements. The City Utility Engineer and the City Stormwater Engineer have noted specific concerns in the proposed design that must be addressed as part of the final design of the subdivision. These elements should not impact the proposed lot or block arrangement as proposed.

Lot and Block Design

Subdivision design standards require that lots meet minimum lot area requirements of the base zoning district. This subdivision has been designed to accommodate development of lots with a minimum area of 5,000 SF per the proposed zoning request. The RS5 district allows a 40' lot width.

However, the code also requires lots that are less than 51' to share driveways. This proposed subdivision includes lots that meet or exceed the minimum lot width requirements in excess of 50' either at the frontage (along the right-of-way) or at the required front yard setback and are not required to provide shared driveways.

The configuration of the property as well as the encumbrance of existing infrastructure has resulted in the creation of 4 lots with "double frontage" or public streets along two sides resulting in a "through-lot." These lots differ from corner lots for obvious reasons. Building can be located on either street and access can be taken from either street. However the location of a utility easement across the south portion of the affected lots results in a probable development pattern with buildings facing W. 30th Street as shown on the plot plan attached to this report.



The proposed subdivision includes three blocks and two tracts. The subdivision is designed with sidewalks that provide pedestrian connections at appropriate locations to accommodate block length. Tract A, located at the west end of Block Two provides additional pedestrian connection to Naismith Park.

Development of Lot 1 and Lot 2, Block 1 has unique location requirements because much of the lots are encumbered by easement. The applicant provided a plat plan to demonstrate that a residence can be constructed compliant with required building setbacks given the restrictive nature of the easements.

Lots adjacent to all drainage easements and the regulatory floodplain are required to note the minimum habitable floor elevation of the structure. This information is shown on the face of the preliminary plat. Additionally the Preliminary Plat shows the required notation per section 20-809 (5) (ii) (c) that states: "If a basement is built on a lot where a minimum elevation has been established, the building design is encouraged to incorporate a sump pump."

Tracts A & B

Tracts A and B are intended to be retained in ownership by a homeowner's association. Tract A provides connection to Naismith Valley Park. Tract A should be further described as a pedestrian easement to clearly articulate that use. The sidewalk connection will be included with public improvement plans in the future and constructed with the initial infrastructure for the subdivision. Tract B is intended as common open space. Notes regarding the use and ownership of these tracts is provided on the face of the plat.

Conformance

The proposed preliminary plat conforms with the minimum required subdivision design standards. The future development will provide additional residential options within the established neighborhood network. The development will also provide additional connectivity to the public street, sidewalk and trail network for the surrounding area.

[illegible]

A TRACT OF LAND LOCATED IN THE SOUTHEAST QUARTER OF SECTION 00 TO NSHIP 00 SOUTH RANGE 00 EAST OF THE SIXTH PRINCIPAL MERIDIAN IN DOUGLAS COUNTY, KANSAS DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTH - EAST CORNER OF SAID SOUTHEAST - QUARTER OF SECTION 12, THENCE NORTH 01°41'46" WEST, ALONG THE WEST LINE OF SAID SOUTHEAST - QUARTER [REDACTED] FEET TO THE SOUTHEAST CORNER OF LOT [REDACTED] NAIMSMITH VALLEY NO. 1, A REPLAT OF A PART OF MEADO - LEA ESTATES, THENCE SOUTH 50°49'48" EAST, 336.18 FEET TO THE SOUTHWEST CORNER OF LOT [REDACTED] OF OLIVER & SONS TRACT, BEING THE CITY OF LAWRENCE, DOUGLAS COUNTY, KANSAS, THENCE SOUTH 88°53'12" EAST, ALONG THE SOUTHERLY LINE OF SAID ADDITION [REDACTED] FEET TO THE SOUTHEAST CORNER OF LOT [REDACTED] OF SAID ADDITION AND THE POINT OF BEGINNING, THENCE CONTINUING SOUTH 68°53'12" EAST, ALONG THE SOUTHERLY LINE OF SAID ADDITION [REDACTED] FEET TO THE ANGLE POINT ON THE SOUTH LINE OF LOT 6, BLOCK 13 OF SAID ADDITION, THENCE NORTH 88°16'16" EAST, ALONG THE SOUTH LINE OF SAID ADDITION A MEASURED DISTANCE OF [REDACTED] FEET TO THE SOUTHEAST CORNER OF LOT [REDACTED] OF SAID ADDITION, THENCE NORTH 88°16'16" WEST, ALONG THE WEST LINE OF SAID LOT [REDACTED] FEET TO THE SOUTH RIGHT - OF - WAY LINE OF EAST 7TH STREET TERRACE, THENCE NORTH 88°16'16" EAST, ALONG SAID SOUTH RIGHT - OF - WAY LINE [REDACTED] FEET, THENCE CONTINUING ALONG SAID SOUTH RIGHT - OF - WAY LINE ON A CURVE TO THE LEFT HAVING A RADIUS OF [REDACTED] FEET, AN ARC LENGTH OF 101.14 FEET, A CHORD BEARING NORTH 80°38'46" EAST AND A CHORD LENGTH OF 100.84 FEET, THENCE SOUTH 1°43'24" EAST, 78.38 FEET, THENCE ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF [REDACTED] FEET, AN ARC LENGTH OF 341.63 FEET, A CHORD BEARING SOUTH 39°03'03" WEST AND AN ARC LENGTH OF 341.63 FEET, THENCE NORTH 88°16'16" WEST, 269.12 FEET, THENCE SOUTH 50°44'45" WEST, 336.18 FEET, THENCE SOUTH 88°50'07" WEST, 155.57 FEET, THENCE SOUTH 78°49'53" WEST, 186.41 FEET, THENCE NORTH 79°47'46" WEST, 444.81 FEET, THENCE NORTH 69°46'08" WEST, 412.68 FEET, THENCE NORTH 88°23'30" WEST, 239.50 FEET, NORTH 21°06'48" EAST, 269.12 FEET TO THE POINT OF BEGINNING. CONTAINS [REDACTED] ACRES MORE OR LESS

O: NER SAVANNAH HOLDINGS: LC
 : BLUESTEM DRIVE
 LA: RENCE ANAS

LAND PLANNER LANDPLAIN ENGINEERING: P: A
 CIVIL ENGINEER : ARUSA DRIVE
 SURVEYOR LA: RENCE ANAS

TOPOGRAPHIC INFORMATION SHOWN AS OBTAINED FROM A FIELD SURVEY PERFORMED BY LANDPLAIN ENGINEERING: P: A: MAY
 E: ISTING ONING: RM: D RM: D FP
 PROPOSED ONING: RS: RS: FP
 E: ISTING LAND USE: VACANT
 PROPOSED LAND USE: LO: DENSITY RESIDENTIAL

A PORTION OF THE SUB: ECT PROPERTY DOES LIE WITHIN A DESIGNATED "SPECIAL FLOOD HAZARD AREA" AS DEFINED BY FLOOD INSURANCE RATE MAP: FIRM: PANEL: N: MAP NUMBER: DOUGLAS COUNTY: ANAS: BEARING AN EFFECTIVE DATE OF SEPTEMBER

TYPICAL SOIL TYPES: ABASH SILTY CLAY LOAM: OCCASIONALLY FLOODED: ABASH SILTY CLAY: OCCASIONALLY FLOODED: SIBLEYVILLE LOAM: TO: SLOPES:
 PROPOSED UTILITY LOCATIONS AND SIZES ARE PRELIMINARY AND WILL BE FINALIZED AT THE TIME OF SITE ENGINEERING

DEVELOPER IS RESPONSIBLE FOR THE COST OF ANY RELOCATION OF E: ISTING UTILITIES: IF NECESSARY TO SERVE THE PROPOSED SUBDIVISION

LANDSCAPING SHALL NOT BE PLACED WITHIN UTILITY EASEMENTS: ITHIN: FEET OF THE AFTER MAIN: OR: ITHIN: FEET OF THE SANITARY SE: ER MAIN:
 ALL UTILITIES: WILL BE LOCATED UNDERGROUND: PER SECTION: OF THE CITY CODE:

IF A BASEMENT IS BUILT ON A LOT: HERE A MINIMUM ELEVATION HAS BEEN ESTABLISHED: THE BUILDING DESIGN IS ENCOURAGED TO INCORPORATE A SUMP PUMP: A FLOODPLAIN DEVELOPMENT PERMIT SHALL BE INITIATED BY ANY PERSON: FIRM: CORPORATION OR UNIT OF GOVERNMENT PROPOSING ANY CONSTRUCTION: SUBSTANTIAL IMPROVEMENT OR OTHER DEVELOPMENT IN THE FLOODPLAIN OVERLAY

DISTRICT BY THE FILING OF AN APPLICATION IN WRITING ON A FORM FURNISHED FOR THAT PURPOSE WITH THE FLOODPLAIN ADMINISTRATOR.

THE BASIS OF THE BEARINGS FOR THIS PLAT IS ANSAS STATE PLANE NORTH.

TRACTS A AND B WILL BE PRIVATELY OWNED AND MAINTAINED BY A HOMEOWNERS ASSOCIATION. THE FINAL PLAT WILL DEDICATE TRACT A AS A PEDESTRIAN EASEMENT IN ORDER TO FACILITATE A CONNECTION TO THE NAIMSITH VALLEY PARK SHARED USE PATH. TRACT B WILL PROVIDE COMMON OPEN SPACE.

GROSS AREA:	000000	SF	000000	AC
RIGHTS OF WAY	000000	SF	000000	AC
TRACTS	000000	SF	000000	AC
NET AREA	000000	SF	000000	AC
TOTAL LOTS	00			
AVERAGE LOT SIZE	000000	SF	000000	AC
MINIMUM LOT AREA	000000	SF	000000	AC
MAXIMUM LOT AREA	000000	SF	000000	AC

BLOCK ONE

NET AREA					SF					AC
TOTAL LOTS										
AVERAGE LOT SIZE					SF					AC
MINIMUM LOT AREA					SF					AC
MAXIMUM LOT AREA					SF					AC

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BM " " ON SOUTH RIM SAN MANHOLE SE

THE SUBDIVISION WILL INCLUDE THE DEDICATION OF RIGHTS OF WAY ASSOCIATED WITH ALL INTERNAL ROADS SHOWN

- THE SUBDIVISION ☐ WILL INCLUDE THE DEDICATION OF RIGHTS OF ☐ AY ASSOCIATED ☐ WITH ALL INTERNAL ROADS SHOWN ☐ ON THE MAP.
- ☐ THE SUBDIVISION ☐ WILL PROVIDE CONNECTIONS TO THE CITY OF LA ☐ RENCE ☐ AFTER SERVICE VIA AN E ☐ ISTING ☐ INCH MAIN LOCATED IN THE EAST HALF OF THE ALABAMA STREET RIGHT OF ☐ AY AND AN E ☐ ISTING ☐ INCH MAIN LOCATED IN THE NORTH HALF OF THE ☐ ☐ TERRACE RIGHT OF ☐ AY ☐.
- ☐ THE SUBDIVISION ☐ WILL PROVIDE CONNECTIONS TO THE CITY OF LA ☐ RENCE SANITARY SE ☐ ER SYSTEM VIA AN E ☐ ISTING ☐ INCH MAIN LOCATED IN A DEDICATED SANITARY SE ☐ ER EASEMENT ☐ WITHIN THE BOUNDARY OF THE SUBDIVISION AND AN E ☐ ISTING ☐ INCH MAIN LOCATED IN A DEDICATED SANITARY SE ☐ ER EASEMENT ADJACENT TO THE NORTH PROPERTY LINE OF THE SUBDIVISION ☐.
- ☐ PURCHASERS OF THE LOTS IN THE SUBDIVISION ☐ WILL NOT BE SUBJECT TO SPECIAL ASSESSMENTS OR OTHER COSTS OF STREETS, ROADS ☐ AFTER LINES AND TREATMENT ☐ AND OR ☐ ASTE ☐ AFTER LINES AND TREATMENT ☐.
- ☐ THE PROVISION OF IMPROVED ROADS ☐ AFTER SERVICE AND OR ☐ ASTE ☐ AFTER SERVICE ☐ WILL NOT DEPEND IN ANY ☐ AY ON A VOTE ☐ PETITION OR OTHER COLLECTIVE ACTION OF PROPERTY ☐ OWNERS IN THE SUBDIVISION ☐.

SCALE: 1" = 60'



REV	DATE	DESCRIPTION
1	6/1/16	LOT DIMS, MEGOS
2	6/30/16	PER DEPT. COMMENTS
3	7/15/16	PER DEPT. COMMENTS

DATE:	5/23/16
PROJECT NO.:	20163015
DESIGNED BY:	LPE
DRAWN BY:	BS
CHECKED BY:	BZ

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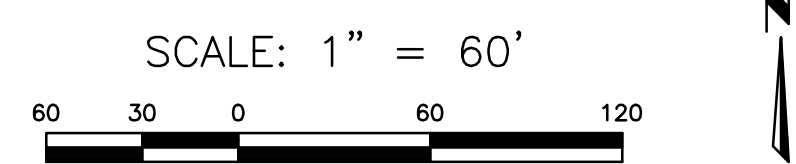
1310 Wakarusa Drive, Suite 100
Lawrence, Kansas 66044
785.843.7550(p) | 785.843.2410(f)
info@landplan-pa.com | www.landplan-pa.com

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**715 w. 29th TERRACE
LAWRENCE, KANSAS 66046**

**A PRELIMINARY PLAT FOR
NAISMITH CREEK ADDITION**

FILE NAME: V:\2016\3012\2\CAD\Planning\pp\163012C-PP.dwg LAST SAVED BY: Brian Sturm SAVED DATE: 7/13/2016 5:01 PM PLOTTED: 7/15/2016 9:06 AM



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Lawrence, KS • Kansas City, MO • The Woodlands, TX

1310 Wakarusa Drive, Suite 100
Lawrence, Kansas 66049
785.843.7530 (p) | 785.843.2410 (f)
info@landplan-pa.com | www.landplan-pa.com

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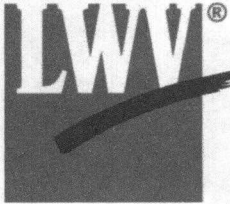
**715 W. 29th TERRACE
LAWRENCE, KANSAS 66046**

**A BUILD OUT PLAN FOR
NAISMITH CREEK ADDITION**

REV	DATE	DESCRIPTION
1	5/17/16	LOT DIMS, MEBO'S
2	6/30/16	PER DEPT. COMMENTS
3	7/15/16	PER DEPT. COMMENTS

DATE: 5/23/16
PROJECT NO.: 20163015
DESIGNED BY: LPE
DRAWN BY: BS
CHECKED BY: BZ

ISSUE	SHEET NO.
	2
	OF 1 SHEETS



LEAGUE OF WOMEN VOTERS®
OF LAWRENCE/DOUGLAS COUNTY

July 24, 2016

RECEIVED

JUL 25 2016

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Lawrence, Kansas

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Vice-President
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Kim Gibson

Midge Grinstead

Carol Klinknett

Ann Carlin Ozegovic

Melissa Wick

To: Mr. Patrick Kelly, Chair, and Planning Commission Members
Lawrence-Douglas County Metropolitan Planning Commission

RE: ITEM NO. 5C: PRELIMINARY PLAT FOR NAISMITH CREEK ADDITION; 751
W 29TH TERRACE

The League of Women Voters of Lawrence-Douglas County is concerned about the proposal for Naismith Creek Addition, which would create 65 single-dwelling residential lots on approximately 15 acres adjacent to Naismith Creek. One of our important Environmental Positions is that "development should avoid hazardous building sites such as unstable slopes, uncompacted or undesignated fill, or areas subject to flooding."

Our concern is with potential flooding of homes that would be built there. Neighbors remember many times when this area has been flooded. In 1993, water was released from Clinton Lake to keep it from over-topping the emergency spillway. This raised the level in the Wakarusa River, thereby closing the flap gates at the south end of the drainage ditch along the west side of the original Baker Wetlands. That resulted in having no outlet for the storm water coming off the KU campus, from the neighborhood and from the commercial development along 23rd Street. Several cars and trucks parked along Belle Haven Drive had water above their tires. If these circumstances occur again, worse flooding will probably happen. Additional impervious surface would be created in the form of rooftops and driveways, which would increase runoff. In anticipation, there is a notation on the plat that if basements are going to be used, it is suggested to install sump pumps.

The plat does not comply with Subdivision Design regulations, which require that a drainage plan be shown. The plat should show "Location of any area in the Floodplain Overlay District, locations and direction of flow of all water courses; and base flood elevation at water course entrances to and exits from the proposed Subdivision."

We think it would be desirable to leave this site as open space. If there is to be development, there should be a buffer between the creek and the buildings.

Thank you for considering our letter.

Sincerely yours,

Marlene Merrill
Co-President

Alan Black, Chairman
Land Use Committee

PO BOX 1072 • LAWRENCE KS 66044-1072

lawrenceksleague@gmail.com • www.lawrenceleague.com
www.facebook.com/lwvldc • www.twitter.com/lwvldc

July 24, 2016

To: Chairman Patrick Kelly, Members of the Lawrence/Douglas County Planning Commission

From: Joyce and Ron Wolf, 1605 East 318 Road, Lecompton, KS 66050

Subject: ITEM NO. 5C PRELIMINARY PLAT FOR NAISMITH CREEK ADDITION

We apologize for being unable to attend the meeting on Monday. We both are members of the Board of Jayhawk Audubon Society and that same date and time will be the annual planning meeting – preparing for the coming 2016-17 year, which closely follows the KU academic calendar.

We are interested in Item 5C because we lived in Indian Hills and were active in the Neighborhood Association and continue to own property in the area. Our first reaction when we looked at the preliminary plat was: “that’s a lot of houses in a relatively small area.” When compared to the lots already built upon to the north, those that already exist look to be about twice as large as the proposed ones. Upon closer examination, it appears that there are hardly any open spaces left for storm water to go during heavy rainfalls. With the addition of all these rooftops and driveways, there will be an enormous amount of extra runoff going into the adjacent Naismith Creek. Has anyone calculated the amount of extra runoff from this area after being developed as proposed?

For those unfamiliar with the Indian Hills area, it is part of a much larger watershed and receives most of the storm water from the KU campus east of Iowa Street and south of Jayhawk Boulevard. We all are aware that more impervious surfaces continue to be added by new construction on the campus. Additionally all the commercial development along 23rd Street from Iowa to Louisiana drains to the south and are part of this drainage area. Naismith Creek is the primary water course which carries most of this runoff. We both remember that during extremely heavy downpours, cars and trucks parked along Belle Haven Drive often would have water above the tires.

During the flood of 1993, the intersection of Louisiana and 31st Streets was completely flooded and impassible. This was due to the discharge from Clinton Lake, which raised the level of the Wakarusa River above normal levels, so that the flap gates (at the southern point where the drainage ditch along the west side of the original Baker Wetlands) were closed. This made all the drainage from Naismith Creek and the “Belle Haven ditch” back up because there was no place for the water to go, thus flooding the above-mentioned intersection. We visited this site on Sunday afternoon; the situation remains the same, so when the Wakarusa River is running higher than normal, the flaps will close and drainage from Naismith will have nowhere to go.

Furthermore, with the suggestion that “lots with basements it is recommended to install sump pumps” indicates that someone recognizes the inherent risks of building in this area so close to and in some places part of the floodplain.

Given the regular and historic nature of flooding in the area, it seems to us that a few adjustments to the new subdivision would be desirable:

I – If the southern-most line of lots from at least lots #20 through #34 would be eliminated, not only would this allow the open space to be available for the smooth continuation of the Naismith Valley Park and Bike

Path it would, more importantly, provide the area with a buffer from floods. And as open space, it would be more complementary with Naismith Park father north. We can think of no other place in Lawrence where a dedicated, non-motorized path utilizes a street designed for motorized traffic. A bike path that is diverted onto a city street, as is proposed, may pose safety hazards that could be avoided by continuing the bike path along the southern portion of the area. At one time, the IHNA adopted this position to maintain part of the area as city-owned open space and continue the park/bike path to a point along Louisiana Street.

II – This reconfiguration of the preliminary plat would also fulfill the top scores of the newly released (July 12, 2016) findings of the Lawrence Parks and Recreation Master Plan. The **Summary of Selected Findings of the Master Plan** states that the most important amenities in Lawrence included: 1) open space and natural area, 2) community parks, 3) trails and pathways, all of which are designed to encourage citizens to spend more time outside. Also as part of this survey, the following were found: The most important aspects of Lawrence's open space and natural areas are "protecting/preserving existing open space and natural areas" and "protecting wildlife habitats." As part of the finding, the top two values were: Promoting Active Lifestyles and Investing in Youth – totally in sync with the suggestion to ensure the area under consideration incorporates open space, preserves and extends natural areas, and provides immeasurable benefits to residents of the area.

Again, by leaving the southernmost portion of the plat as open space, this would provide a wonderful example of development being compatible with and recognizing the opinions expressed by a large majority of citizens of Lawrence regarding what they see as desirable. Perhaps this portion of the park could be named in memory of a loved one of the developers – much like the McGrew Nature Preserve near Quail Run Elementary School and the Pat Dawson Billings Nature Area near Southwest Middle School.

III – By turning the southern portion of the preliminary plat into dedicated open space, it would provide not only a buffer from floods, it would also be a great place for neighborhood children to be able to play close to home without having to cross Louisiana Street, which is a major arterial and carries a lot of traffic.

We appreciate your thoughtful consideration of these comments.

Sincerely,

Joyce and Ron Wolf

PS: We will try to also send a photo of the flap gates, to make that part of the letter clearer as to what happens when they are closed by the pressure of high water levels in the Wakarusa River.

PLANNING COMMISSION REPORT
Non-Public Hearing Item

PC Staff Report
07/25/16

**ITEM NO. 6 FINAL DEVELOPMENT PLAN FOR PETSMART, BAUER FARM; 4820
BAUER FARM DR (SLD)**

FDP-16-00216: Consider a Final Development Plan for PetSmart, located at 4820 Bauer Farm Dr. The plan proposes a new 18,000 sq. ft. building west of Sprout's. Submitted by Treanor Architects, for Wakarusa Investors, LLC, property owner of record.

STAFF RECOMMENDATION: Planning staff recommends approval of the Final Development Plan based upon the findings of fact presented in the body of the Staff Report subject to the following conditions:

1. Prior to recording the Final Development Plan with the Register of Deeds Office the applicant shall provide the following additional documentation:
 - a. Submission of a site plan performance agreement.
2. Prior to recording the Final Development Plan with the Register of Deeds Office the applicant shall provide a revised drawing with the following notes and changes:
 - a. Revise the off-street parking table based on the Net Square Feet of the building at 1 space per 200 NSF.
 - b. Provision of a revised landscape plan to show shrubs planted between street trees along Overland Drive to screen the service drive per staff approval.
 - c. Provision of a note that states that outdoor storage of materials, supplies, products, display equipment and containers in the rear of the building is prohibited.

REASON FOR REQUEST: *Construction of single use retail building.*

ATTACHMENTS:

Attachment A: Proposed Final Development Plan
Attachment B: Proposed Building Elevations
Attachment C: Approved Final Development Plan
Attachment D: Approved Building Elevations

DESIGN STANDARDS TO CONSIDER

- Consistency with approved Preliminary Development Plan, PDP-14-00055.
- Consistency with Article 10 of the 1966 Zoning Code.
- Compliance with zoning Ordinance No. 8986.

KEY POINTS:

- Final Development Plans for projects with preliminary approvals prior to the adoption of the 2006 Development Code require Planning Commission approval.
- Preliminary Development Plan for Bauer Farms Northwest PCD [PDP-14-00055] was approved by the City Commission on May 6, 2014.
- A Final Development Plan for Lots 2 and 3 [FDP-14-00207] was approved by the Planning Commission on July 21, 2014.
- The existing parking lot was constructed with the Sprouts development to the east and is not altered by this application.

- This Proposed development modifies the building footprint from a multi-tenant building with 11,623 SF to a single use building with 18,000 SF.
- Approval of this application will reduce the allowable retail area permitted for the building on Lot 4 to 4,725 SF to maintain the approved threshold as noted on the face of the plan.
- Overall retail SF for the Bauer Farm Northwest including Sprouts and this application is 45,275 SF. Maximum allowed for the development is 50,000.

ASSOCIATED CASES/OTHER ACTION REQUIRED

Associated Cases

- CPA-14-00055; Comprehensive Plan Amendment to revise Chapter 6 and the 6th & Wakarusa Nodal Plan.
- Z-14-00057; amending the uses and retail square foot limitation of existing PCD approved per Ordinance 8986 by the City Commission on May 6, 2014.
- PDP-14-00055; Revised Preliminary Development Plan approved by the City Commission on May 6, 2014.
- PF-14-00054; Final Plat Bauer Farm 6th Plat. Easements accepted by the City commission on May 13, 2014 and recorded with the Douglas County Register of Deeds Office on June 25, 2014, Book 18 Page 801.
- FDP-14-00207; Final Development Plan for Sprouts and future multi-tenant retail building.

Other Actions Required

- Recording the Final Development Plan with the Douglas County Register of Deeds Office.

PUBLIC COMMENT

None

PLANS AND STUDIES REQUIRED

- *Traffic Study* – Study provided and accepted by staff.
- *Downstream Sanitary Sewer Analysis* - The cover letter dated May 23, 2016 provided by Treanor Architects states that the proposed use has not changed since the previously approved downstream sanitary sewer analysis. The previously approved DSSA dated February 14, 2014 provided by Landplan Engineering was reviewed and accepted for the project to satisfy the criteria required for the DSSA as outlined in Administrative Policy 76.
- *Drainage Study* – The revised drainage study dated 9-24-2014 meets the specified requirements and is approved..
- *Retail Market Study* – Not applicable to application.

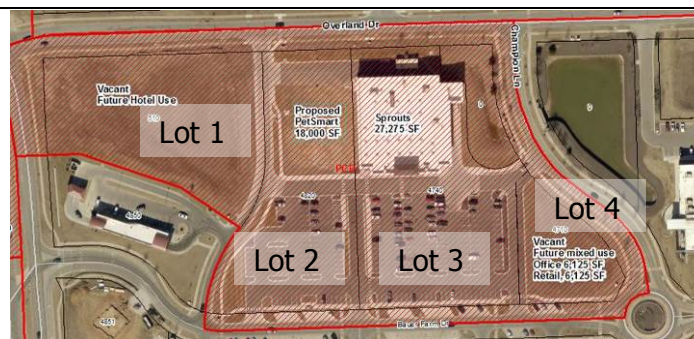


Figure 1: Bauer Farm Northwest PCD Lots 1-4

GENERAL INFORMATION

Current Zoning and Land Use:	PD-[Bauer Farm Northwest] Planned Commercial Development District.
Surrounding Zoning and Land Use:	PD-[Bauer Farm] Planned Commercial Development District to the southwest, south and east of Champion Lane.
To the west:	PD-[Bauer Farm Northwest] Planned Commercial Development District; vacant land to the west on the west side of property and interior access drive. And PD-[Bauer Farm] Planned Commercial Development District; <i>Vehicle Sales and Service, Cleaning</i> , Hurricane Car Wash.
To the south:	Mixed use including <i>Retail</i> and <i>Fast Order Food</i> with and without drive-thru located on the south side of Bauer Farm Drive.
To the east:	Immediately east; existing <i>Food and Beverage sales</i> (Sprouts) and vacant land. East side of Champion Lane. PD-[Bauer Farm] Planned Commercial Development District; existing Theater Lawrence.
To the north:	GPI (General Public and Institutional Uses) District north of Overland Drive; School, Free State High School and Participant Sports & Recreation, Indoor, Lawrence Indoor Aquatic Center

STAFF REVIEW

The commercial area has been platted, creating developable lots and an interior access drive (private street) between Bauer Farm Drive and Overland Drive. Development of Sprouts (Lot 3) resulted in the completion of the private street. Development of Lot 1 (on the southeast corner of Wakarusa Drive and Overland Drive) and Lot 4 (on the northwest corner of Champion Lane and Bauer Farm Drive) will require future Final Development Plan approval prior to a building permit. However approval of this request impacts Lot 4 because the applicant is taking away some of the assigned retail area (SF) and applying it to the proposed development on Lot 2.

Zoning and Subdivision Compliance:

This property was rezoned with restrictions, per Ordinance No. 8986, addressing specific land uses and establishing a maximum retail development cap of 50,000 SF (for all four lots). The zoning also prohibits a single retail or commercial building from being larger than 50,000 SF. The land use for Lot 1 is identified in the approved Preliminary Development Plan as a *Transient Accommodation (Hotel) Use*. This use does not count toward the maximum retail area limit. Lot 4 was shown as a vertical (2 story) mixed retail and office use building. A portion of the total retail area was allotted to Lot 4. The bulk of the retail area was approved for Lots 2 and 3. A Final Development Plan was approved for both Lots 2 and 3.

This Final Development Plan proposes a change to the approved Final Development Plan for Lot 2 Bauer Farm 6th Plat from a multi-tenant retail building with 11,623 SF to a single tenant building with 18,000 SF. This application has the effect of reducing the available area for retail development of Lot 4 (4710 Bauer Farm Drive). A note on the face of the plan restricts the *"building floor area on Lot 4 to 5,107 GSF max. (2 story)"*.

- The Final Development Plan as proposed exceeds the commercial cap of 50,000 SF as summarized below. The note should be revised to clearly state that the maximum allowed retail SF for Lot 4 is restricted.

Table 1: Approved/Proposed Retail SF

Site Summary	Approved Retail Area	Built/Proposed	Change
Lot 1; 510 Wakarusa Dr. Not included in project Future Hotel Use/No Retail SF proposed	0 SF	0 SF	0%
Lot 2; 4820 Bauer Farm Dr. Proposed Development Proposed PetSmart	11,623 SF	18,000 SF	6,377 SF +55%
Lot 3; 4740 Bauer Farm Dr. Not included in project Sprouts	27,275 SF	27,275 SF	0%
Lot 4; 4710 Bauer Farm Dr. Not included in project Future Office/Retail Use	6,125 SF	4,725 SF	1,018 SF -23%
Total	45,023 SF	50,000 SF	+11%
Maximum Permitted 50,000 SF			

This application represents a deviation from both the approved Preliminary Development Plan for Bauer Farm Northwest and the approved Final Development Plan that included Lots 2 and 3. This development does not include a residential component. The proposed building represents a 12% increase in the proposed commercial development from the approved project but is under the maximum allowed 50,000 SF cap.

Off-Street Parking

This Final Development Plan was previously approved with the following parking:

- Lot 2: 11,305 NSF retail use; Required spaces 57; Proposed spaces 88
- Lot 3: 26,700 NSF grocery use; Required spaces 134; Proposed spaces 135

Additionally, 28 on-street parking spaces were constructed with the Sprouts improvement along Bauer Farm Drive. Current off-street parking requirements are based on gross square feet of the building. Additionally, parking is required at a rate of 1 space per 300 SF. This application does not modify parking for Lot 3. The original PCD required parking at a ratio of 1 space per 200 NSF regardless of use. Parking within a Planned Commercial Development is shared across uses allowing some uses to have more or less parking so long as the minimum required off-street parking is met.

This revised Final Development Plan shows off-street parking based on the Gross Square Feet. The applicant indicates that the net square footage of the proposed building is 13,356 SF. The existing parking lot provides excess parking for the proposed use.

Lot 2 Parking summary	@ 1 space per 200 SF Per PCD	@ 1 space per 300 SF Per Conventional Zoning
18,000 GSF	90 spaces	60 spaces
13,356 NSF	67 spaces	45 spaces
Total Spaces Provided		77 spaces

Required off-street parking needs to be shown uniformly per the previous methods of parking calculations shown for this development. Staff recommends the parking be revised to show the

Net Square Feet of the proposed building and a revised parking table consistent with the standard accounting of 1 space per 200 NSF for this development

- 67 spaces required
- 77 spaces provided

Common Open Space

A minimum of 20% of the area in the Planned Development is required to be placed into Common Open Space. Common open space within the commercial development typically occurs as peripheral yards and interior open space. The approved Preliminary Development Plan showed that while Lots 2 and 3 do not meet the required open space individually, the combined four lots in the Bauer Farm Northwest PCD did comply with this design standard. Excess open space is provided in Tract A (detention pond east of Sprouts), and in the future development of Lot 1 and Lot 4 of Bauer Farm Northwest.

The approved Final Development Plan showed 16,574 SF (18%) of open space for Lot 2. This proposed Final Development Plan shows 16,974 SF (19%) open space.

Table 2: Property Surface Summary Lot 2

Approved Final Development Plan <u>LOT 2</u>				Proposed Final Development Plan Property surface summary			
EXISTING		PROPOSED		EXISTING		PROPOSED	
BUILDINGS	0 S.F. (0%)	11,623	S.F. (12%)	BUILDINGS	0 S.F. (0%)	18,000	S.F. (19%)
PAVEMENT	1,928 S.F. (2%)	64,532	S.F. (70%)	PAVEMENT	55,980 S.F. (60%)	57,755	S.F. (62%)
IMPERVIOUS	1,928 S.F. (2%)	76,155	S.F. (82%)	IMPERVIOUS	55,980 S.F. (60%)	75,755	S.F. (81%)
PERVIOUS	90,801 S.F. (98%)	16,574	S.F. (18%)	PERVIOUS	36,749 S.F. (40%)	16,974	S.F. (19%)
LOT AREA	92,729 S.F.	92,729	S.F.	LOT AREA	92,729 S.F.	92,729	S.F.

Green Space/Landscape:

This proposed project does not alter the interior green space of the parking lot. The area affected by the proposed building increase is located in the northwest corner of Lot 2. The following graphic shows the approved and proposed changes to the green spaces within the site that result from the change to the building footprint.

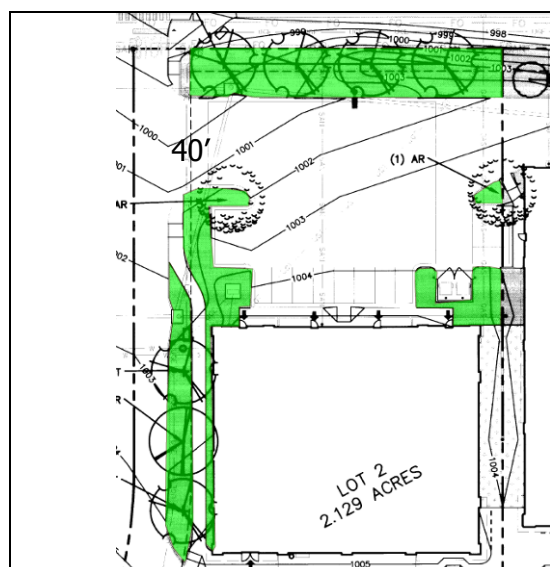


Figure 2: Approved FDP Landscape/Access Plan

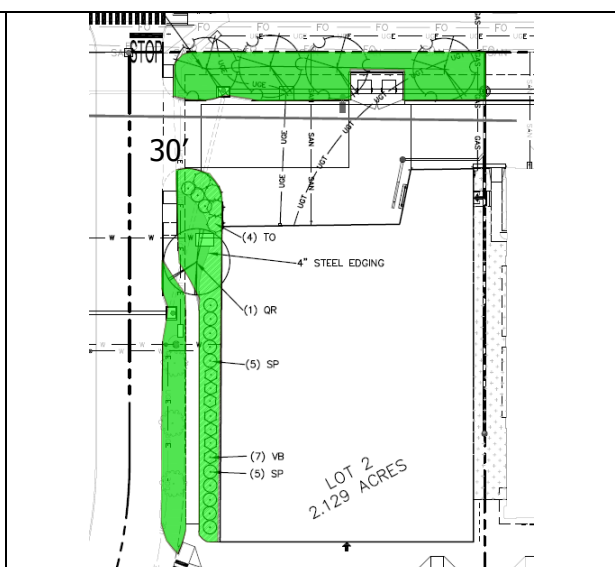


Figure 3: Proposed FDP Landscape/Access Plan

Greenspace is added along the private street and the delivery drive on the north side of the site is reduced from 40' to 30'. Green space along the west side of the building has also been increased. These elements will enhance the pedestrian experience of the site.

Landscape requirements address street trees, parking lot interior and perimeter landscaping, buffer yards, and requirements to screen mechanical equipment. Parking lot landscaping was reviewed and approved with the previous Final Development Plan for Sprouts.

The parking lot was constructed per the 2014 plan. This application does not revise the parking lot area south of the building. The approved plan included off-street parking north of the building. The proposed increase in the retail building removed parking in this area.

Street Trees: Street trees are applicable along Overland Drive (4 trees), Bauer Farm Drive (6 trees) and within the development along the private street (13 trees). The proposed development includes the minimum required street trees with additional trees provided along Bauer Farm Drive.

The proposed building has been reconfigured to provide a larger greenspace along the west side of the building and retain the existing street trees along the private street.

Buffer Yard: A buffer yard was not originally required with the proposed development. The intent was to allow reduced building setbacks to bring buildings closer to the public streets. Portions of the development include screening walls such as those along Wakarusa Drive and W. 6th Street. Interior streets and lot lines are not required to provide a designated buffer yard. This standard is generally applicable along zoning district boundary lines. In all directions but the north the subject property is surrounded by the same zoning and part of an integrated commercial development. Overland Drive to the north marks the north boundary of the district. Both Sprouts and the subject property are located adjacent to this boundary. The approved preliminary development plan included minimal landscape material along the north side of Lots 2 and 3. The delivery drive is intended for access and not storage of supplies and products. Highly detailed building facades are intended to mitigate the requirement for screening and provide a visual connection between the commercial development and the surrounding area.

However the Sprouts site has made use of this space for both smaller deliveries and for storage as seen in the following pictures.

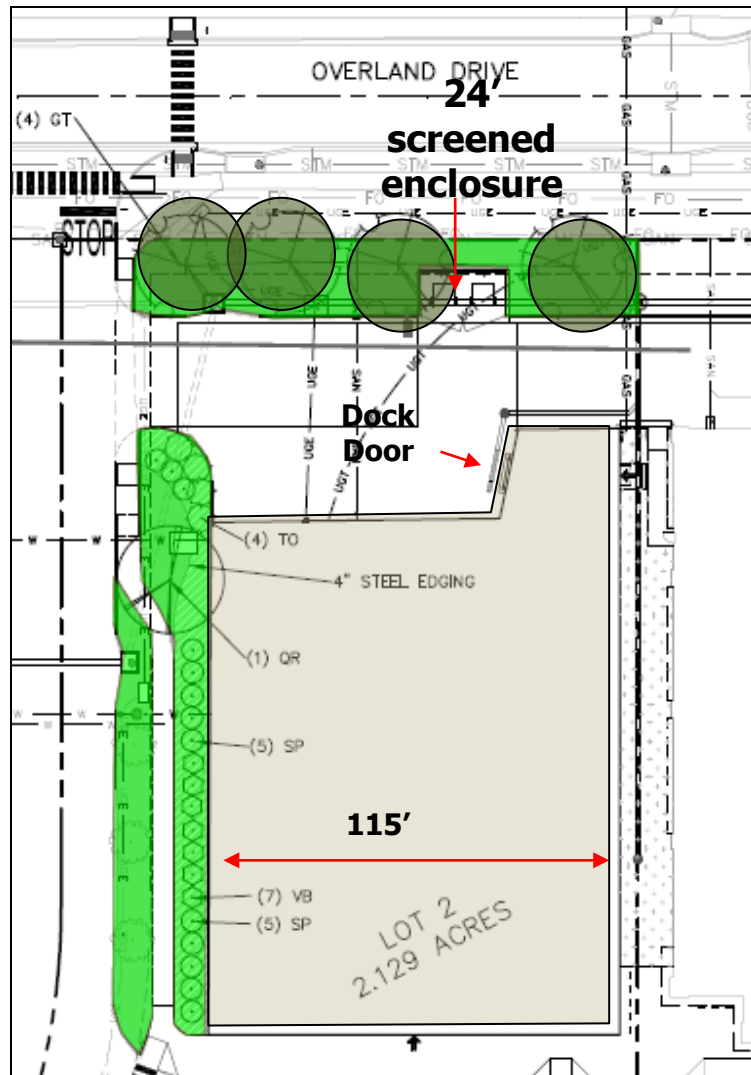


The trash enclosure for the proposed building will provide 24' of screening along the rear of the building specifically in the area of the loading dock. The orientation of the loading dock is toward the private street rather than facing Overland Drive as in the Sprouts example above. Street trees along Overland Drive have been installed for both Lots 2 and 3 and are shown on the attached Final Development Plan.

Screening is needed when there are frequent deliveries, storage of materials or trash enclosures in a particular area. Prohibiting storage within the delivery area would facilitate the use of the space per the approved plans. The addition of a screening wall or dense landscape would result in providing an appropriate visual aesthetic for the surrounding area. While sprouts is not included in this application is exemplifies the concerns.

Screening walls can be provided but must maintain a minimum separation from utilities, specifically the sanitary sewer. The plan also shows a proposed transformer to be located in the rear of the property. These utility boxes are quite large and should also be appropriately screened.

Staff recommends that a dense landscape screen provided along the north side of Lot 2 to provide additional screening. Shrubs, located in groupings, between street trees will provide adequate screening. Staff also recommends that a note be added to the plan that prohibits the outdoor storage of equipment, supplies, materials, and similar item within the access drive on the north side of the property.



Access and Circulation

Primary vehicular access to this site is provided from Bauer Farm Drive, Overland Drive, and Champion Lane and an interior access drive (private street) between Bauer Farm Drive and Overland Drive. Direct access to both Overland Drive and Champion Lane are restricted to specific access points established with the Final Plat.

Delivery access to Lots 2 and 3 were approved along the private street with a 40' wide driveway intended to serve both buildings. The sprout's building was approved with delivery access to the side of the building as well. However, that east side of the building is separated by a drainage easement (dedicated open space) with a standard public sidewalk that is separated from the street with a tree lawn.

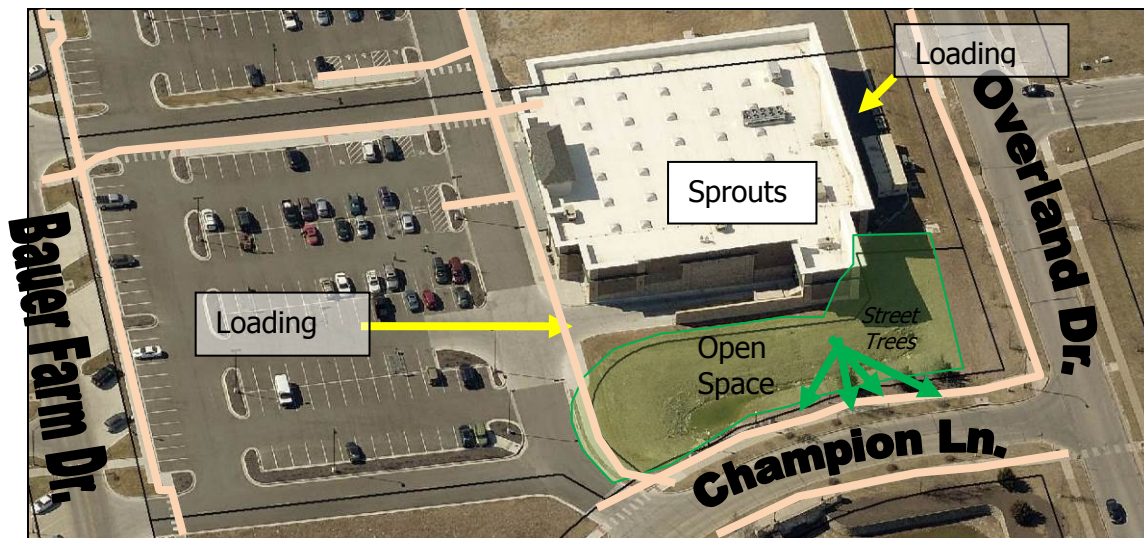


Figure 4: Looking West

This Final Development Plan includes a reconfigured building footprint with a rear loading dock area that incorporates the existing north driveway as shown in Figure 3 above.

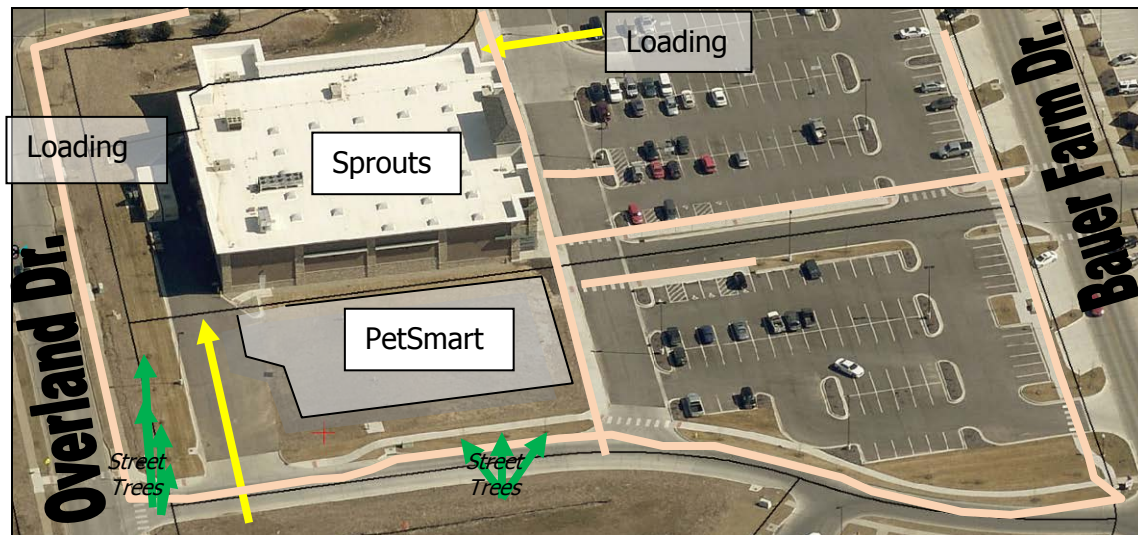


Figure 5: Looking East

This application does not alter the primary customer vehicular and bikeway systems and access to and around the proposed development.

Pedestrian Connectivity

Internal pedestrian walkways are provided throughout the development. The plan shows pedestrian connections within the site and connecting to existing public sidewalks surrounding the property. The original approval oriented the Sprouts building and the future commercial building along a common shared drive and parking lot area with a shared area accessed from the private street on the west side. This design provided a connected pedestrian pathway along the west side of the commercial building with a public sidewalk separated from the street with a green space as shown in the following figure on the left.

The revised plan, with the bigger building creates two driveways along the private street and relocates the sidewalk to the back of the street curb and reduces the green space. The interior pedestrian sidewalks are not changed with this application with the exception noted.

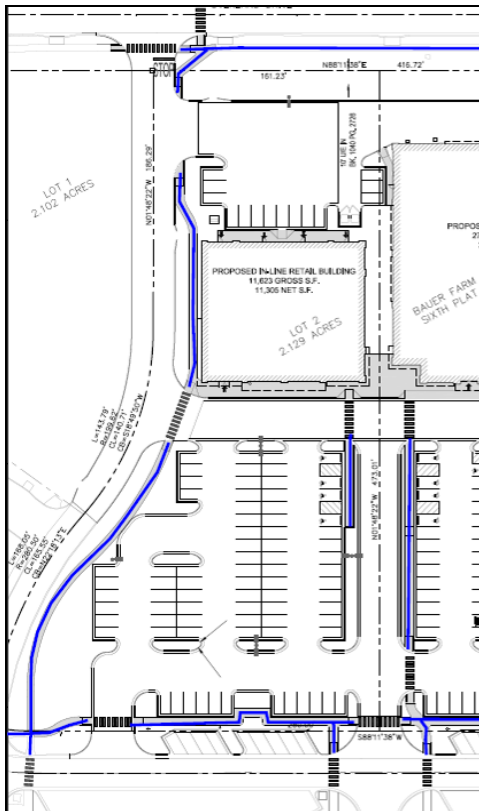


Figure 6: Approved FDP with 11,000 SF building

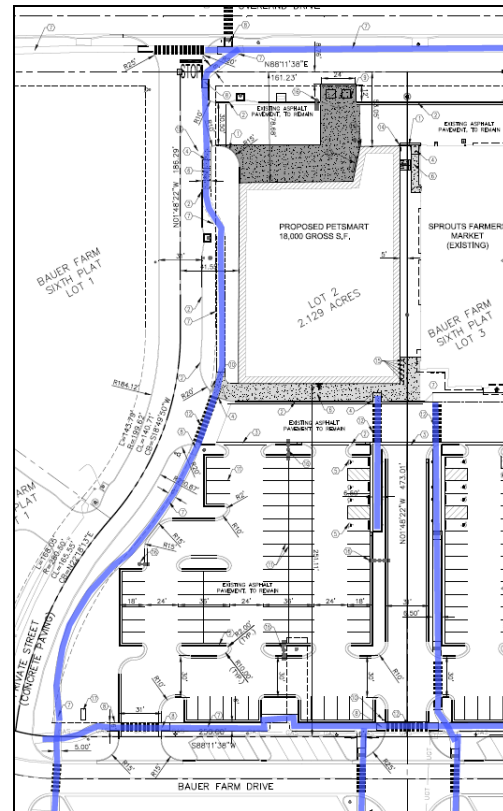


Figure 7: Proposed FDP with 18,000 SF building

Lighting

The parking lot on Lot 2 was constructed with the Sprouts development. New lighting for the site will be applicable to building lighting for the retail building. Changes to the parking lot lighting are not proposed (FDP-14-00207). However, this application replaces the approved development for Lot 2. An updated point-by-point photometric plan was provided for this proposed development. Building fixture lighting will be reviewed with the submission of a building permit to ensure that fixtures are also compliant with the design requirements.

Commercial Design Standards

As the overall design of the site was approved with the Preliminary Development Plan, the changes being proposed to the site and the building façade were reviewed with the Commercial Design Standards included as Section Two of the Community Design Manual. The site provides overall good pedestrian and vehicular access.

The building façade contains both vertical and horizontal variation and a mix of materials is used in both buildings. The design provides a consistent theme for all commercial buildings within the development. Additional glazing has been added to the front and exterior side of the building. A parapet is intended to screen any roof mounted mechanical equipment. Building mounted mechanical equipment will require landscape screening to avoid public views found in other areas of the development. Additional follow up will be required to ensure that building not included in this application also comply with this design standard.

Table 3: Mechanical Screening Examples

				
Electric meters facing W. 6 th Street	Gas meters along interior street with partial screening	Meter box screened from W. 6 th Street but not from sidewalk	Additional dumpster added to site but not screened.	Gas meter not screened.

Front Façade: Buildings within the commercial development are oriented either to the corner or to the street face. Corner elements are lifted establishing a prominent building feature. The exception to this design is Sprouts located in the north center portion of the development where the main building entrance, front façade is centered. The proposed retail building mimics this same architecture.



Figure 8: Front Elevation - PetSmart



Figure 9: Front Elevation - Sprouts

Unlike Sprouts, the truck dock is located to the rear of the building rather than on the building side.

Exterior Side Façade: The exterior side of the building is softened with the landscape areas along the private street that accommodates street trees and an existing sidewalk.

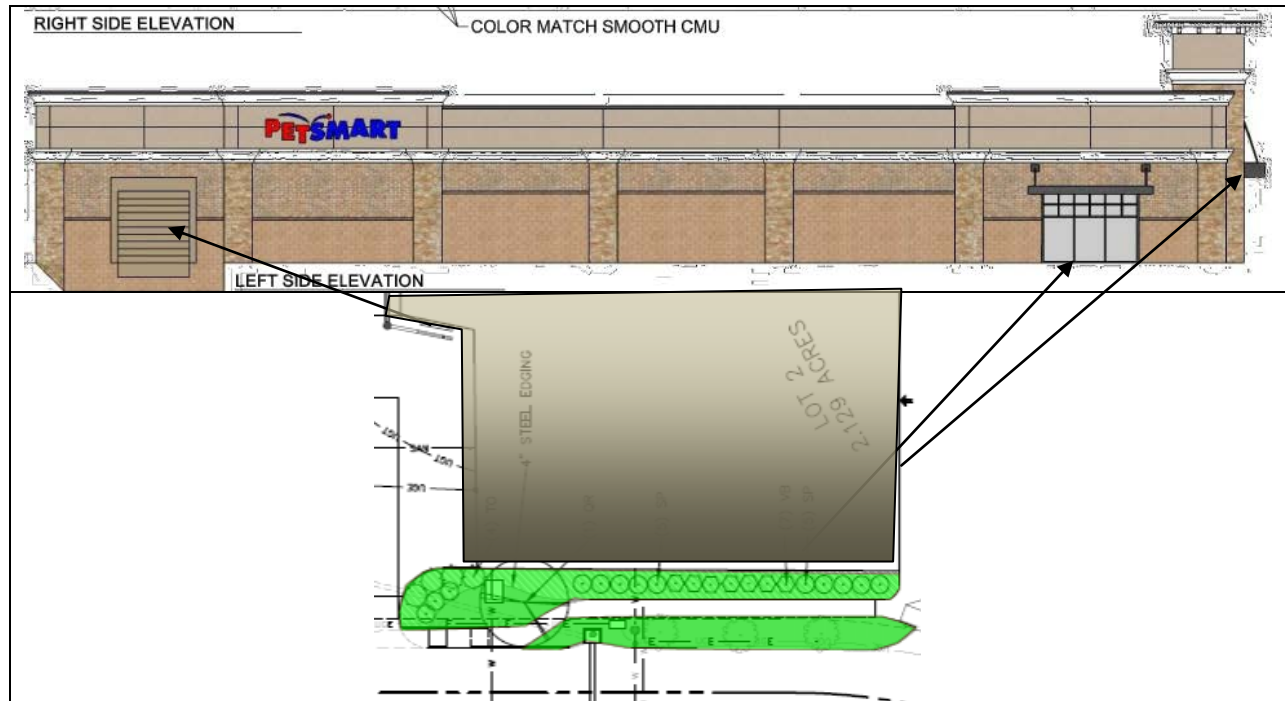


Figure 10: Exterior Side Elevation

Rear Façade: The rear of the building is oriented to Overland Drive and was discussed with the landscape section previously in this staff report. The loading dock is oriented to the private street and is setback into the site providing significant separation from the pedestrian pathways along the west side of the building. The picture below (top) shows the proposed building façade that would face Overland Drive. The picture below (bottom) shows the existing Overland Drive street scape with a portion of the Sprouts façade visible.

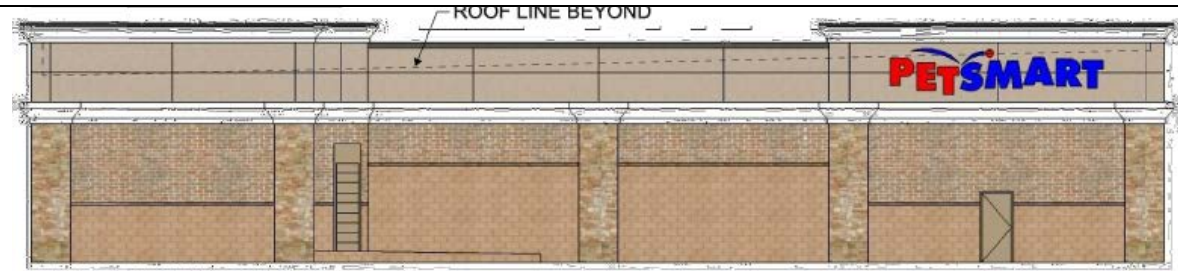


Figure 11: Rear Elevation



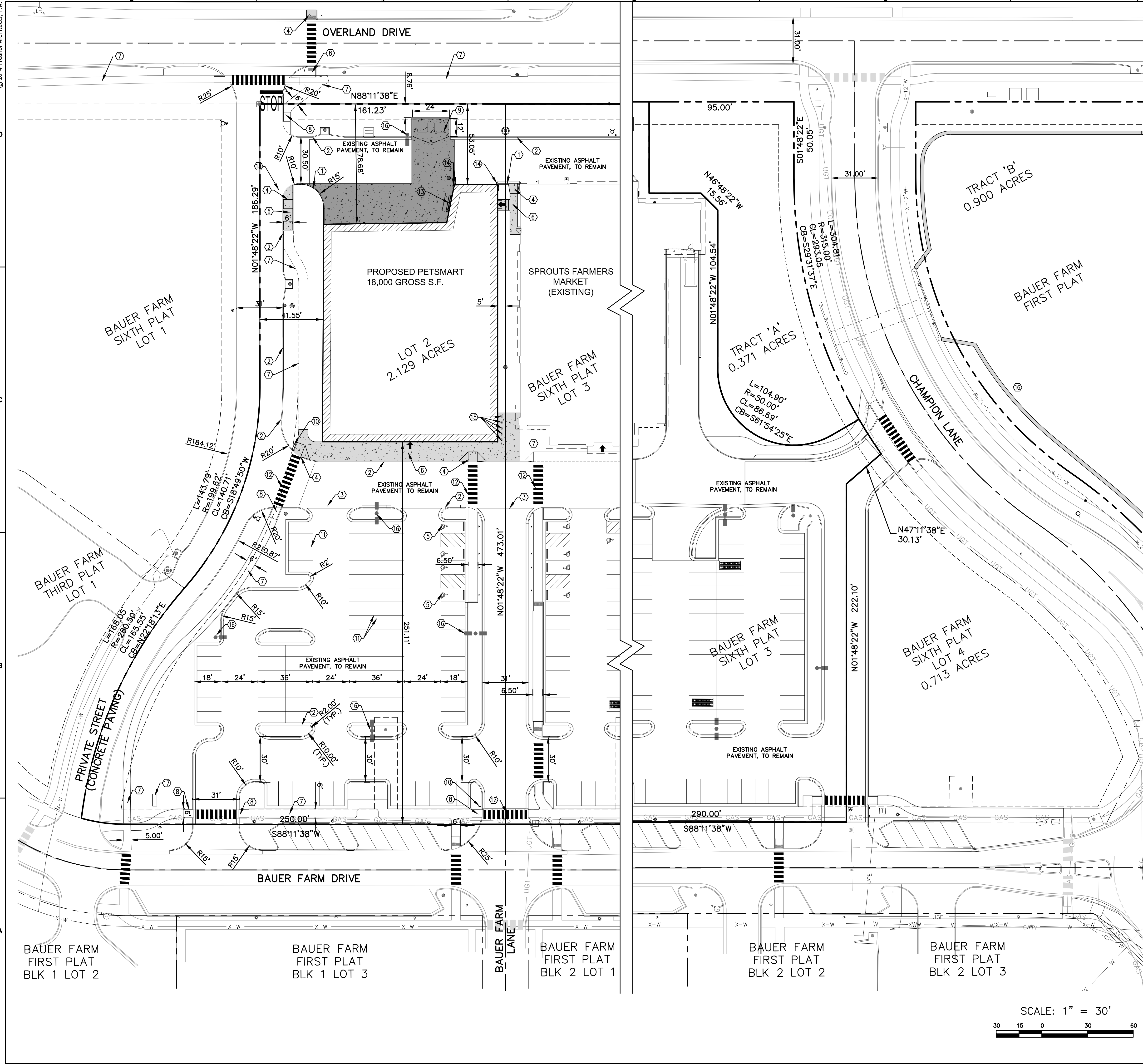
Figure 12: Overland Drive Street View

The building materials and pattern of the front of the building are continued on all sides. The addition of landscape shrubs would mitigate the activity associated with the commercial use from delivery vehicles and enhance the pedestrian experience of Overland Drive.

Conclusion

The proposed development is consistent with the planned development for this area and is compliant with the provisions of the Land Development Code and the Commercial Design Standards.

FILE PATH: P:_Civil\CE16.003 Bauer Farm PetSmart\Drawings\PLANNING\Final Development Plan\01 SITE LAYOUT PLANNING
DATE PRINTED: Monday, July 18, 2016
LAST SAVED: Monday, July 18, 2016
PRINTED BY: Brian Kemp



GENERAL NOTES

- THIS PROJECT HAS BEEN DESIGNED TO COMPLY WITH THE PROVISIONS OF THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) FOR BUILDING AND FACILITIES, APPENDIX A TO 28 CFR PART 36.
- CITY OF LAWRENCE WILL NOT BE HELD RESPONSIBLE FOR PAVEMENT DAMAGE DUE TO REFUSE COLLECTION.
- OCCUPANCY PERMITS FOR PROPOSED BUILDINGS WILL NOT BE ISSUED UNTIL THE REQUIRED PUBLIC DRAINAGE IMPROVEMENTS ARE COMPLETE, FINAL INSPECTED AND ACCEPTED BY THE PUBLIC WORKS DEPARTMENT.
- THE RETENTION BASIN WILL BE PRIVATELY-OWNED AND MAINTAINED. THE LANDOWNER IS RESPONSIBLE FOR THE MAINTENANCE OF THE RETENTION BASIN. FAILURE TO MAINTAIN THE RETENTION BASIN WILL RESULT IN THE LOSS OF THE STORMWATER DETENTION CREDIT. THE RETENTION BASIN WILL REMAIN FREE OF ANY NATURAL OR NON-NATURAL STRUCTURES OR VEGETATIVE BARRIERS (INCLUDING BUT NOT LIMITED TO TREES, SHRUBBERY, BERMS, FENCES, AND WALLS).
- ALL CONCRETE BOX CURB INLETS WILL BE CONSTRUCTED PER CITY OF LAWRENCE STORM SEWER STANDARD DETAILS.
- BUILDING FLOOR AREA ON LOT 4 TO BE 4,725 G.S.F. MAX.

LEGAL DESCRIPTION
LOT 2, BAUER FARM SIXTH PLAT, A SUBDIVISION IN THE CITY OF LAWRENCE, DOUGLAS COUNTY, KANSAS

PARKING REQUIREMENTS

OFF-STREET STALLS REQ'D	OFF-STREET STALLS PROVIDED	ADA STALLS REQ'D	ADA STALLS PROVIDED	BICYCLE STALLS REQUIRED	BICYCLE STALLS PROVIDED
90	77	4	4	9	10

NOTE:
-28 ON-STREET ADDITIONAL STALLS ALONG BAUER FARM DRIVE WERE CONSTRUCTED IN CONJUNCTION WITH THE SPROUTS PROJECT.
-SHARED PARKING IS PART OF OVERALL DEVELOPMENT AND COVENANT; 13 SHARED STALLS, TOTALING 90, AS REQUIRED.

PROPERTY SURFACE SUMMARY

EXISTING	PROPOSED
BUILDINGS 0 S.F. (0%)	18,000 S.F. (19%)
PAVEMENT 55,980 S.F. (60%)	57,755 S.F. (62%)
IMPERVIOUS 55,980 S.F. (60%)	75,755 S.F. (81%)
PERVIOUS 36,749 S.F. (40%)	16,974 S.F. (19%)
LOT AREA 92,729 S.F.	92,729 S.F.

PAVEMENT LEGEND

- HEAVY-DUTY REINFORCED CONCRETE (DRIVE APRONS, TRUCK ZONES)
- UNREINFORCED CONCRETE (SIDEWALKS)

KEY NOTES

- 6" CONCRETE CURB & GUTTER
- EXISTING CONCRETE CURB AND GUTTER
- EXISTING CONCRETE VALLEY GUTTER
- ACCESSIBLE SIDEWALK RAMP
- EXISTING ACCESSIBLE PARKING STALLS. INCLUDES AT LEAST ONE VAN-ACCESSIBLE STALL PER BAY.
- 4" CONCRETE SIDEWALK, WIDTH VARIES.
- EXISTING SIDEWALK, WIDTH VARIES.
- EXISTING ACCESSIBLE RAMP.
- DUMPSTER ENCLOSURE. BRICK WITH STONE ACCENT TO MATCH BUILDING. GATE POSTS TO MAINTAIN 24 FOOT INTERIOR CLEARANCE.
- STOP SIGN. POLE TO MATCH EXISTING SIGN POLES IN BAUER FARM.
- EXISTING 4" WIDE WHITE PAINTED PARKING STALL MARKINGS, TYP..
- EXISTING CROSSWALK MARKINGS.
- 4' LOADING DOCK.
- CONCRETE-FILLED PIPE BOLLARD.
- BICYCLE RACK. LANDSCAPE FORMS "PI" TYPE, OR APPROVED EQUAL TO MATCH DEVELOPMENT, POWDER COATED BLACK, IN-GROUND MOUNTED.
- EXISTING PARKING LOT LIGHT.
- MONUMENT SIGN.

PATRICK KELLY, PLANNING COMMISSION CHAIR
LAWRENCE-DOUGLAS COUNTY PLANNING COMMISSION

SCOTT MCCULLOUGH, PLANNING DIRECTOR
LAWRENCE-DOUGLAS COUNTY PLANNING AND DEVELOPMENT SERVICES

CURRENT OWNER
WAKARUSA INVESTORS, LLC
CONTACT PERSON: DAVID GEIST
6834 SOUTH UNIVERSITY BLVD, SUITE 415
CENTENNIAL, COLORADO 80122
(720) 489-0424

ARCHITECT
CASCO
CONTACT PERSON: BRIAN WONDOLSKI
1090 KING GEORGE ROAD, SUITE 1201
EDISON, NEW JERSEY 08837
(732) 661-1400

DEVELOPER
THOMPSON THRIFT
CONTACT PERSON: GREG RIPPLE, P.E.
111 MONUMENT CIRCLE, SUITE 1600
INDIANAPOLIS, INDIANA 46204
(317) 853-5482

LAND SURVEYOR
ATLAS SURVEYORS, LLC
CONTACT PERSON: ROGER B. DILL, P.S.
207 SOUTH FIFTH STREET
LEAVENWORTH, KANSAS 66048
(913) 530-8422

CIVIL ENGINEER - PRIMARY CONTACT
TREANOR ARCHITECTS, P.A.
CONTACT PERSON: MATTHEW L. MURPHY, PE
1040 VERMONT STREET
LAWRENCE, KANSAS 66044
(785) 727-2407

CE16.003.00B

Final Development Plan For

Bauer Farm Northwest - PetSmart

4820 Bauer Farm Drive, Lawrence, Kansas 66049

APPROVED BY:

Name:	Date:
Name:	Date:
Name:	Date:

TREANOR ARCHITECTS P.A.
1040 Vermont Street
Lawrence, Kansas 66044-2920
Office: 785.842.4858
Fax: 785.842.7536
www.treanorarchitects.com

THIS SITE PLAN IS FOR PLANNING PURPOSES ONLY NOT FOR CONSTRUCTION

REVISIONS

REVISIONS
7.7.16 REVISED PER CITY
REVIEW COMMENTS
7.13.16 REVISED PER CITY
REVIEW COMMENTS
7.18.16 REVISED PER CITY
REVIEW COMMENTS

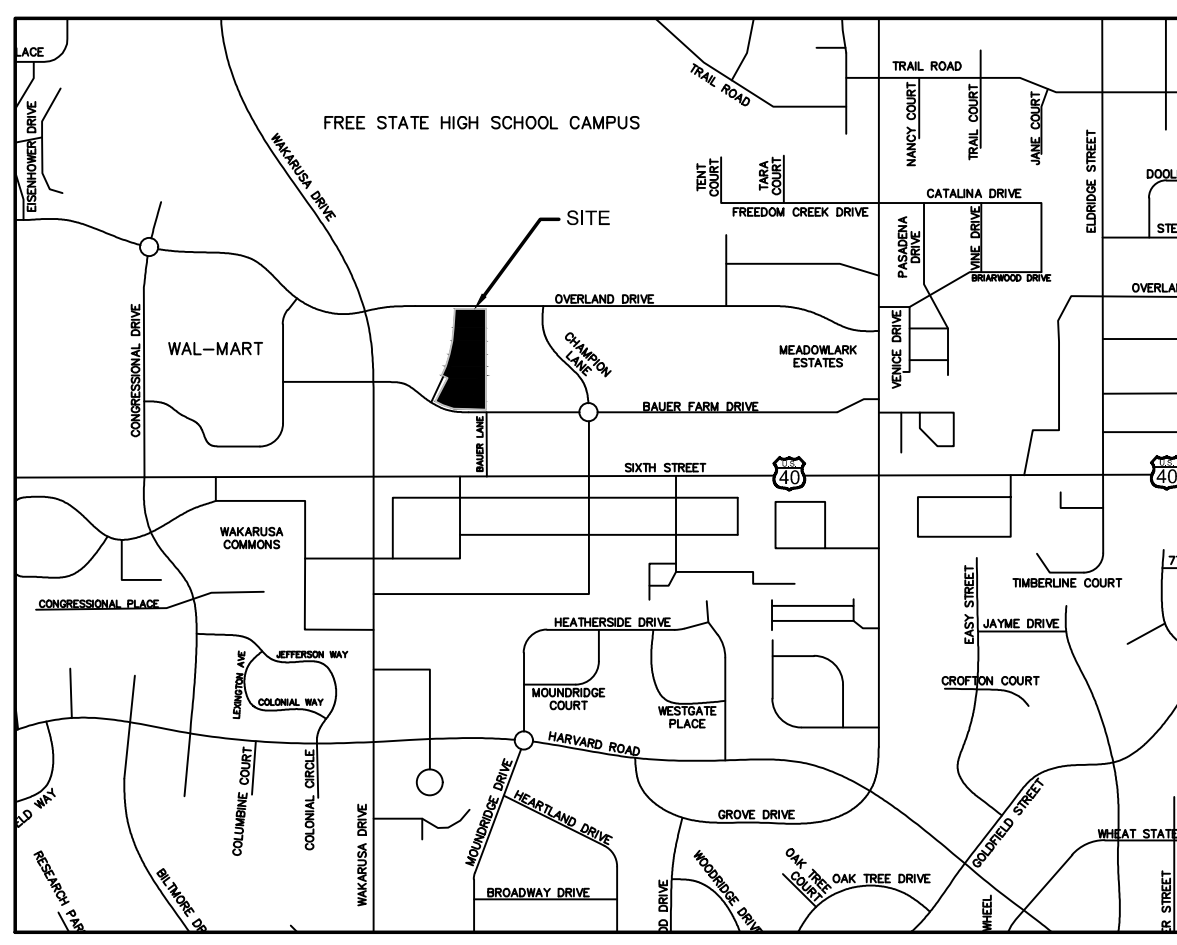
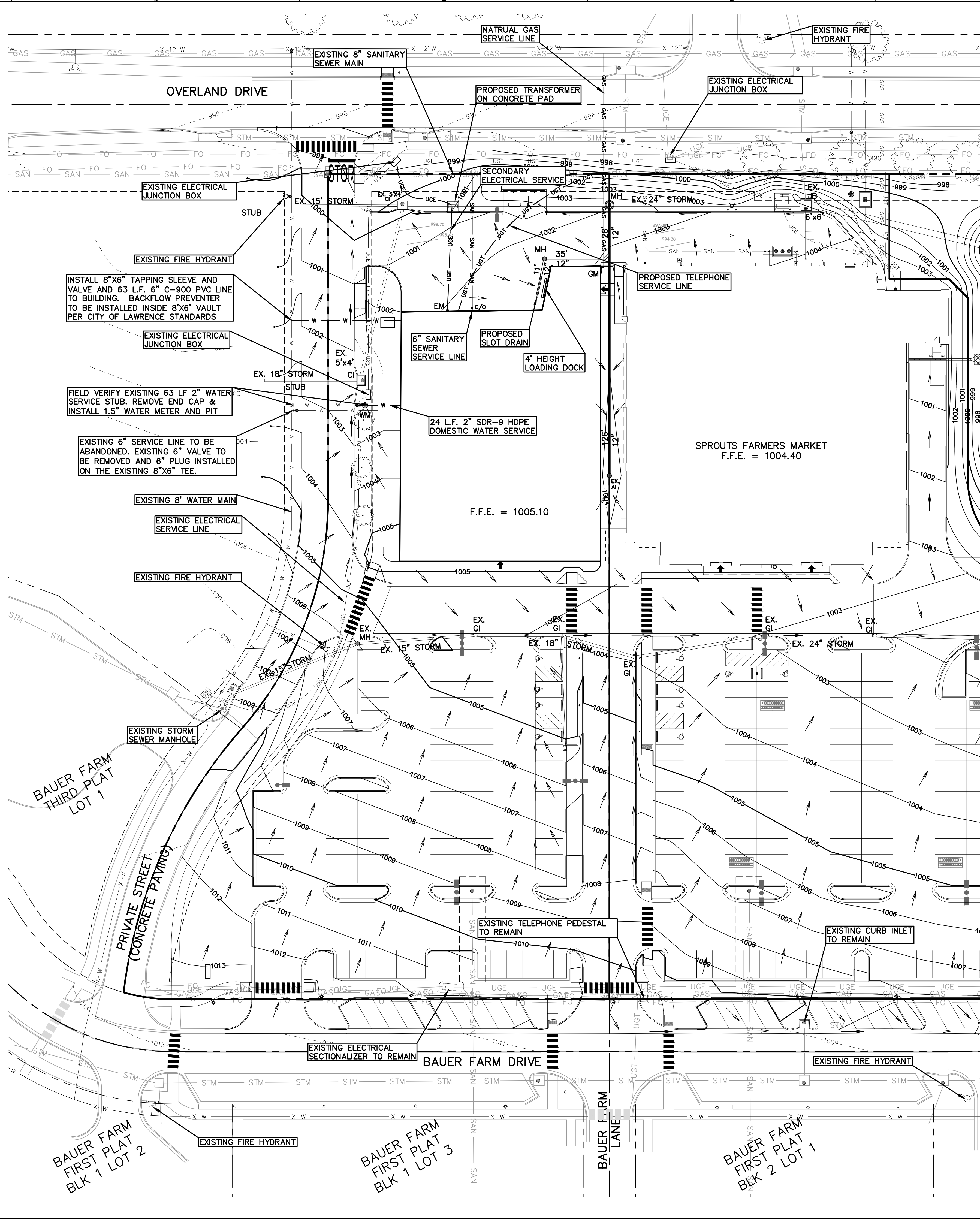
Rel. Date: 05.23.2016

SITE LAYOUT PLAN

1

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DATE PRINTED: Monday, July 18, 2016
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VICINITY MAP
SCALE: 1" = 1000'

- GRADING AND UTILITY LEGEND
- CATV- EXISTING CABLE TV LINE
 - FO- EXISTING FIBER OPTIC LINE
 - GAS- EXISTING NATURAL GAS MAIN
 - SAN- EXISTING SANITARY SEWER MAIN
 - STM- EXISTING STORM SEWER PIPE
 - UGE- EXISTING UNDERGROUND ELECTRIC LINE
 - UGT- EXISTING UNDERGROUND TELEPHONE LINE
 - W- EXISTING WATER MAIN
 - 12"W- EXISTING 12" WATER MAIN

- GAS- PROPOSED GAS LINE
- SAN- PROPOSED SANITARY SEWER LINE
- UGE- PROPOSED UNDERGROUND ELECTRIC LINE
- UGT- PROPOSED UNDERGROUND TELEPHONE LINE
- W- PROPOSED WATER LINE
- BW BOTTOM OF WALL ELEVATION
- EM PROPOSED ELECTRIC METER
- GM PROPOSED GAS METER
- WM PROPOSED WATER METER
- C/O PROPOSED CLEANOUT
- DS PROPOSED DOWNSPOUT
- F.F.E. FINISHED FLOOR ELEVATION
- JB PROPOSED STORM SEWER JUNCTION BOX
- MH PROPOSED STORM SEWER MANHOLE
- CI PROPOSED STORM SEWER CONCRETE BOX CURB INLET
- CI PROPOSED 2'x3' NYLOPLAST CURB INLET WITH HIGH-FLOW VANE GRATE
- SP PROPOSED SANITARY SEWER SAMPLING PORT
- STUB PROPOSED STORM SEWER LINE WITH END PLUG FOR FUTURE SERVICE TO ADJACENT LOT

- UTILITY NOTES
1. ALL STORM SEWER PIPING WILL BE HIGH-DENSITY POLYETHYLENE (HDPE) DUAL-WALL PIPE WITH SMOOTH INTERIOR.
 2. ALL SANITARY SEWER PIPING WILL BE STANDARD DIMENSION RATIO (SDR) 26 POLYVINYL CHLORIDE (PVC).
 3. DOMESTIC WATER SERVICE LINES WILL BE SDR-9 HDPE
 4. FIRE SPRINKLER SERVICE LINES AND WATER MAINS MUST BE RATED AS DR14 AS DEFINED IN AWWA C900.
 5. OCCUPANCY PERMITS FOR PROPOSED BUILDINGS WILL NOT BE ISSUED UNTIL THE REQUIRED PUBLIC DRAINAGE IMPROVEMENTS ARE COMPLETE, INSPECTED AND ACCEPTED BY THE PUBLIC WORKS DEPARTMENT.
 6. THE RETENTION BASIN WILL BE PRIVATELY OWNED AND MAINTAINED. THE LAND OWNER IS RESPONSIBLE FOR THE MAINTENANCE OF THE RETENTION BASIN. FAILURE TO MAINTAIN THE RETENTION BASIN WILL RESULT IN THE LOSS OF THE STORMWATER DETENTION CREDIT. THE RETENTION BASIN WILL REMAIN FREE OF ANY NATURAL OR NON-NATURAL STRUCTURES OR VEGETATIVE BARRIERS (INCLUDING BUT NOT LIMITED TO TREES, SHRUBBERY, BERMS, FENCES, AND WALLS)
 7. ALL CURB INLETS WILL BE CONSTRUCTED PER CITY OF LAWRENCE STORM SEWER STANDARD DETAILS.
 8. BACKFLOW PREVENTER TO BE INSTALLED INSIDE BUILDING APPROXIMATELY 48 FEET FROM TAP ON MAIN.

- BENCHMARKS
- BM #1 CHISELED SQUARE ON THE CENTER FRONT FACE OF THE FIRST CURB INLET EAST OF THE ENTRANCE TO PEOPLE'S BANK (4831 WEST SIXTH STREET) ON THE SOUTH SIDE OF 6TH STREET (U.S. HIGHWAY 40)
ELEVATION: 1012.98 FEET A.M.S.L.
- BM #2 CHISELED SQUARE ON THE CENTER FRONT FACE OF THE CURB INLET IN THE SOUTHEAST QUADRANT OF THE INTERSECTION OF OVERLAND DRIVE AND WAKARUSA DRIVE.
ELEVATION: 1002.75 FEET A.M.S.L.

PATRICK KELLY, PLANNING COMMISSION CHAIR
LAWRENCE-DOUGLAS COUNTY PLANNING COMMISSION

SCOTT MCCULLOUGH, PLANNING DIRECTOR
LAWRENCE-DOUGLAS COUNTY PLANNING AND DEVELOPMENT SERVICES

SCALE: 1" = 30'

30 15 0 30 60

CE16.003.00B

Final Development Plan For

Bauer Farm Northwest - PetSmart

4820 Bauer Farm Drive, Lawrence, Kansas 66049

APPROVED BY:

Name:	Date:
Name:	Date:
Name:	Date:

TREANOR
ARCHITECTS P.A.
1040 Vermont Street
Lawrence, Kansas 66044-2920
Office: 785.842.4858
Fax: 785.842.7536
www.treanorarchitects.com

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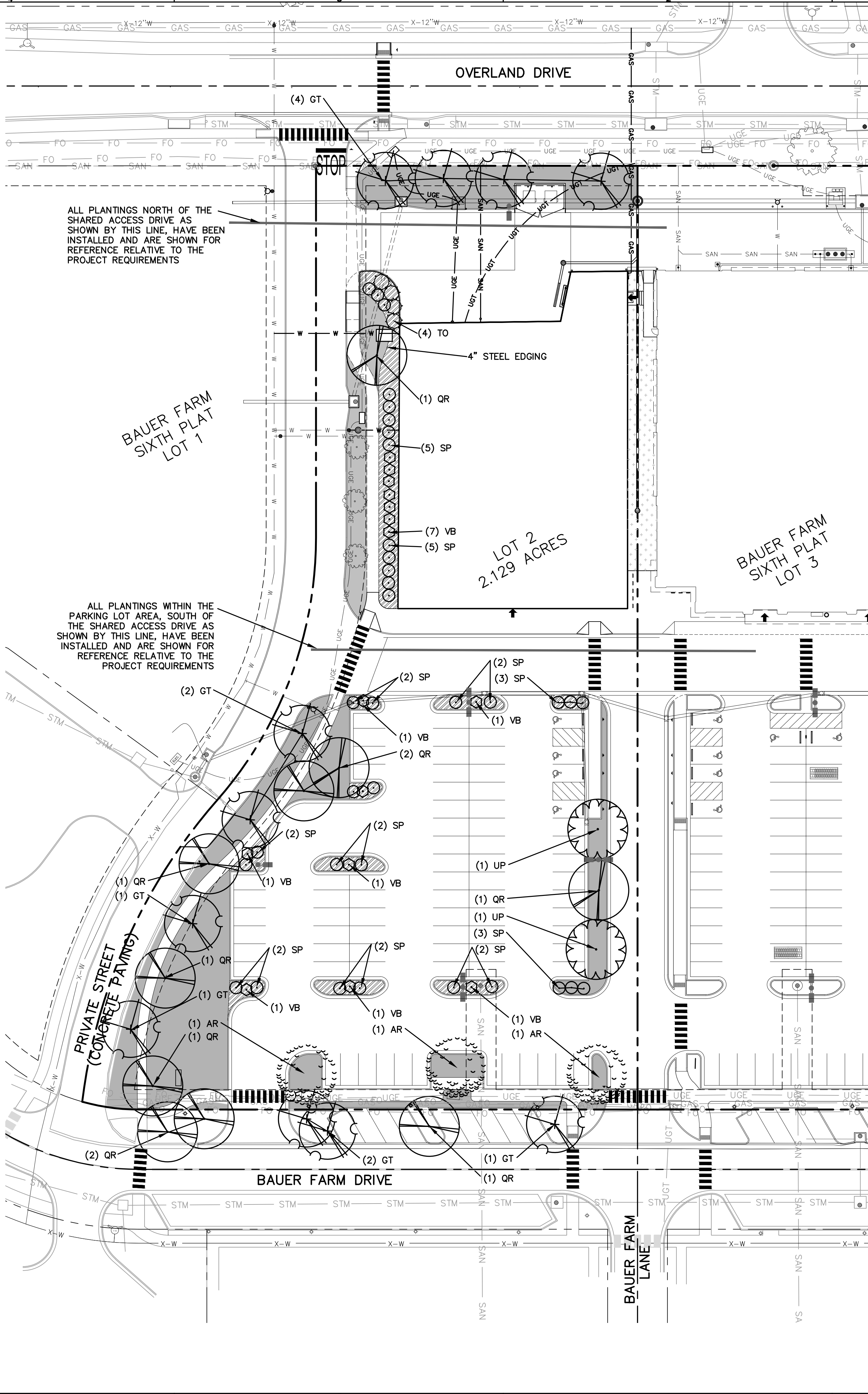
REVISIONS
7.7.16 REVISED PER CITY
REVIEW COMMENTS
7.13.16 REVISED PER CITY
REVIEW COMMENTS
7.18.16 REVISED PER CITY
REVIEW COMMENTS






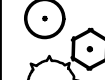


Rel. Date: 05.23.2016

GRADING AND UTILITY PLAN

2

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[illegible]

PLANT SCHEDULE					
SYMBOL	KEY	QTY.	NAME	SIZE	COND.
		0	EXISTING TREE --NOTE: 19 TREES PLANTED UNDER PREVIOUS FDP.		
		0	EXISTING TREE TO REMAIN		
		20	SHADE TREE		
	UP	2	DYNASTY ELM - ULMUS PARIFOLIA 'DYNASTY'		
	QR	8	NORTHERN RED OAK - QUERCUS RUBRA		
	AR	3	PACIFIC SUNSET MAPLE - ACER RUBRUM 'PACIFIC SUNSET'		
	GT	7	SKYLINE THORNLESS LOCUST - GLEDITSIA TRICANTHOS 'SKYLINE'		
		53	DECIDUOUS/EVERGREEN SHRUBS/ORNAMENTAL GRASSES/PERENNIALS		
	SP	32	GOLDMOUND SPIREA - SPIRAEA JAPONICA 'GOLDMOUND'	2 GAL.	CONT.
	VB	15	MOWHAWK VIBURNUM - VIBURNUM X BURKWOODII X CORLESII 'MOWHAWK'		
	TO	4	EMERALD GREEN PYRAMIDAL ARBORVITAE - THUJA OCCIDENTALIS 'SMARAGD'	6"-8"	CONT.
		62 C.Y.	DOUBLE SHREDDED HARDWOOD MULCH (3" LAYER MIN.)	N/A	N/A
		24 C.Y.	RIVER ROCK (3" LAYER MIN.)	3"-4"	N/A
	N/A	10,131 S.F.	TURF TYPE FESCUE (SOD) --NOTE: LIGHTER HATCH INDICATES INSTALLED AREAS	SOD	N/A

NOTE: AS NOTED ON THE PLAN, A MAJORITY OF THE REQUIRED PLANT MATERIAL HAS BEEN INSTALLED, PER THE ORIGINAL FDP FOR BAUER FARM NORTHWEST - PHASE ONE. WITHIN THE PARKING AREAS AND NORTH OF THE SERVICE DRIVE. ANY PLANT MATERIAL PREVIOUSLY PLANTED THAT IS DEAD OR DAMAGED BY CONSTRUCTION SHALL BE REPLACED PER THE PLANT SCHEDULE ABOVE.

LANDSCAPE AREA REQUIRED OF STALLS	TREES SHRUBS REQUIRED	TREE SHRUBS STALLS
LOT 2 - 77 STALLS PROVIDED	LOT 2 - 77 / 10 = 7.7 (8)	
REQUIRED: 4,620 SQ. FT.	REQUIRED: 8 TREES, & 24 SHRUBS	
PROVIDED: 4,215 SQ. FT.	PROPOSED: 10 TREES, & 39 SHRUBS	

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Final Development Plan For

Bauer Farm Northwest - PetSmart

4820 Bauer Farm Drive, Lawrence, Kansas 66049

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Name:	Date:
Name:	Date:



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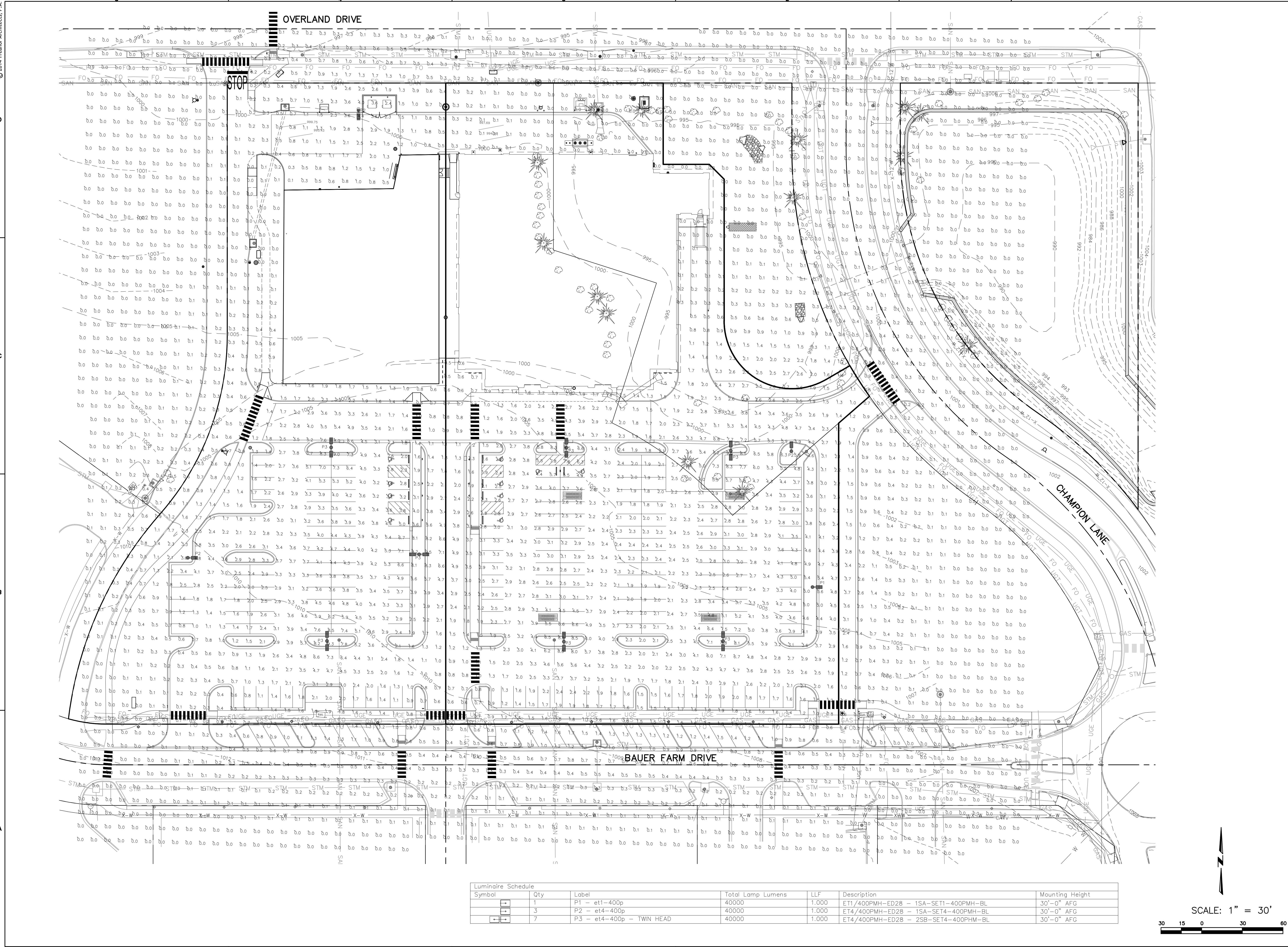
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REVIEW COMMENTS
7.13.16 REVISED PER CITY
REVIEW COMMENTS
7.18.16 REVISED PER CITY
REVIEW COMMENTS

Rel. Date: 05.23.2016

LANDSCAPE PLAN

3

FILE PATH: P:\CE14.002.00B Bauer Farms Grocer - NW Corner Champion- 3 Drawings\AutoCAD\PLANNING\Final Development Plan\04 PHOTOMETRIC PLAN.DWG
DATE PRINTED: Monday, July 18, 2016
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PRINTED BY: Brian Kemp



Luminaire Schedule						
Symbol	Qty	Label	Total Lamp Lumens	LLF	Description	Mounting Height
	1	P1 - et1-400p	40000	1.000	ET1/400PMH-ED28 - 1SA-SET1-400PMH-BL	30'-0" AFG
	3	P2 - et4-400p	40000	1.000	ET4/400PMH-ED28 - 1SA-SET4-400PMH-BL	30'-0" AFG
	7	P3 - et4-400p - TWIN HEAD	40000	1.000	ET4/400PMH-ED28 - 2SB-SET4-400PHM-BL	30'-0" AFG

CE14.002.00B

Final Development Plan For
Bauer Farm Northwest - Phase I
4700 Block of Bauer Farm Drive, Lawrence, Kansas 66049

APPROVED BY:

Name: _____ Date: _____

Name: _____ Date: _____

Name: _____ Date: _____

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PHOTOMETRIC
PLAN

5

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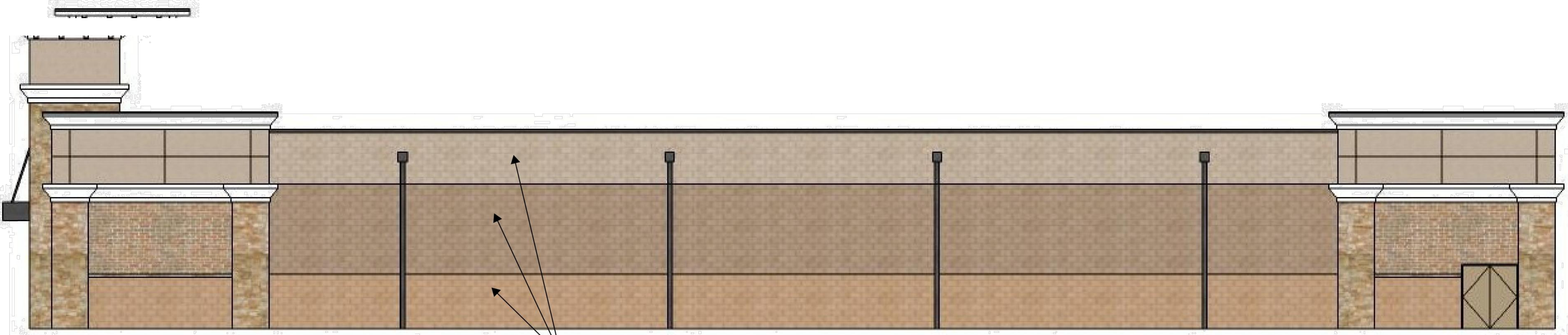
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FRONT ELEVATION



REAR ELEVATION

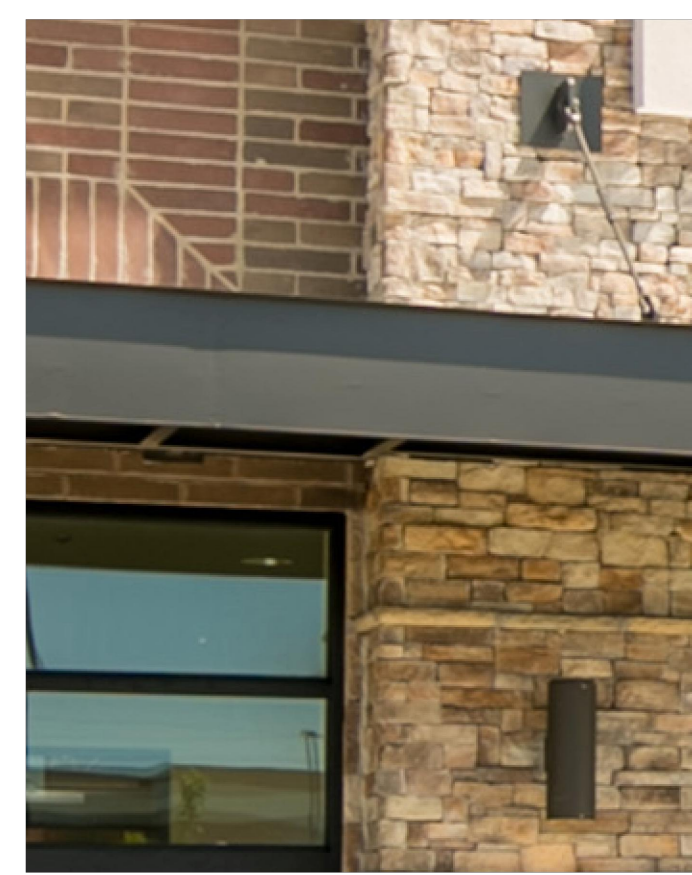


RIGHT SIDE ELEVATION



LEFT SIDE ELEVATION

NOTE:
BUILDING SIGNS TO BE FINALIZED WITH SIGNAGE
PERMIT



(Materials to match Sprouts)



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LAWRENCE-DOUGLAS COUNTY PLANNING COMMISSION

SCOTT MCCULLOUGH, PLANNING DIRECTOR
LAWRENCE-DOUGLAS COUNTY PLANNING AND
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Final Development Plan For
Bauer Farm Northwest - PetSmart
4820 Bauer Farm Drive, Lawrence, Kansas 66049

APPROVED BY:	
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REVIEW COMMENTS	
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REVIEW COMMENTS	

Rel. Date: 05.23.2016

BUILDING
ELEVATIONS

4

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Bauer Farm Northwest - Phase I

4700 Block of Bauer Farm Drive, Lawrence, Kansas 66049

Final Development Plan For

KEY NOTES

- 6" CONCRETE CURB & GUTTER
- CONCRETE LAYDOWN CURB & GUTTER
- CONCRETE VALLEY GUTTER
- ACCESSIBLE SIDEWALK RAMP
- ACCESSIBLE PARKING STALLS. INCLUDES AT LEAST ONE VAN-ACCESSIBLE STALL PER BAY.
- CONCRETE SIDEWALK.
- CONCRETE COMMERCIAL DRIVEWAY APPROACH PER CITY OF LAWRENCE STANDARD.
- PUBLIC ENTRANCE TO BUILDING.
- GARBAGE COMPACTOR.
- SCHOOL CROSSING SIGN.
- 4" WIDE WHITE PAINTED PARKING STALL MARKINGS.
- CROSSWALK MARKINGS.
- 6" MONOLITHIC CURB.
- 2' STOP BAR AND "STOP" MARKING.
- STOP SIGN. POLE TO MATCH EXISTING SIGN POLES IN BAUER FARM.
- REDI-ROCK LEDGESTONE RETAINING WALL. SEE PAGE 2 FOR ELEVATIONS.
- DUMPSTER ENCLOSURE. BRICK WITH STONE ACCENT TO MATCH BUILDINGS.
- SCREEN WALL. CONCRETE MASONRY UNITS WITH STONE VENEER. REFER TO BUILDING ELEVATIONS FOR WALL HEIGHT.
- 4' LOADING DOCK.
- CONCRETE-FILLED PIPE BOLLARD.
- GROCERY CART CORRAL.
- BICYCLE RACK. LANDSCAPE FORMS "PI" TYPE. POWDER COATED BLACK, IN-GROUND MOUNTED.
- BENCH.
- REFUSE RECEPTACLE.
- LANDSCAPED AREA. REFER TO LANDSCAPE PLAN.
- EXISTING CURB & GUTTER TO REMAIN.
- EXISTING SIDEWALK TO REMAIN.
- EXISTING BRIDGE TO REMAIN.
- 4' WIDE CURB NOTCH FOR DRAINAGE.
- PEDESTRIAN LIGHT. TYPE TO MATCH EXISTING LIGHTS ALONG SOUTH SIDE OF BAUER FARM DRIVE.
- PARKING LOT LIGHT.

PARKING REQUIREMENTS

	OFF-STREET STALLS REQ'D	OFF-STREET STALLS PROVIDED	ADA STALLS REQ'D	ADA STALLS PROVIDED	BICYCLE STALLS REQUIRED	BICYCLE STALLS PROVIDED
INLINE RETAIL (LOT 2)	57	88	4	4	9	10
GROCER (LOT 3)	134	135	5	6	13	14

NOTE:

- AN ADDITIONAL 28 ON-STREET STALLS ALONG BAUER FARM DRIVE ARE TO BE CONSTRUCTED WITH THIS PROJECT.
- SHARED PARKING IS PART OF OVERALL DEVELOPMENT AND COVENANT.

LEGAL DESCRIPTION

LOTS 2, 3 AND TRACT 'A' IN BLOCK 1, BAUER FARM SIXTH PLAT, A SUBDIVISION IN THE SOUTHEAST QUARTER OF SECTION 28, TOWNSHIP 12 SOUTH, RANGE 19 EAST OF THE SIXTH PRINCIPAL MERIDIAN IN THE CITY OF LAWRENCE, DOUGLAS COUNTY, KANSAS, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 18 AT PAGE 701 IN THE OFFICE OF THE REGISTER OF DEEDS IN THE COURTHOUSE OF SAID COUNTY AND STATE. CONTAINS 225,478 SQUARE FEET OR 5.176 ACRES, MORE OR LESS.

GENERAL NOTES

- THIS PROJECT HAS BEEN DESIGNED TO COMPLY WITH THE PROVISIONS OF THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) FOR BUILDING AND FACILITIES, APPENDIX A TO 28 CFR PART 36.
- CITY OF LAWRENCE WILL NOT BE HELD RESPONSIBLE FOR PAVEMENT DAMAGE DUE TO REFUSE COLLECTION.
- OCCUPANCY PERMITS FOR PROPOSED BUILDINGS WILL NOT BE ISSUED UNTIL THE REQUIRED PUBLIC DRAINAGE IMPROVEMENTS ARE COMPLETE, FINAL INSPECTED AND ACCEPTED BY THE PUBLIC WORKS DEPARTMENT.
- THE RETENTION BASIN WILL BE PRIVATELY-OWNED AND MAINTAINED. THE LANDOWNER IS RESPONSIBLE FOR THE MAINTENANCE OF THE RETENTION BASIN. FAILURE TO MAINTAIN THE RETENTION BASIN WILL RESULT IN THE LOSS OF THE STORMWATER DETENTION CREDIT. THE RETENTION BASIN WILL REMAIN FREE OF ANY NATURAL OR NON-NATURAL STRUCTURES OR VEGETATIVE BARRIERS (INCLUDING BUT NOT LIMITED TO TREES, SHRUBBERY, BERMS, FENCES, AND WALLS).
- ALL CONCRETE BOX CURB INLETS WILL BE CONSTRUCTED PER CITY OF LAWRENCE STORM SEWER STANDARD DETAILS.

PAVEMENT LEGEND

- HEAVY-DUTY REINFORCED CONCRETE (STREETS, DRIVE APRONS, TRUCK ZONES)
- LIGHT-DUTY REINFORCED CONCRETE (ON-STREET PARKING STALLS)
- UNREINFORCED CONCRETE (SIDEWALKS)
- HEAVY-DUTY ASPHALT PAVEMENT (LOADING AREA)
- ASPHALT PAVEMENT (PARKING LOT)

CHAIR
LAWRENCE-DOUGLAS COUNTY PLANNING COMMISSION

SECRETARY
LAWRENCE-DOUGLAS COUNTY PLANNING COMMISSION

SCALE: 1" = 30'



LOT 2

EXISTING	PROPOSED
BUILDINGS 0 S.F. (0%)	11,623 S.F. (12%)
PAVEMENT 1,928 S.F. (2%)	64,532 S.F. (70%)
IMPERVIOUS 1,928 S.F. (2%)	76,155 S.F. (82%)
PERVIOUS 90,801 S.F. (98%)	16,574 S.F. (18%)
LOT AREA 92,729 S.F.	92,729 S.F.

CURRENT OWNER
FREESTATE GROUP, LLC
CONTACT PERSON: MICHAEL L. TREANOR
1040 VERMONT STREET
LAWRENCE, KANSAS 66044
(785) 371-0781

LOT 3

EXISTING	PROPOSED
BUILDINGS 0 S.F. (0%)	27,275 S.F. (23%)
PAVEMENT 0 S.F. (0%)	76,586 S.F. (66%)
IMPERVIOUS 0 S.F. (0%)	103,861 S.F. (89%)
PERVIOUS 116,582 S.F. (100%)	12,721 S.F. (11%)
LOT AREA 116,582 S.F.	116,582 S.F.

DEVELOPER / PROPOSED OWNER
WAKARUSA INVESTORS, LLC
CONTACT PERSON: DAVID GEIST
850 EAST 13TH STREET, SUITE 415
CENTENNIAL, COLORADO 80122
(720) 489-0424

TRACT 'A'

EXISTING	PROPOSED
BUILDINGS 0 S.F. (0%)	0 S.F. (0%)
PAVEMENT 0 S.F. (0%)	0 S.F. (0%)
IMPERVIOUS 0 S.F. (0%)	0 S.F. (0%)
PERVIOUS 16,167 S.F. (100%)	16,167 S.F. (100%)
LOT AREA 16,167 S.F.	16,167 S.F.

LAND SURVEYOR
COOK, FLATT & STROBEL ENGINEERS, P.A.
CONTACT PERSON: KIRK R. BALDWIN, LS
850 EAST 13TH STREET, SUITE B
LAWRENCE, KANSAS 66044
(785) 856-9600

CIVIL ENGINEER

TREANOR ARCHITECTS, P.A.
CONTACT PERSON: MATTHEW L. MURPHY, PE
1040 VERMONT STREET
LAWRENCE, KANSAS 66044
(785) 727-2407

ARCHITECT

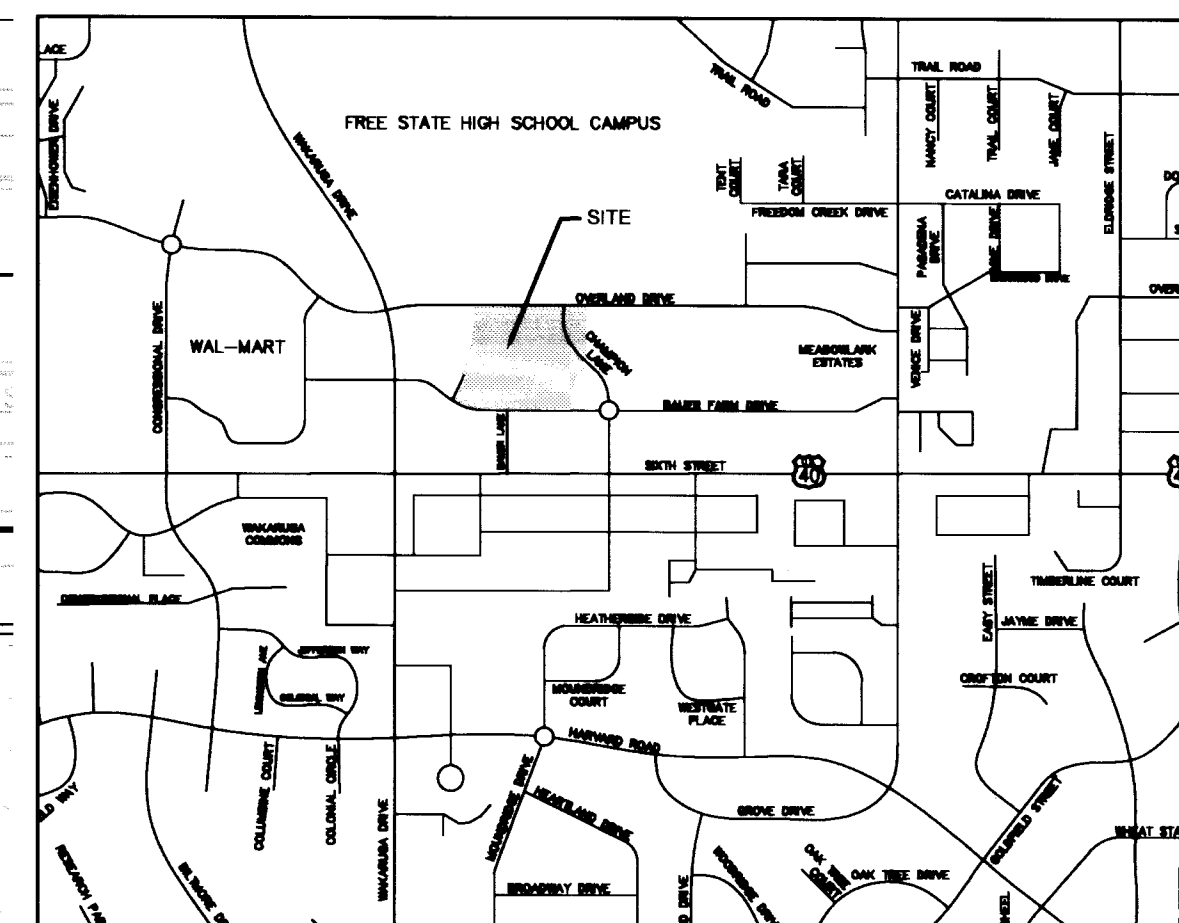
CAHEN ARCHITECTURAL GROUP, P.C.
CONTACT PERSON: CRAIG I. CAHEN, AIA
7076 SOUTH ALTON WAY, BUILDING A
CENTENNIAL, COLORADO 80112
(303) 743-0002 EXT. 211

FILE PATH: P:\CE14.002.00B Bauer Farm Forms Grocer - NW Corner Champion-3 Drawings\1_AutoCAD\PLANNING\Final Development Plan\01 SITE LAYOUT PLANDWG
DATE PRINTED: Thursday, July 24, 2014
LAST SAVED: Wednesday, June 25, 2014
PRINTED BY: Matt Murphy

Final Development Plan For

Bauer Farm Northwest - Phase I

4700 Block of Bauer Farm Drive, Lawrence, Kansas 66049

VICINITY MAP
SCALE: 1" = 1000'

GRADING AND UTILITY LEGEND

EXISTING CABLE TV LINE
EXISTING FIBER OPTIC LINE
EXISTING NATURAL GAS MAIN
EXISTING SANITARY SEWER MAIN
EXISTING STORM SEWER PIPE
EXISTING UNDERGROUND ELECTRIC LINE
EXISTING UNDERGROUND TELEPHONE LINE
EXISTING WATER MAIN
EXISTING 12" WATER MAIN

PROPOSED GAS LINE
PROPOSED SANITARY SEWER LINE
PROPOSED UNDERGROUND ELECTRIC LINE
PROPOSED UNDERGROUND TELEPHONE LINE
PROPOSED WATER LINE
BOTTOM OF WALL ELEVATION
PROPOSED ELECTRIC METER
PROPOSED GAS METER
PROPOSED WATER METER
PROPOSED CLEANOUT
PROPOSED DOWNSPOUT
FINISHED FLOOR ELEVATION
PROPOSED STORM SEWER JUNCTION BOX
PROPOSED STORM SEWER MANHOLE
PROPOSED STORM SEWER CONCRETE BOX CURB INLET
PROPOSED 2'x3' NYLOPLAST CURB INLET
WITH HIGH-FLOW VANE GRATE
PROPOSED SANITARY SEWER SAMPLING PORT
PROPOSED STORM SEWER LINE WITH END PLUG

FOR FUTURE SERVICE TO ADJACENT LOT
TOP OF WALL ELEVATION

UTILITY NOTES

1. ALL STORM SEWER PIPING WILL BE HIGH-DENSITY POLYETHYLENE (HDPE) DUAL-WALL PIPE WITH SMOOTH INTERIOR.
2. ALL SANITARY SEWER PIPING WILL BE STANDARD DIMENSION RATIO (SDR) 26 POLYVINYL CHLORIDE (PVC).
3. DOMESTIC WATER SERVICE LINES WILL BE SDR-9 HDPE
4. FIRE SPRINKLER SERVICE LINES AND WATER MAINS MUST BE RATED AS DR14 AS DEFINED IN AWWA C900.
5. OCCUPANCY PERMITS FOR PROPOSED BUILDINGS WILL NOT BE ISSUED UNTIL THE REQUIRED PUBLIC DRAINAGE IMPROVEMENTS ARE COMPLETE, INSPECTED AND ACCEPTED BY THE PUBLIC WORKS DEPARTMENT.
6. THE RETENTION BASIN WILL BE PRIVATELY OWNED AND MAINTAINED. THE LAND OWNER IS RESPONSIBLE FOR THE MAINTENANCE OF THE RETENTION BASIN. FAILURE TO MAINTAIN THE RETENTION BASIN WILL RESULT IN THE LOSS OF THE STORMWATER DETENTION CREDIT. THE RETENTION BASIN WILL REMAIN FREE OF ANY NATURAL OR NON-NATURAL STRUCTURES OR VEGETATIVE BARRIERS (INCLUDING BUT NOT LIMITED TO TREES, SHRUBBERY, BERMS, FENCES, AND WALLS)
7. ALL CURB INLETS WILL BE CONSTRUCTED PER CITY OF LAWRENCE STORM SEWER STANDARD DETAILS.

BENCHMARKS

- BM #1 CHISELED SQUARE ON THE CENTER FRONT FACE OF THE FIRST CURB INLET EAST OF THE ENTRANCE TO PEOPLE'S BANK (4831 WEST SIXTH STREET) ON THE SOUTH SIDE OF 6TH STREET (U.S. HIGHWAY 40)
ELEVATION: 1012.98 FEET A.M.S.L.
- BM #2 CHISELED SQUARE ON THE CENTER FRONT FACE OF THE CURB INLET IN THE SOUTHEAST QUADRANT OF THE INTERSECTION OF OVERLAND DRIVE AND WAKARUSA DRIVE.
ELEVATION: 1002.75 FEET A.M.S.L.



SCALE: 1" = 30'



APPROVED BY:

Name: _____ Date: _____
Name: _____ Date: _____
Name: _____ Date: _____



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1040 Vermont Street
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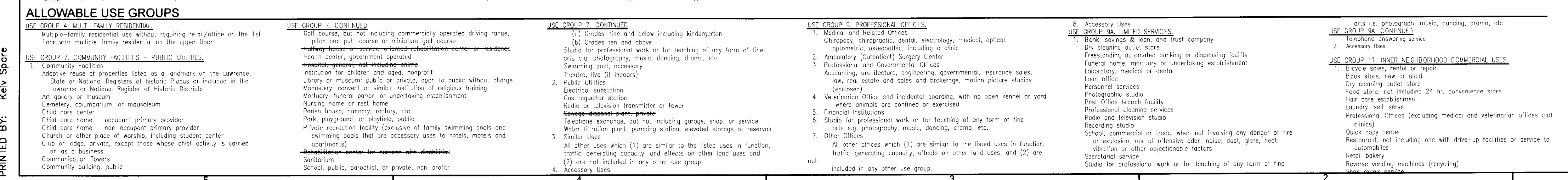
REVISIONS

NO.	REVISION
1	CITY COMMENTS 2014-08-13
2	
3	
4	

Rel. Date: 05.14.2014

GRADING AND
UTILITY PLAN

2



INTERIOR PARKING LANDSCAPE SUMMARY			
LANDSCAPE AREA & REQUIRED: # OF STALLS x 60 S.F.		TREES & SHRUBS REQUIRED: 1 TREE & 3 SHRUBS / 10 STALLS	
LOT 1 - 21 STALLS PROVIDED	REQUIRED: 5,460 SQ. FT. PROVIDED: 4,215 SQ. FT.	LOT 2 - 91 / 10 = 9 (10)	REQUIRED: 10 TREES & 30 SHRUBS PROPOSED: 10 TREES, & 30 SHRUBS
LOT 3 - 133 STALLS PROVIDED	REQUIRED: 7,980 SQ. FT. PROVIDED: 3,417 SQ. FT.	LOT 3 - 133 / 10 = 13.3 (14)	REQUIRED: 14 TREES, & 42 SHRUBS PROPOSED: 15 TREES, & 42 SHRUBS

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DATE: Wednesday, June 23, 2014
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PRINTED BY: Kev Spore

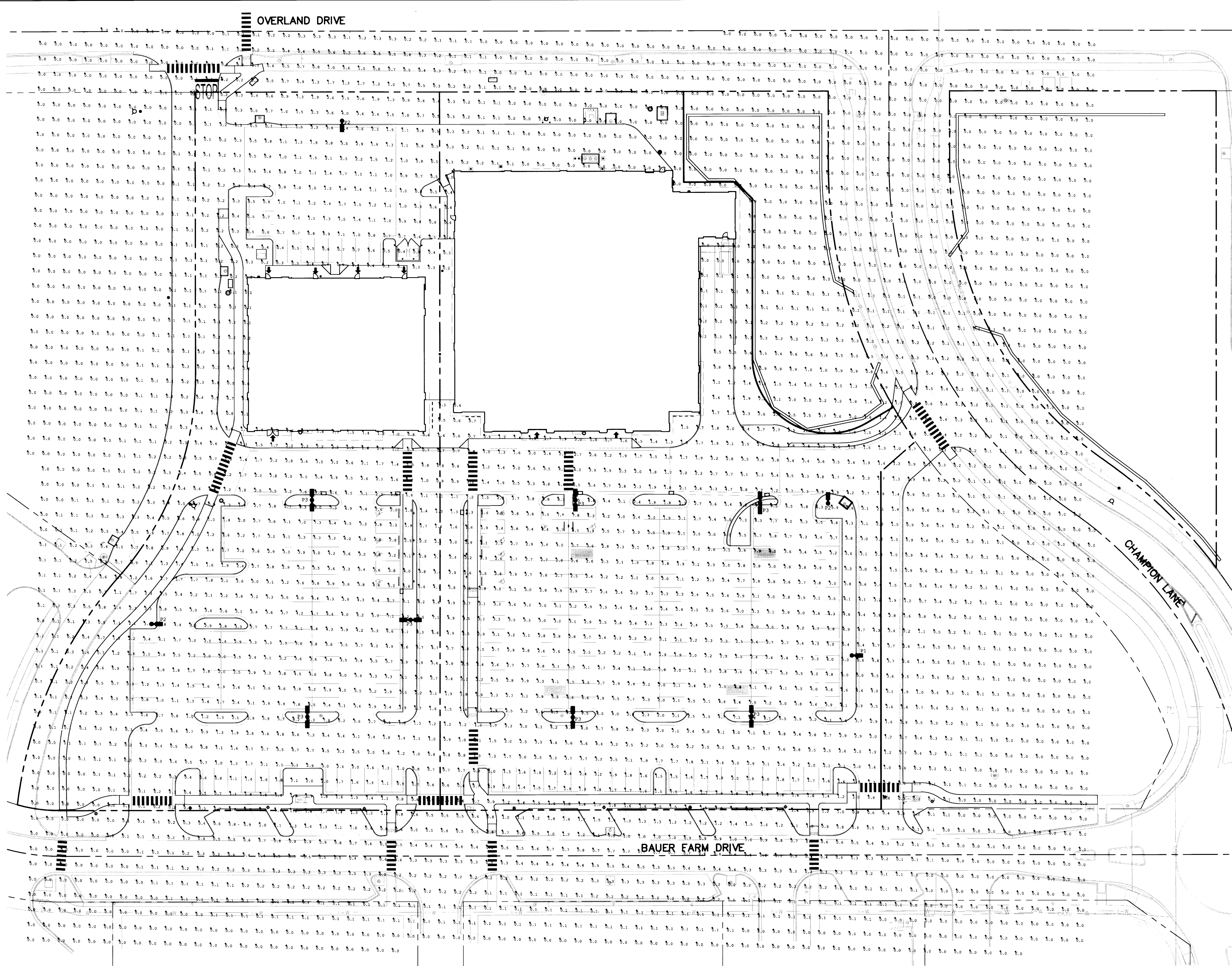
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B

A



SCALE: 1" = 30'



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Final Development Plan For
Bauer Farm Northwest - Phase I
4700 Block of Bauer Farm Drive, Lawrence, Kansas 66049

APPROVED BY:

Name: _____ Date: _____
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Name: _____ Date: _____

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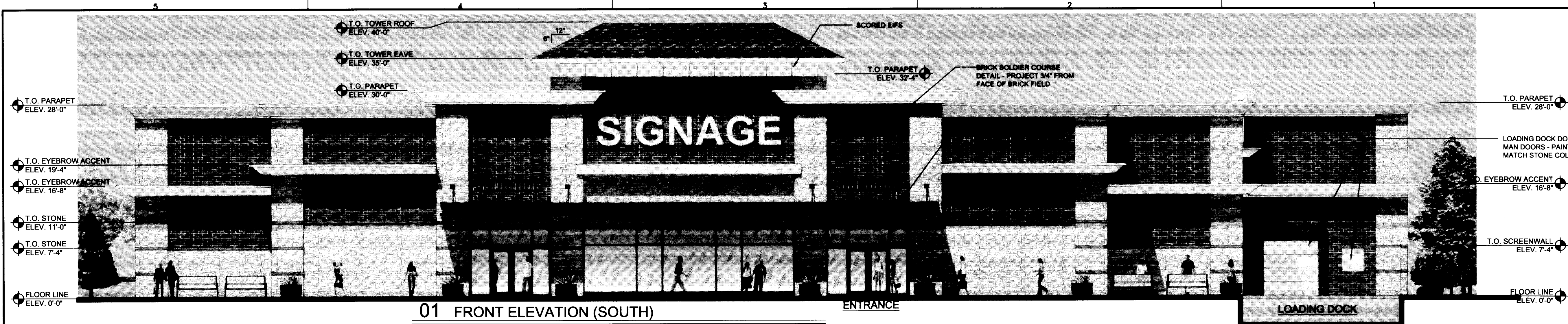
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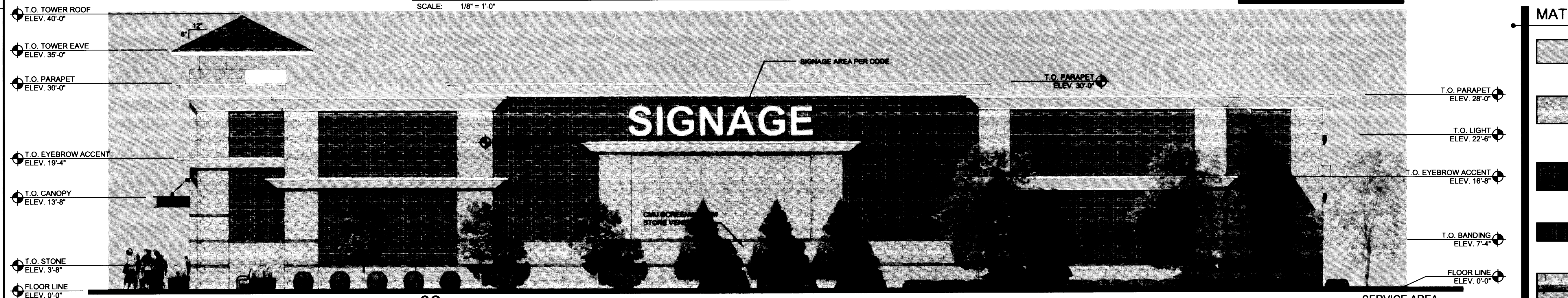
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PLAN

4

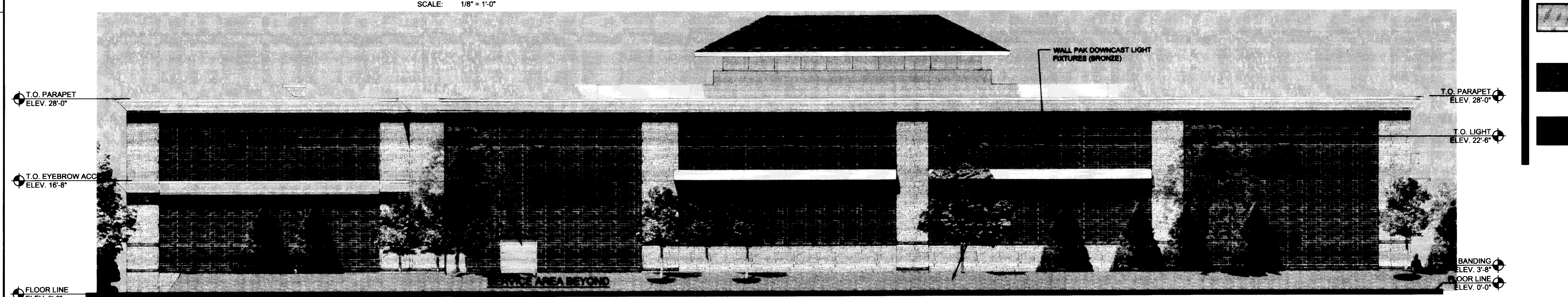
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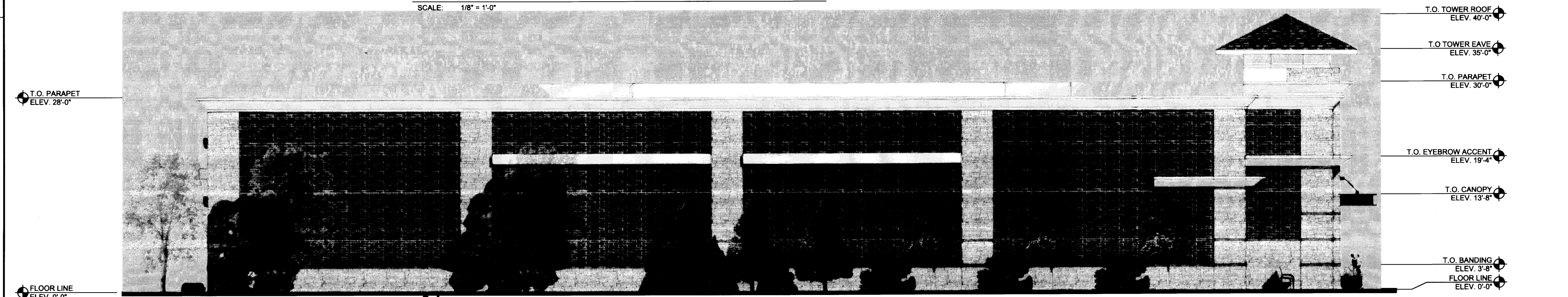
01 FRONT ELEVATION (SOUTH)



02 RIGHT SIDE ELEVATION (EAST)



03 REAR ELEVATION (NORTH)



04 LEFT SIDE ELEVATION (WEST)

MATERIALS & COLOR LEGEND

- E.I.F.S. (SANDBLASTED FINISH) COLOR: OFF-WHITE
- CULTURED STONE VENEER (LEDGESTONE - DRY STACK) COLOR: "BAUER FARMS BLEND"
- BRICK (THRU-WALL) COLOR: REDDISH-BROWN
- SOLDIER COURSE BRICK ACCENT (PROJECT 3/4" FROM FACE OF FIELD BRICK) COLOR: REDDISH-BROWN
- STONE BANDING / SILL COLOR: MEDIUM BROWN
- STOREFRONT SYSTEM W/ CLEAR NON-REFLECTIVE GLASS FRAMING COLOR: DARK ANODIZED BRONZE
- CONCRETE TILE ROOFING COLOR: SLATE GRAY
- PAINTED STEEL CANOPY COLOR: FOREST GREEN

Final Development Plan For
Bauer Farm Northwest - Phase I
4700 Block of Bauer Farm Drive, Lawrence, Kansas 66049

APPROVED BY:
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Name: _____ Date: _____
Name: _____ Date: _____


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1	10/14/2014
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3	
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Rel. Date: 05.14.2014
EXTERIOR ELEVATIONS
ANCHOR BUILDING

Final Development Plan For
Bauer Farm Northwest - Phase I
4700 Block of Bauer Farm Drive, Lawrence, Kansas 66049

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2	
3	
4	

Rel. Date: 05.14.2014

EXTERIOR ELEVATIONS
RETAIL BUILDING

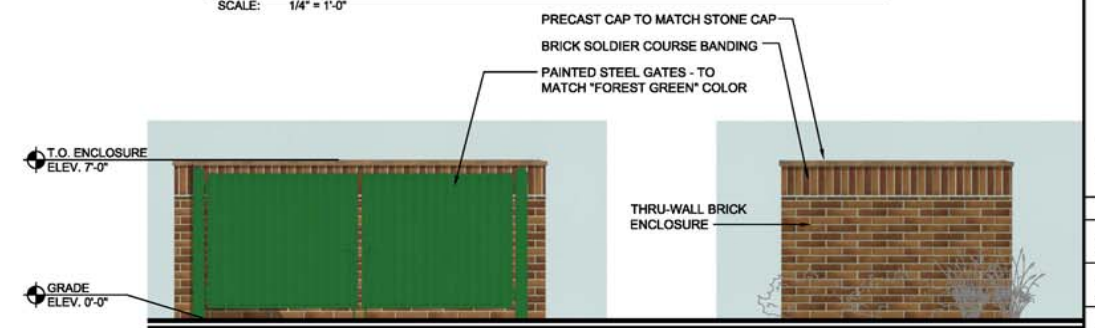
MATERIALS & COLOR LEGEND

- E.I.F.S. (SANDBLASTED FINISH)
COLOR: OFF-WHITE
- CULTURED STONE VENEER
(LEDGESTONE - DRY STACK)
COLOR: "BAUER FARMS BLEND"
- BRICK (THRU-WALL)
COLOR: REDDISH-BROWN
- SOLDIER COURSE BRICK
ACCENT (PROJECT 3/4" FROM
FACE OF FIELD BRICK)
COLOR: REDDISH-BROWN
- STONE BANDING / SILL
COLOR: MEDIUM BROWN
- STOREFRONT SYSTEM W/
CLEAR NON-REFLECTIVE
GLASS
FRAMING COLOR: DARK
ANODIZED BRONZE
- CONCRETE TILE ROOFING
COLOR: SLATE GRAY
- PAINTED STEEL CANOPY
COLOR: FOREST GREEN
- WALL SCONCE LIGHT FIXTURE
COLOR: DARK BRONZE
- WALL PAK LIGHT FIXTURE
COLOR: DARK BRONZE



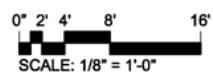
05 MONUMENT SIGN

SCALE: 1/4" = 1'-0"



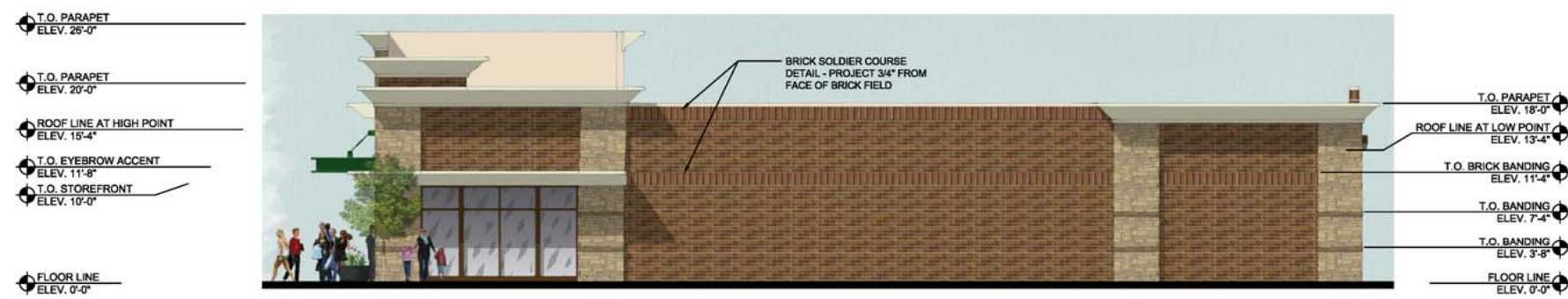
06 TRASH ENCLOSURE

SCALE: 1/4" = 1'-0"



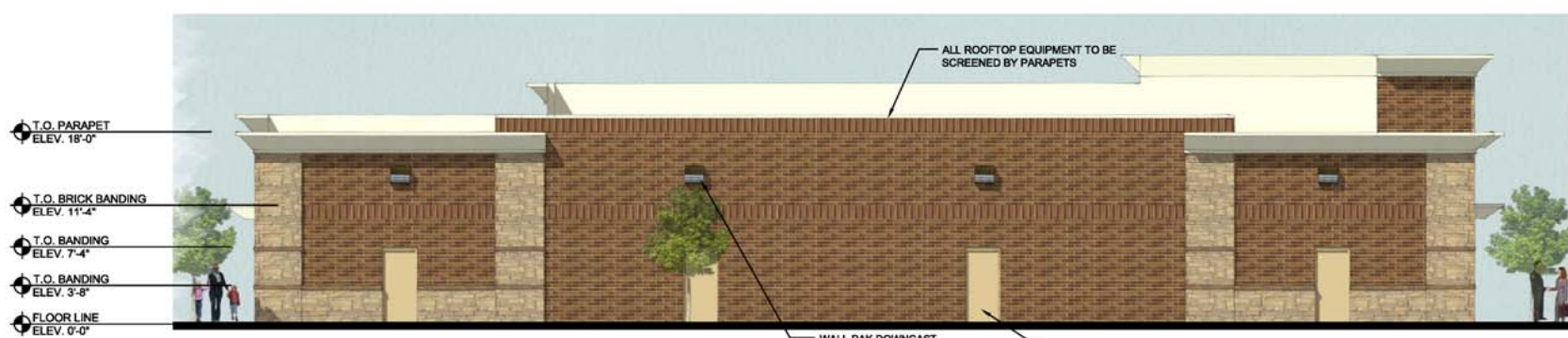
01 FRONT ELEVATION (SOUTH)

SCALE: 1/8" = 1'-0"



02 RIGHT SIDE ELEVATION (EAST)

SCALE: 1/8" = 1'-0"



03 REAR ELEVATION (NORTH)

SCALE: 1/8" = 1'-0"



04 LEFT SIDE ELEVATION (WEST)

SCALE: 1/8" = 1'-0"

Memorandum

City of Lawrence/Douglas County

Planning and Development Services

TO: Lawrence – Douglas County Planning Commission
Lawrence Historic Resources Commission

FROM: Jeff Crick, AICP, Planner II

CC: Scott McCullough; Planning and Development Services Director
Lynne Braddock Zollner, AICP; Historic Resources Administrator

Date: For the June 16th, 2016 Historic Resources Commission Meeting, and
For the July 25th, 2016 Planning Commission Meeting

RE: *Oread Neighborhood Design Guidelines* Rehearing Process and Outline

Background

The Lawrence – Douglas Co. Planning Commission and the Lawrence Historic Resources Commission held two joint meetings to discuss the proposed *Oread Neighborhood Design Guidelines* on [Thursday, February 18th, 2016](#) and [Monday, March 21st, 2016](#). Both commissions recommended approval of the guidelines and associated rezoning requests to implement them. After the official hearing of March 21st, 2016, staff discovered that the rezoning effort was not properly noticed in the neighborhood and a re-hearing must be completed.

Rehearing Process

To proceed with the adoption process for the *Oread Neighborhood Design Guidelines*, staff will restart the public notice and hearing process for both the Historic Resources Commission and the Planning Commission. This entails hearings before both recommending bodies, not in a joint format as was conducted previously in March. The items will be submitted to the City Commission after the re-hearings occur, which staff anticipates will be July at the earliest.

The issue does not stem from a fault with the Guidelines document, nor the recommendations by either commission. Instead, this stems from a process concern of not placing notice signs throughout the neighborhood as required by code. Proper notice and a rehearing of the items are required to remedy the situation. Staff has modified the *Oread Neighborhood Design Guidelines* based on the comments and recommendations from the March joint meeting, and anticipates a similar finding by the commissions as this process continues.

Document Updates

Since the meetings in the spring of 2016, Staff has incorporated the changes recommended at the joint meeting, while correcting grammatical and typographical errors. The major changes since that meeting are listed below:

1. Inclusion of nonconformity text to page 53
2. Inclusion of text indicating Oread Neighborhood Design Guidelines precedence over the Land Development Code when applicable to page 53
3. Correcting a District identification label on page 60: from District 5 to District 2

Lawrence/Douglas County Planning Office

OREAD NEIGHBORHOOD DESIGN GUIDELINES CITY OF LAWRENCE, KANSAS

6 JUNE 2016



Urban Development Services
Ann Benson McGlone, LLC.

LAWRENCE-DOUGLAS COUNTY PLANNING OFFICE

OREAD NEIGHBORHOOD DESIGN GUIDELINES

June 2016

URBAN DEVELOPMENT SERVICES + ANN BENSON MCGLONE, LLC

OREAD NEIGHBORHOOD DESIGN GUIDELINES

ACKNOWLEDGMENTS

CITY COMMISSION

2012

*Robert Schumm, Mayor
Michael Dever, Vice Mayor
Aron Cromwell
Hugh Carter
Mike Amyx*

2016

*Mike Amyx, Mayor
Leslie Soden, Vice Mayor
Stuart Boley
Matthew Herbert
Lisa Larsen*

LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION

2012

*Bruce Liese, Chair
Bryan Culver, Vice Chair
Chad Lamer
Charles Blaser
Clay Britton
Jon Josserand
Lara Adams Burger
Pennie von Achen
Richard Hird*

2016

*Clay Britton, Chair
Patrick Kelly, Vice Chair
Bryan Culver
Julia Butler
Jim Denney
Pennie von Achen
Bruce Liese
Eric Struckhoff
Rob Sands
James Carpenter*

HISTORIC RESOURCES COMMISSION

2012

*Aaron Bailey
Chad Foster
Leslie Tuttle
Michael Arp
Sean Williams
Stan Hernly
Tracy Quillin*

2016

*Chad Foster
Aaron Bailey
Michael Arp
Stan Hernly
Tracy Quillin
Keny Fry
Brenna Buchanan Young*

OREAD DESIGN GUIDELINES SUBCOMMITTEE

*Bryan Culver,
Jon Josserand
Chad Foster
Stan Hernly*

PLANNING AND DEVELOPMENT SERVICES DEPARTMENT

*Scott McCullough, Director
Amy Miller, Assistant Director
Sheila Stogsdill, Assistant Director
Michelle Leininger
Lynne Braddock Zollner
Jeff Crick*

OREAD NEIGHBORHOOD COMMUNITY

*Oread Neighborhood Association
Oread Residents Association*

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ORGANIZATION AND USE OF GUIDELINES



INTRODUCTION

The introduction to these Guidelines provides a very brief background and history of the Oread Neighborhood, a description of the forces that have shaped it over time and the factors leading to the preparation of these Design Guidelines.

It is recommended that all users of the Guidelines review this section to gain an understanding of the essential design qualities of the Oread Neighborhood that are important to the community.

CHAPTER 1

USER'S GUIDE

Chapter 2 provides basic information on the use of the Guidelines including a description of the processes for new construction, infill redevelopment, and renovation of existing buildings and property. Specifically, this section addresses Design Review, Permitting, Variances and Penalties for Violations.

CHAPTER 2

CHARACTER OF THE NEIGHBORHOOD

Chapter 3 contains a general description of the character defining features of the neighborhood in three important areas.

- The public realm includes the areas that are outside the property lines, such as streets, sidewalks and alleys, landscaping, historic elements and variations by districts.
- Individual lot and site character is defined by configuration and the relationship of the principal building to the street and side yards.
- Architectural styles and related scale and size of buildings are also identified.

CHAPTER 3

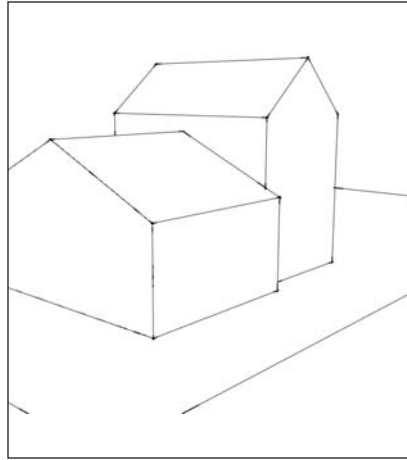


NEIGHBORHOOD-WIDE DESIGN GUIDELINES

Chapter 4 addresses guidelines that are applicable across the neighborhood in all districts. Guidelines address:

- Public realm – streets, sidewalks, parkways, front yard landscaping, retaining walls and fences, alleys and front yard setbacks.
- Site development guidelines address the relationship of lots/buildings to one another, the development of individual lots including open space, on-site parking, main and accessory structures.
- Building guidelines address the defining architectural elements specific to each building such as massing, roof shape and slope, porches, fenestration, building materials, details, additions, exterior lighting, emergency egress and sustainability features.

CHAPTER 4



DISTRICT SPECIFIC DESIGN GUIDELINES

Chapter 5 identifies guidelines for variations within Stadium North (District 1), High Density (District 2) and the two historic districts – Hancock (District 4) and Oread (District 5).

CHAPTER 5



COMMERCIAL & MIXED USE DESIGN GUIDELINES

Chapter 6 contains guidelines for non-residential properties, addressing site development, building design, location of parking and types and design of signage.

CHAPTER 6



CHAPTER 1: INTRODUCTION

The purpose of the Oread Neighborhood Design Guidelines is to provide a shared understanding of the principal design characteristics that help shape and define the Oread Neighborhood. Over time, as the neighborhood changes and evolves, these essential design characteristics will be maintained and enhanced to ensure that the unique and historic character of the neighborhood remains.

The Design Guidelines are intended to give owners, developers, design review board members, staff and elected officials a clear understanding of the expectations of the design review process. The Design Guidelines should make the review process more predictable and therefore more efficient.

The Design Guidelines are intended to encourage development that conforms to the size, orientation and setting of existing buildings in the neighborhood; reduce the need for zoning variances for structures that conform to the patterns of the existing neighborhood; identify the important physical characteristics of the neighborhood; foster development that is

compatible; conserve cultural and historic resources; maintain property values; and encourage investment.

The design guidelines are intended to be a resource to homeowners, property owners, architects, developers, Commissioners and City Staff. The guidelines should lay the groundwork for positive dialogue between the various stakeholders. It is anticipated that the guidelines will be a source of inspiration that will help property owners understand what it means to build structures that are compatible with the neighborhood.

The Goals for the Oread Neighborhood Design Guidelines are intended to capture the breadth and depth of the community's vision for their neighborhood, and can be a reminder of the neighborhood's ability to work together to achieve common solutions that benefit all. The goals were developed by the community during the public workshops.

1.A. BACKGROUND & 1.B. COMMUNITY PARTICIPATION



Much of the neighborhood is occupied by students living in traditional larger single-family residences.



The Oread Neighborhood Plan, approved in 2010, seeks to retain the character of the neighborhood.

A. BACKGROUND AND NEIGHBORHOOD PLAN

The Oread Neighborhood is situated between a thriving downtown and the University of Kansas. The neighborhood is attractive to home owners as well as students because of the proximity to both the university and downtown. These same attributes make it attractive to landlords, developers and others looking for investment opportunities. Each of these different interest groups has a vision and interest in the development of the neighborhood.

The Oread Neighborhood differs significantly from adjacent single family neighborhoods in that the majority of Oread Neighborhood is zoned multi-family. The higher density brings more concerns about parking, trash, privacy, noise and code compliance. As residences are enlarged or converted to rental units, issues have been raised concerning the design integrity of the neighborhood.

In an attempt to address these concerns the City/County Planning Commission recommended and the City Commission adopted a Neighborhood Plan for Oread in 2010. Through a lengthy inclusive process, many of the neighborhood issues and concerns were identified. An action plan was developed to systematically address these issues. It was recognized at that time that the Oread Neighborhood consisted of five distinct areas whereby design guidelines would be beneficial. Accordingly, an implementation plan was established to create overlay districts to address the specific needs of certain areas. These guidelines address the specific needs of each overlay area.

B. COMMUNITY PARTICIPATION

These guidelines were developed in a collaborative manner with input from community interest groups and City Staff. A community meeting and three community workshops were held to determine the community's interest, concerns, priorities and goals regarding the physical development of their neighborhood.

The meetings were attended by a mixture of people who both owned property and lived in the neighborhood, or who owned property and invested in the neighborhood. There was very little attendance by people who rented in the neighborhood. Most of the participants had been actively involved in the earlier neighborhood planning process.

Attendees were asked what was the one thing they would change about their neighborhood, what their favorite architectural feature was, what was the top design priority that needed to be resolved and what were their worries about adopting design guidelines.

The community also had an opportunity to react to a variety of housing types, densities and styles to help establish the level of community preferences, interest and support for alternative housing forms. The guidelines are a direct result of the community input.

These guidelines are exclusive to the Oread Neighborhood and are a direct reflection of the diversity and interest of the people who compose this distinctive place.



Initial community meeting on April 13, 2012.



Community workshop on April 14, 2012.



Community Workshop on April 14, 2012.

1.C. WHY HAVE DESIGN GUIDELINES & 1.D. HISTORY OF PLANNING



Guidelines should encourage changes that are compatible with the neighborhood.



Guidelines should encourage quality development.



Guidelines should protect unique character.

C. WHY HAVE DESIGN GUIDELINES?

- To sustain the unique character of a place
- Assure that changes to the neighborhood are in keeping with that character
- Encourage quality development which protects investment and helps attract other quality developers
- Establish clear development expectations for both neighbors and developers
- Reduce tensions related to development/redevelopment within the neighborhood
- Establish a framework for change
- Create a cohesive neighborhood attractive to both homeowners and developers
- Encourage creativity and a diverse range of architecture within a framework of community expectations

D. STRONG HISTORY OF COMMUNITY PLANNING

The Design Guidelines are built on a strong history of community planning. Beginning in 1979, the neighborhood created the original Oread Neighborhood Plan. In 1998 the neighborhood participated in the city-wide *Horizon 2020*, a Comprehensive Plan for Lawrence and Unincorporated Douglas County. The Hancock Historic District was listed in the National Register of Historic Places in 2004 and in 2007 the Oread Historic District was placed on the National Register of Historic Places. An updated Neighborhood Plan was adopted in 2010.

These Design Guidelines are based on the Goals, Policies and Implementation Strategies outlined in the Oread Neighborhood Plan of 2010. Listed on the following page are those policies and strategies that are specific to the overlay district design guidelines.

2010 OREAD NEIGHBORHOOD PLAN DESIGN GUIDELINE EXCERPTS

3.1.1.1 Land Use Policies

B. Create overlay district(s) to establish appropriate standards for specific areas regarding density, bulk, massing, and scale, building coverage, mix of housing types, and parking requirements.

E. New development should respect the historic integrity of the neighborhood.

3.1.1.2 Land Use Implementation Strategies

A. Encouraging more owner occupants and families to locate in the planning area is an important key to the long-term health of the neighborhood. Families can bring an increased level of stability and contribute to the diversity of the character of the area. (Policy 3.1.1.1.A)

1. Action - Amend the Land Development Code to permit by right detached dwellings (single-dwelling use) in multi-dwelling zoning districts (RM) with certain standards.

B. There is growing concern about congregate living. Parking, building lot coverage, and number of bedrooms permitted are some of the issues that have been identified. (Policy 3.1.1.1.B and Policy 3.1.1.1.C)

1. Action - Amend the Land Development Code to address neighborhood concerns while maintaining congregate living as a feasible option for owners and students.

C. There are areas of the neighborhood that are zoned more intensely than the historical development pattern and zoning regulations are not always consistent with development expectations. . . .

1. Action - Create an overlay district(s) that establish standards to regulate bulk and mass of structures, maintain open space on individually platted lots, and regulate parking.

2. Action - Create an overlay district(s) that provides greater latitude to certain areas (generally most closely adjacent to KU) to develop more densely by allowing increased building heights, etc. 3.1.2 Goal 2 – Preservation

3.1.2.1 Preservation Policies

A. Continue the preservation and protection of historic resources in the neighborhood.

B. Infill structures should be compatible with the massing, scale, and bulk of the historic structures in the surrounding area.

C. Historic infrastructure should be preserved and repaired. (eg. stone curbs, hitching posts, steps, brick streets and sidewalks).

3.1.2.2 Preservation Implementation Strategies

B. Utilization of the Lawrence Register of Historic Places will help with the protection of identified historic resources. (Policy 3.1.2.1.A and Policy 3.1.2.1.E)

1. Action - Consider creating a local ordinance historic district with design guidelines for the Hancock Historic District.

2. Action - Consider creating design guidelines for the Lawrence Register, Oread Historic District.

1.E. GOAL OF THE DESIGN GUIDELINES

1.E. GOALS OF THE DESIGN GUIDELINES

The Goals for the Oread Neighborhood Design Guidelines are intended to capture the breadth and depth of the community's vision for their neighborhood, and can be a reminder of the neighborhood's ability to work together to achieve common solutions that benefit all. The goals were developed by the community during public workshops.

1

Retain existing historic character.



2

Preserve the visual continuity of each district within the neighborhood.



3

Create a framework so that mixed residential uses can thrive.



4

Distinguish the difference between historic development patterns and current zoning regulations.



5

Recognize differing densities in different parts of neighborhood.



6

Ensure that additions, alterations and infill are compatible.



7

Encourage good design without stifling creativity.



8

Protect private property values and investment.



9

Better utilize and enhance alleys.



1.F. PROCESS

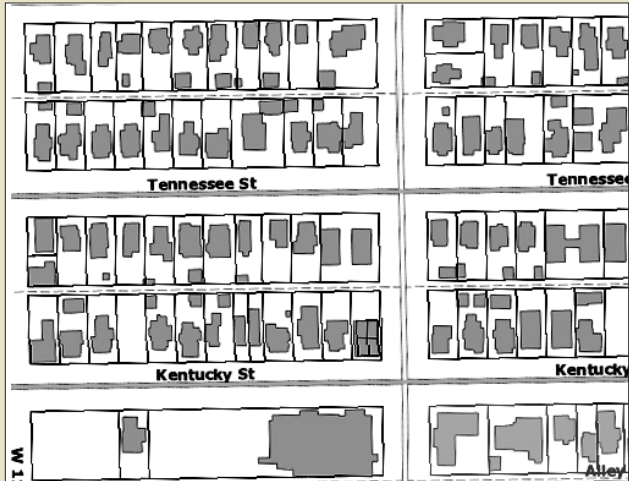


Figure-ground plane analysis shows a fairly dense neighborhood.



Historic block pattern.



Character of the neighborhood.

F. THE PROCESS USED FOR ESTABLISHING THE OREAD DESIGN GUIDELINES.

F1. Historic Patterns

Before determining if a new development, addition or remodel is in keeping with the neighborhood character, it is important to understand the distinctiveness of the historic development pattern and character. That way a project may be examined in terms of how well and to what extent it follows these patterns. A community workshop was held to examine and define the current and historic characteristics and patterns of each district. This was done through a process that engaged the community in determining and verifying the findings.

F2. Character Areas

The characteristics of the Oread Neighborhood were sub-divided into three categories – the public realm, the development of the lot and site, and the design of buildings. The historic patterns that occur in each of these three areas are described in Chapter 3.

F3. Tolerance for Change

After determining the patterns that make Oread a unique and beautiful neighborhood, the consultant team explored the community's tolerance for change and diversity away from the original patterns. After reviewing myriad illustrations of public amenities, housing densities and building styles, there was a general consensus that not all of the examples would be appropriate to the Oread Neighborhood. This process helped to clarify and refine the expectations of the community. Chapter 4 describes the specific community expectations through a series of neighborhood wide design guidelines.

F4. Neighborhood Characteristics

According to the 2010 Neighborhood Plan, there are five distinct character districts within the Oread Neighborhood by which these guidelines will be applied. The most typical of the neighborhood conditions and patterns can generally be found in the largest district - District 3. It is the district of medium density with District 1 being less dense and District 2 being more dense. District 3 is not a designated historic area, unlike the Oread and Hancock Historic Districts, although it does contain historic properties some of which fall under the historic review.

F5. Distinct Districts

These Design Guidelines use District 3 as the normal condition or neighborhood-wide standards. Variations to that norm, which vary in the other districts are described in Chapter 5.

F6. Hierarchy of Patterns

These guidelines look at existing character patterns in a methodical order. First, the guidelines examine the Public Realm, then the Development of the Lot or Site, and finally examine the design character of individual structures. The idea is that by first looking at the broader public issues, narrowing in on the relationships to surrounding lots, and lastly the design of the structure, the applicant and HRC Staff will begin to make decisions and determinations will be made in a systematic way.



Oread Neighborhood.



Public Realm - street, sidewalks, front yard.



Development of the lot and site - relationship of primary building to street and neighbors.

1.F PROCESS



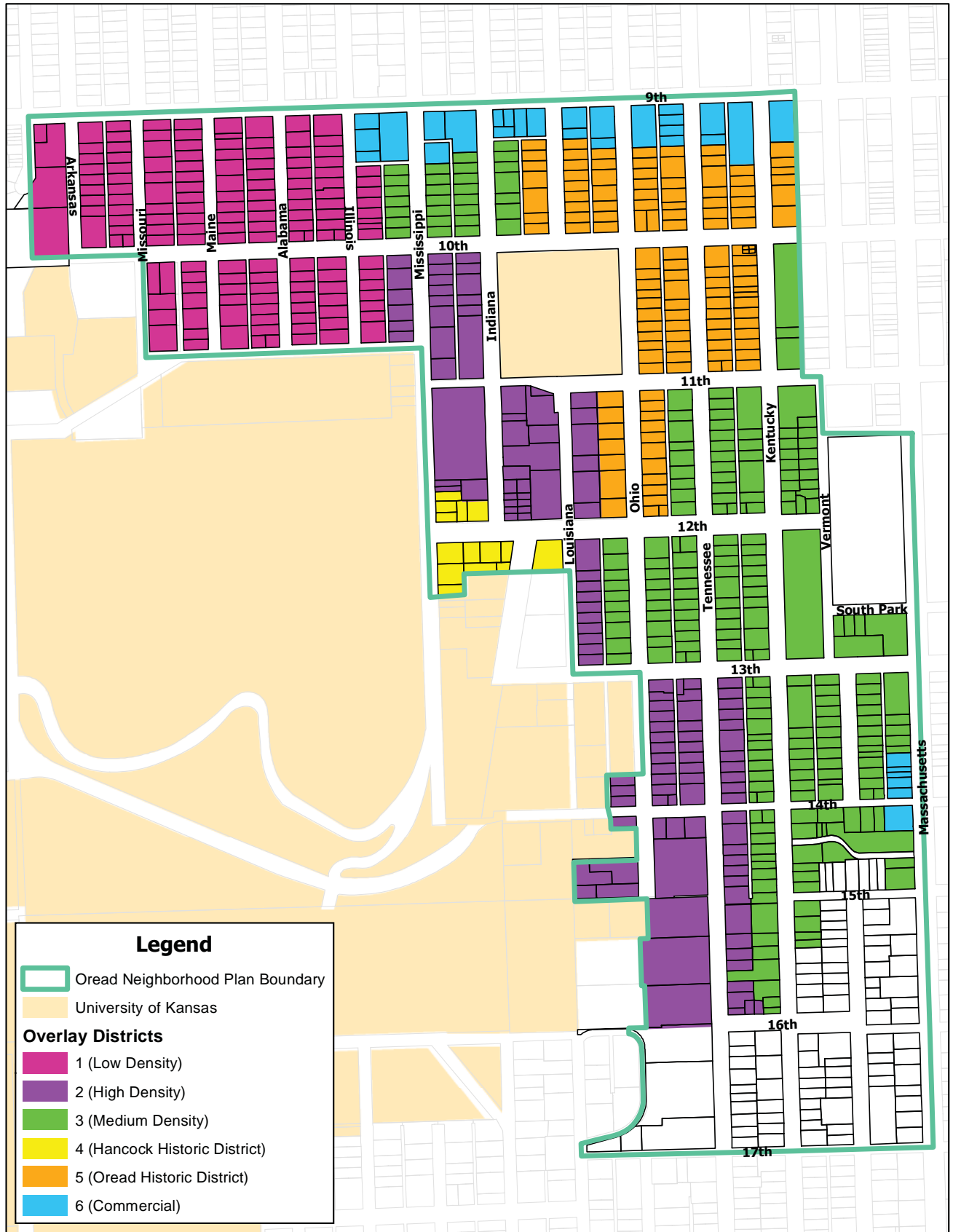
Design of the structure: roof, porch, doors, windows, etc.

G. THE FIVE DISTRICTS OF THE OREAD NEIGHBORHOOD PLAN

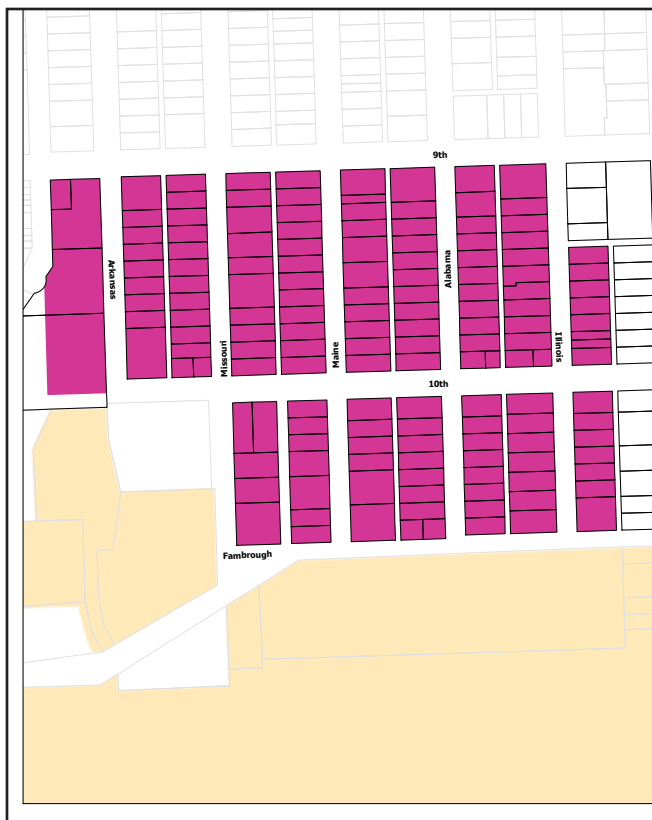
This section reflects the district boundaries that were established by the 2010 neighborhood planning process. The design guidelines are written for these specific districts and in no way tried to redefine or change boundaries.

Starting with a map of the entire neighborhood, there are individual district maps that generally describe the character areas of the districts. Chapter 3 has more in-depth information regarding neighborhood character.

The portions of the map within the boundary that are left white are parts of the Oread Neighborhood Overlay do not require additional standards.



DISTRICT ONE



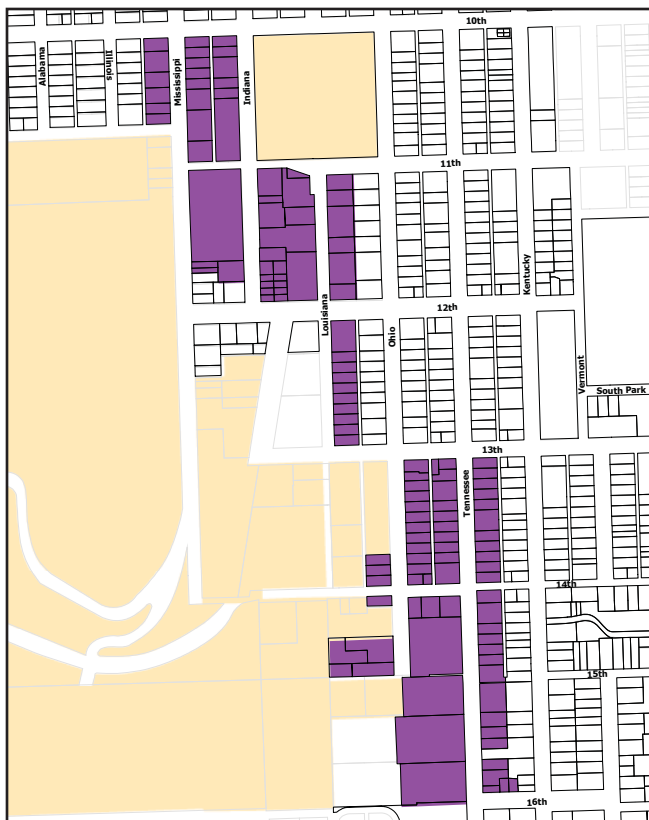
District 1 Low Density

Located just north of the University of Kansas, this district is predominately single family residences with intermittent duplexes, triplexes and quadplexes. The district is less dense (6 or less dwelling units per acre) than most parts of Oread, with smaller scale homes, larger yards, and houses spaced farther apart. The many one story homes in this area give it a unique character.

The district is composed of a wide variety of architectural styles constructed over a long period. Here you will find Victorian Queen Annes, Craftsman Bungalows, and 20th Century Revival Tudors. This was the last area of Oread to develop so Post-War Minimal Traditional houses are unique to this area. More recent apartments exist on the fringes.

The tree lined streets are slightly narrower and the houses closer to the street giving a comfortable and more intimate scale to this part of the neighborhood. Direct adjacency to campus puts parking pressure on the area, especially during football games.

DISTRICT TWO



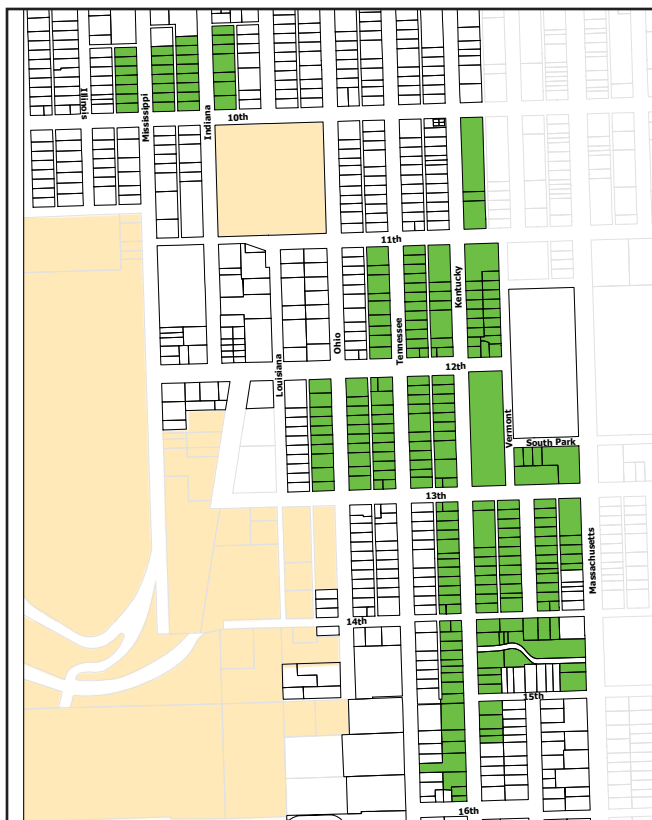
District 2 High Density

District 2 is located adjacent to the east side of the University of Kansas campus. It has the highest density (16+ dwelling units per acre) of any Oread district. The proximity to campus has made it a prime location for apartments, multi-dwelling residential and mixed-use developments.

The character of the neighborhood is diverse. There are large Greek houses, Scholarship Halls, and multi-dwelling complexes that are of a “campus” scale. Interspersed are substantial pockets of older homes, converted to congregate or multi-dwelling uses. The character of this area is distinctly different with larger scale structures, fewer street trees, and hilly terrain. Off-street parking is more visible due to lack of alleys. The prominent homes are large “Comfortable House” Foursquares with wide eaves and deep porches.

Apartment complexes in this area, constructed in the 70's and '80's, are now approaching 30 to 40 years in age and offer re-development potential and opportunities for improvements in the neighborhood.

DISTRICT THREE



District 3 Medium Density

District 3 is located mainly on the eastern edge of the Oread Neighborhood. It serves as the basis for the design guidelines for all the districts in the overlay area. The other district guidelines are modifications to this standard.

Here the streets are lined with stately trees that create a magnificent canopy. Most off-street parking is accessed from the alleys, so driveways are rare. The homes sit back from the street with simple, elegant facades. Wide, shaded porches add to the character. The original single family homes, constructed between the 1880's and the turn-of-the-century, are generally two stories and represent a wide array of styles and sizes. Many have been converted to multi-dwelling uses. Others have been replaced with modern apartments (7 to 15 dwelling units per acre).

Older commercial buildings and more recent professional buildings can be found near Massachusetts Street.

HANCOCK HISTORIC DISTRICT



District 4

Hancock Historic District

Hancock Historic District is a unique and charming area of the Oread Neighborhood located on a substantial rise that overlooks the University of Kansas.

This small district boasts a beautiful and historic public improvement project that adds to the allure of the area: a pair of curved stone steps ascends from Mississippi Street to W. 12th Street in the grand fashion of the City Beautiful Movement. At the top of the stairs on W. 12th Street a planter of native stone forms a charming esplanade in the center of the street until it intersects with Indiana Street.

The homes in this district are mature and stately, perched on top of the hill overlooking the campus. Most of the lots are larger than those found in the rest of neighborhood, creating a more landscaped setting for most of the homes. The variety of styles, mostly 20th century revivals, add to the character.

OREAD HISTORIC DISTRICT



District 5

Oread Historic District

Oread Historic District is listed in the National Register of Historic Places. It is a charming and distinguished area with some of the oldest homes in the City. There are excellent examples of the Italianate, Queen Anne, Craftsman, and 20th Century Revival styles side by side with the more vernacular National Folk and Foursquare homes. They range in size from grand to simple.

The streets are lined with tall deciduous trees. On the west side of the streets, the houses are set back with elevated front yards. Dominant porches and shade dappled yards add to the appeal of this area. On the east side of the streets, the homes are placed closer to the street with shallow front yards that are relatively flat. Parking is off the alleys so driveways are rare.

Remnants of the historic past remain. Some streets are still paved with the original brick. Hitching posts and stone steps can still be found.



2

CHAPTER 2 : USER'S GUIDE

The User Guide provides property owners, architects/engineers/designers and others with an overview and general description of the steps to be used when considering new development, redevelopment, additions, and renovations to existing buildings in the Oread Neighborhood. The User's Guide is meant to assist anyone contemplating improvements to property and buildings in an orderly process of evaluation, study of alternatives, and recommendations for City Staff and the Historic Resources Commission.

A

HOW TO USE THE OREAD NEIGHBORHOOD DESIGN GUIDELINES



1

The Owner Initiates a Project

These guidelines only take effect when an owner decides to undertake a change to their property. An owner is not required to make changes to meet the guidelines. However, once an owner has decided to begin a development project in the Oread Neighborhood, these Design Guidelines are applied and are intended to assist in making the design review process as smooth and efficient as possible.

2

Confirm the Zoning is Appropriate for the Intended Use

The underlying zoning for the property must support the intended use or the project will not be able to proceed. Zoning regulations will apply to all properties and it is a good idea to be familiar with the general regulations.

3

Read Chapter 1 to Understand the Intent of the Design Guidelines

Chapter 1 will help an owner understand the community's vision for their neighborhood. It will also help an owner understand how these guidelines are part of a more comprehensive plan for the city.

4

Review the Neighborhood Boundaries and Locate the Property

Determine which Overlay District in which the property is located. Read the description of that specific district in Chapter 1 to get an overview of the design characteristics common in that particular area of the neighborhood.

5

Read Chapter 3 to Become Familiar with the Established Patterns

Chapter 3 sets the model for prioritizing the guidelines by establishing a hierarchy of review. The hierarchy includes the character of the Public Realm, the character of individual lots, and the character of the architecture.

6

Read Chapter 4 for the Neighborhood Wide Design Guidelines

This chapter is the heart of the Design Guidelines that applies to all districts. First read the section on the Public Realm and make sure the project fits within the larger framework of the neighborhood. Then focus on the next section which deals with the development of the site. Finally read the section on the architecture to ensure the project will fit into the architectural character of the neighborhood.

7

If the Project is in District 1, 2, 4, or 5 - Review Chapter 5

Chapter 5 describes conditions that are unique to the individual districts. In addition to the neighborhood-wide design guidelines in Chapter 4, districts 1, 2, 4, and 5 have district-specific guidelines as noted in Chapter 5.

8

If the Project is Commercial - Review Chapter 6

The guidelines for commercial and mixed-use properties are located in this chapter.

B UNDERSTANDING THE PRINCIPALS AND APPLICABILITY OF DESIGN REVIEW

The following design principals apply to all projects, both public and private, within the Overlay District boundaries of the Oread Neighborhood.



1

The Oread Design Guidelines are to be utilized to plan projects. Additionally, the guidelines establish criteria for City Staff and the Historic Resources Commission and City Commission to review and evaluate various development applications including site plans, rezoning, and plats, building permits, and Certificates of Appropriateness, in a fair and timely manner.

2

The guidelines apply when improvements to the exterior of structures and sites requiring City review, such as a site plan, rezoning, plats, and building permit are submitted. When improvements do not require City review, the guidelines provide recommendations for improvements such as landscaping and fences.

3

Given the variations between districts and the variety of architecture, each project will be evaluated on a case-by-case basis.

4

Structures that contain original, character defining features shall be more carefully reviewed than those structures that do not contain features consistent with the historic patterns of design.

CHAPTER 2 USERS'S GUIDE TO DESIGN GUIDELINES

5

Individual guidelines should be looked at in relation to the purpose and goals of the guidelines. The intent of the guidelines is important and solutions should be reviewed in that light recognizing that creative solutions may meet the intent of the guideline without specific adherence.

6

The "shall" statements offer relatively little flexibility, unless choices are provided within the statements themselves. The "should" or "may" statements offer flexibility and indicate that the City is open to design features that are equal to or better than those stated so long as the intent is satisfied.

7

The Design Guidelines are not applied retroactively. The Design Guidelines are applied to proposed improvements.

8

Existing structures that have non-contributing elements are encouraged to make alterations that will improve the overall appearance of the building.

9

Projects that have received approval elsewhere in the district do not necessarily act as a precedent for proposed projects. All proposals will be evaluated on a case-by-case basis.

10

City staff and the Historic Resources Commission and City Commission have the authority and discretion to examine the whole situation, or extenuating circumstances, and approve projects that do not meet the letter of these guidelines. Where exceptions are granted, staff will clearly document the reasons.

11

Nothing in these Design Guidelines will be construed to prevent routine maintenance or repair, nor shall it be construed to prevent the City of Lawrence from performing work that is required for public safety. However, the City agrees to take into consideration the historic and neighborhood importance of these guidelines and will strive to meet the community's intent as expressed in these guidelines.

C

REVIEW PROCESS

The following is the basic process laid out in a step by step manner to help guide the applicant through the design review process with the Lawrence-Douglas County Planning Office.

1

APPLICANT

Applicant decides to begin a project in the Oread Neighborhood.

Gather all available information about the lot/site and building(s), including a concept plan (or boundary improvements survey) and zoning designation.

2

APPLICANT / CITY STAFF

Meet with City staff to review objectives for the project and initial conclusions regarding applicable zoning standards, general, and district specific guidelines.

Review with staff any additional issues pertinent to the project such as provisions of utility services, streets, alleys, etc.

3

APPLICANT / ARCHITECT-ENGINEER-DESIGNER / CITY STAFF

Incorporate staff comments and complete a design that you believe meets the intentions and criteria of the Design Guidelines for the neighborhood and the district. Work with your project designer, architect, or engineer to prepare a design for the proposed project and submit the appropriate application.

City staff may administratively approve the design at this point. Depending on the scale and scope of the project, or compliance with the Design Guidelines, the project may need to be submitted to Historic Resources Commission for review.

4

APPLICANT / HISTORIC RESOURCES COMMISSION

If city staff is unable to approve the project, the applicant may appeal to HRC for review in a public meeting. The HRC will review the project for compliance with the intentions and criteria of the Design Guidelines.

A person aggrieved by City staff's decision determining whether the Development/Design Standards have been met, may file a written appeal with the Historic Resources Commission within 10 working days after the decision is rendered.

5

APPLICANT / CITY COMMISSION

A person aggrieved by a Historic Resources Commission decision determining whether the Development/Design Standards are met, may file a written appeal with the City Commission within 10 working days after the decision has been rendered. Within 30 days after the City Commission's final decision, in passing upon an appeal pursuant to this Section, any person aggrieved by the decision may file an action in District Court to determine the reasonableness of the decision.



3

CHAPTER 3: CHARACTER of the OREAD NEIGHBORHOOD

One of the first steps in maintaining and enhancing the character of a neighborhood is to understand the basic features and attributes that make up the general traits of a place. Knowing why a certain place is special makes it easier to identify and verbalize why some structures seem to “fit in” and others don’t.

This chapter describes the character of the Oread Neighborhood on three basic levels, from the broader neighborhood to the more specific structures. The first level of scrutiny begins at the **Public Realm**. This can generally be described as that area where the public has access to the neighborhood. It can be both a physical access, such as on streets, alleys, and sidewalks, as well as a visual access of front yards and to some extent the side yards. The public realm encompasses the space between the front of one house and the front of another house across the street. It can be thought of as a great outdoor room created with facades of houses being the walls and the tree canopy being the roof. Another aspect of the Public Realm is the alleys. Because they provide vehicular access to most of the properties, alleys become “the

other front door.” A place where neighbors meet neighbors, alleys can also be shared space of the Public Realm.

The second level of scrutiny of the neighborhood character is to understand the **lot** and **site** elements how buildings are placed relative to each other and the neighboring site. The location of driveways, parking, accessory structures, of trees, and fences contribute to the patterns of the individual lot.

Finally this chapter looks at the character of the **Architectural styles**. There are certain styles and forms that are consistently found within the neighborhood. Understanding the massing, materials, patterns and ornamentation of existing buildings will inform and direct the construction of new buildings. This does not mean that new buildings need to replicate old styles, but that new construction is influenced by the defining characteristics found in the structures already within the neighborhood.

3.A. GENERAL DESCRIPTION



Front yards have an open feeling.



The streets are defined by tall, canopy trees.

A. GENERAL DESCRIPTION OF OREAD

The Oread Neighborhood is a diverse neighborhood situated between a vibrant downtown and the campus of the University of Kansas. It is filled with beautiful old homes, shade covered streets and residents who care deeply about what happens in their neighborhood.

The neighborhood is one of the oldest in the city and boasts two historic districts and many individually listed historic properties. The majority construction occurred between 1877 and 1945, the period of significance. Although there are a wide range of architectural styles represented, the neighborhood is a harmonious blending of architectural forms.

B. CHARACTER OF THE PUBLIC REALM

NOTE: Historic patterns should be based on the Period of Significance.

Only properties built within the period of significance should be used to establish patterns. Incongruent, later construction in the neighborhood should not be used as precedents to set patterns or a gradual character transformation will occur.

In Districts 2 - 5 use buildings built before 1929 as examples to establish pattern. These properties can be identified on historic maps. In District 1, use buildings built before 1945 to establish historic patterns.

The Public Realm is the area that can be seen from the streets, alleys, and sidewalks. It includes streets, sidewalks, parkways and front yards.

B1. Topography

The neighborhood gets its name from Mount Oread, a significant geological rise above the Kansas River. The significant change in elevation is an important character defining feature of the neighborhood. The steep hill begins at Tennessee Street and rises to the west

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

and begins rising from W. 9th Street to the south. The natural elevation change was embraced by the early builders and houses were placed well above the street, the front yards following a natural grade up to a sidewalk and up again to the front porches. Houses literally step up the hill on streets like Louisiana or W. 12th.

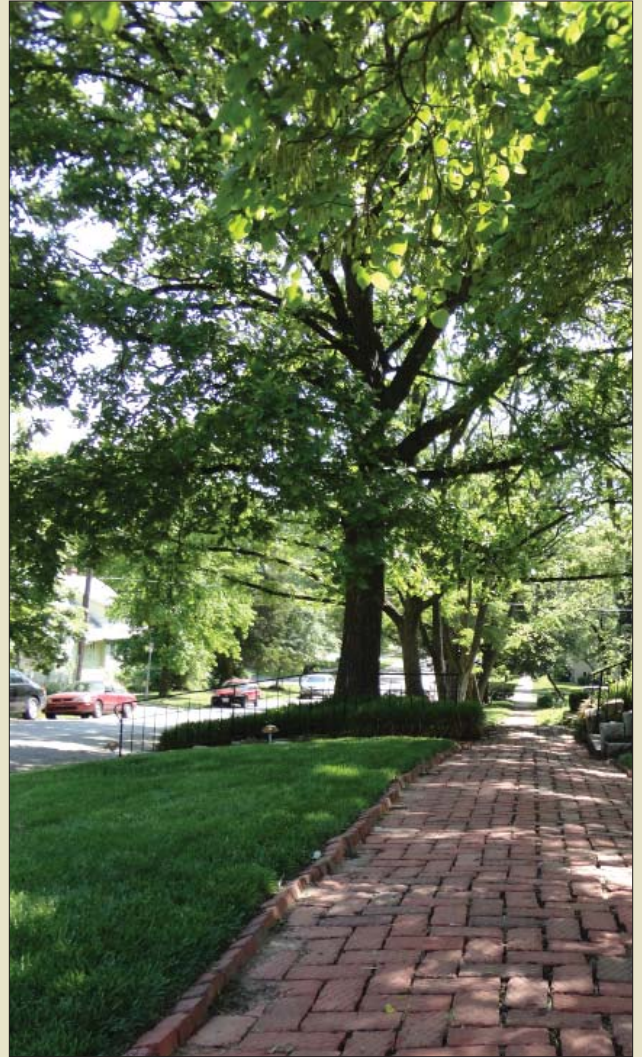
At the apex of the hill was Old College (currently Gertrude Sellards Pearson and Corbin Residence Halls) the site of the original college. Also located at the top of the hill is the new Oread Hotel, a number of residences, as well as scholarship halls and Greek houses associated with the university.

There have been some significant changes to the topography in recent years that have altered the character of the neighborhood. Large pieces of hillside have been removed to create flat parking and building sites.

B2. Network of Streets and Sidewalks

The Oread Neighborhood reflects the original street and block configuration of Lawrence that was laid out in 1854. The primary blocks run north/south with the residential lots facing east or west. This layout is a character defining feature of the neighborhood.

The streets are laid out in a rigid grid pattern. This very inflexible grid occurs despite the great change in elevation caused by the presence of Mount Oread. The result is streets that have remarkably steep grades, seemingly straight up the hill. These steep streets are a character defining feature. Many residents and students alike who describe the Oread Neighborhood refer to the steep incline of the streets.



Sidewalks are separated from the street with a wide parkway. Many of the original brick sidewalks are still common.



Neighborhood topography.

3.B. CHARACTER OF THE PUBLIC REALM



Lawns and boulevards are generally graded down to the sidewalk and curb.



Significantly sloped sites are often more heavily landscaped.



Typical front yard landscaping.

Buildings materials of both the sidewalks and the streets vary. A significant character defining material is the red brick streets and sidewalks. These streets and sidewalks give the neighborhood a character not found in newer developments.

Sidewalks in Oread are almost always situated away from the curb with a planting area located between the street and the sidewalk. This greenspace is called a parkway. Although parkways can also be referred to as right-of-ways; the term parkway is a more descriptive name for the greenspace common between street and curb.

This pattern of connected sidewalks separated from the street by a planting area is a character defining feature of the neighborhood. Within this overall neighborhood pattern there are a great variety of conditions and experiences that exist from block to block. Some of the obvious variables include the vertical location of the sidewalk and the distance from the street. However, within a particular block there is a constant pattern, which does not necessarily match what is happening across the street.

B3. Landscape

The landscape of the public realm consists of both the planting areas between the sidewalks and the street, and the front yards. It also includes those special areas of unique street features, including wide sidewalks and stairways.

Landscaping is an important part of the character of the Oread Neighborhood. The stately deciduous trees that line the streets create a canopy of shade and shadow that gives depth and character to the neighborhood. The large trees are generally planted in the parkway with smaller ornamental trees closer to the

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

houses, although there are blocks where the larger trees are located in the front yard. Where trees have been removed or are missing there is an abrupt change in the landscape, the light quality and the ambiance of the street.

Parkways and front yards are generally grass or low plantings that allow for a clear view of the house beyond. The exception is on the significantly sloped sites with large parkways that are landscaped more densely with native trees and shrubs.

B4. Fences and Retaining Walls

Front yard fences are rare in the Oread Neighborhood. Front lawns that gently merge, one into another down the block, is by far the most typical condition. Those historic fences that do exist are cast iron or wooden picket, usually no higher than 42 inches, with significant spacing between the pickets. This spacing makes the fences appear transparent which is a character defining feature. In recent years there has been a proliferation of front yard fences on newer or renovated structures that are taller than the older fences.

Some historic retaining walls exist in the neighborhood. These are low, usually not higher than 24 inches, made of native limestone with a planting bed behind that allows plants to trail over the wall. They are usually located within the front yard at the edge of the sidewalk. The exception is the very large, limestone retaining walls around Old College.



Front yard fences are not that common.



A more traditional style fence.



Retaining walls were traditionally low and made of native stone.

3.B. CHARACTER OF THE PUBLIC REALM



Hitching posts are still commonly found.



Brick streets and stone curbs are important to neighborhood character.



Most parking is uncovered since many of the garages are no longer in existence.

B5. Historic Elements

Sprinkled throughout the neighborhood's Public Realm are interesting artifacts from different periods of history. These are important character defining features. These individual items give the neighborhood a place in time, and attest to the age and dignity of the residences. Items such as hitching posts and limestone curbs are a wonderful reminder of just how old this neighborhood is. Limestone curbs are a valuable asset that are often found today only in upscale developments.

Stairs leading from street to the sidewalk, or from the sidewalk to front porches contribute to the character of the public realm. Bricks with various stamped names of manufacturers or the "City of Lawrence" add to the charm of the neighborhood.

B6. Alleys

Alleys are an important part of the neighborhood's Public Realm. Because most parking occurs off the alley, it assumes an important role in the interface of neighbor to neighbor. It is the primary access for residents, while front doors are generally reserved for use by visitors and guests.

Alleys are both paved and unpaved. Most are not in good condition, often marked by potholes and deteriorating asphalt. Since garbage collection also occurs in most alleys, the alleys bear the added burden of twice weekly heavy garbage truck use.

Garbage receptacles vary greatly, even within a block. Some units share large receptacles, some have the medium receptacles, and others have numerous standard garbage cans.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

Rear fences exist intermittently along the alleys. The frequency varies between the different areas of the neighborhood. Where they do exist, they are often privacy fences. In general, landscaping and other aesthetic amenities are not present in the alleys. Occasionally someone will have planted a tree or a small flower bed, but these are the exception rather than the rule.

Lighting in the alleys is provided by individual property owners and some utility pole lighting.

Alley parking will be discussed in a later section on the characteristics of individual lots.

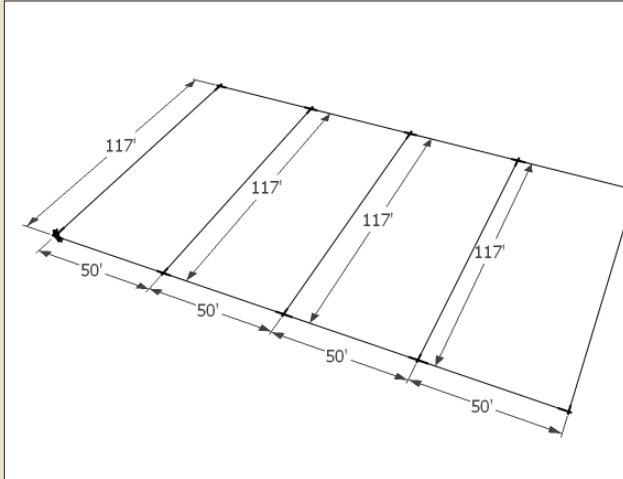


Some units use large dumpsters for alley garbage pickup.



One of the more traditional garages.

3.C. CHARACTER OF INDIVIDUAL LOTS



The typical lot measures 50' wide by 117' deep. Variations do exist.



Figure-ground plan analysis shows a fairly dense and fully built out neighborhood.



A newer structure that has been built to the current side yard setback increases density.

C. CHARACTER OF INDIVIDUAL LOTS

C1. Lot Configuration

The consistent size and shape of lots in the Oread Neighborhood create a recognizable pattern that gives definition to the neighborhood. Those properties that are different from the original grid pattern, in size or shape, appear unrelated and contrasting.

The original lots were rectilinear. In general the width along the street face was 50 feet and the depth of the lot was 117 feet.

Over the years some lots have been combined and reconfigured, but for the most part the individual lots still remain. Lot consolidation is the exception rather than the rule. In a few instances corner lots were subdivided and a second home was added that faced the side street. In the late 20th Century, lots were combined and apartment houses were built that drastically changed the character of some districts.

There is some variation in lot size between districts. Although slight, these differences can be perceived in the spacing and rhythm of the streetscape, and help to delineate the character of each district.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

C2. Principal Building

The character of a neighborhood is often determined by the location of the principal building on the site. How close the building is to the street and how close the building is to the neighboring structures are essential elements in determining character.

a. Relationship to Street

The location of the principal building to the street outlines the basic shape of the public realm. The outdoor volume that exists is framed by the existing faces of the structures on either side. In an existing neighborhood, like the Oread Neighborhood, the general pattern of front yard setbacks has long been established. City-wide zoning codes do not reflect these established patterns, but rather the patterns of new greenfield developments on the edge of town. Therefore, to maintain the character defining features of the Oread Neighborhood, it is important to establish and define the existing pattern.

The pattern is not as cleanly defined in later developments. Even in the original pattern there is a certain variation between the faces of the main structures along the street. That said, the variation is within a range that can be clearly defined.



The average front yard set back varies within each block and on each side of the block.

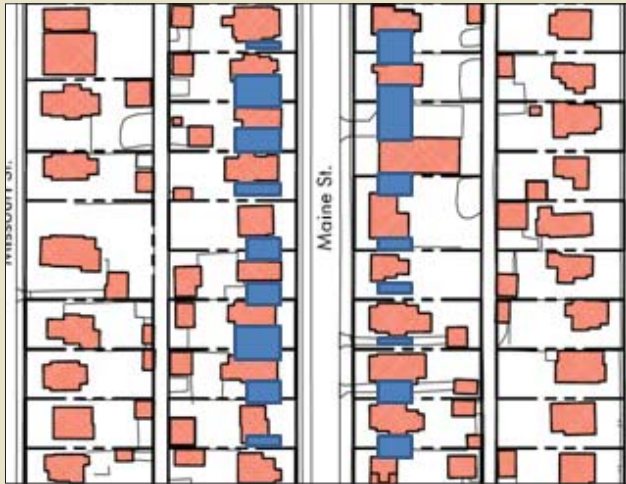


Typical block face.



House to sidewalk and sidewalk to curb distances help define the character.

3.C. CHARACTER OF INDIVIDUAL LOTS



Blue blocks show the variation in how much open space there is between structures.

Neighbor to Neighbor Relationships



The rhythm of the blue blocks (side yard open space) establishes neighborhood character.



Green denotes open space behind the structure, some of which is used for surface parking.

b. Relationship to Side Yards

The location of the principal building to the side yards also has a tremendous impact on the character of a neighborhood. The distance between buildings is important because it affects the views and light quality of both the principal building and the adjacent house.

There is a pattern or rhythm of building – space – building that varies from district to district. Tighter spaced buildings are found in the oldest part of the neighborhood where residences were constructed in the 1800's. A typical distance is 12 feet. After the turn of the century the homes were spaced with slightly more distance between buildings with approximately 15 feet between buildings. And those homes built in the 1920's to 1940's are spaced even farther apart, around 20 feet. This evolving change in spacing is part of the character differences that help differentiate the districts of the neighborhood.

Interestingly, there are also a number of buildings that were originally constructed off-center on their lot. Pushed to either one side or another, they are practically built to the lot line. They represent charming deviations from the norm, but should not be viewed as precedents for future development.

C3. Open Space and Rear Yards

Unlike the rear yards of post war houses, the rear yards of houses built at the end of the 19th century and the beginning of the 20th century were functional. Carriage houses, out houses, and kitchen gardens played a significant role in the rear yards of houses. The front of the house served a more prominent social function. Front porches were for gatherings, visiting and passing a quiet evening with family. Back porches were for washtubs and laundry, canning and

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

crocks, and served the utilitarian needs of a family.

Modern Lawrence continues this utilitarian function for rear yards. Although parking for automobiles has replaced the carriages, rear yards with spacious green lawns are the exception rather than the rule. Social gatherings typically still happen on the front porch rather than in the rear yard.

On large lots, the rear yards and open space still offer respite from the urban lifestyle. Maps showing the footprint of houses allude to a significant open space in the rear yards, but closer examination shows that the open space is in actuality modern parking space.

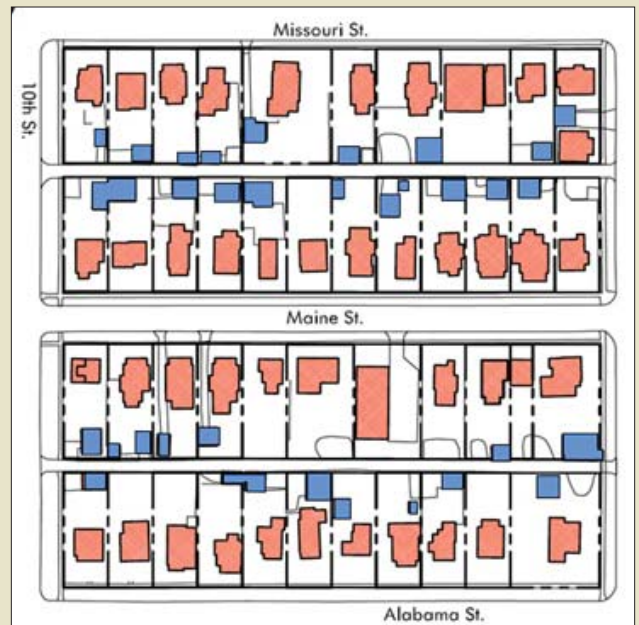
C4. Accessory Structures

Accessory structures in the Oread Neighborhood are often undersized wooden garages built in the early 20th century. Small in scale, they are more than likely used for storage than for automobiles. Often located on the rear lot line, they are reminders of another era in time. In the historic districts, these outbuildings take on a more significant role, adding character and a connection to the past.

A few larger, more elaborate carriage houses still exist. These are part of the charm of the neighborhood and should be retained.



Gold denotes surface parking.



Blue denotes accessory structures.



Historic accessory structures like this carriage house are important to the character of a neighborhood.

3.D. CHARACTER OF THE ARCHITECTURE



Parking pads in the front yard diminish neighborhood character.



Parking lots have crept up to the rear doors.



Front yards have been converted to parking.

C5. Driveways & Parking

Driveways do not exist in many parts of the Oread Neighborhood. Where they do exist historically, they are narrow, no larger than 10 feet in width. In a block face the driveways would historically occur on the same side of every house in the block. Side by side drives are rare. Ribbon driveways, with a grass median, can still be found in the neighborhood.

Curb cuts have been made along the street to allow front yard parking when alleys do not exist, interrupting the flow of front yard. Cars are sometimes parked over the sidewalk when the driveway is off of the street.

Some of the less pleasing newer parking arrangements have resulted in cars parked double stacked, driveways have been constructed that are overly wide allowing for cars to be parked in what used to be the front lawn.

Parking occurs most often in the rear of lots, directly off the alleys and is set back from the primary structure. A variety of patterns exist to try and maximize parking at the rear of lots. Sometimes the pattern is intentional and defined. Haphazard parking arrangements worked out between tenants is not pleasing from an aesthetic perspective. Parking that is allowed to encroach into the backyard open space with cars parked adjacent to the back door is the least desirable.

Paving standards have changed over time. As a result, some are concrete while others are gravel and dirt and these exist on a variety of grade elevations.

Parking off of alleys is not efficiently laid out resulting in a loss of open space which causes excessive runoff.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

D. CHARACTER OF THE PERIOD OF SIGNIFICANCE

The architectural character of the buildings within the Oread Neighborhood is an important component in defining the overall character of the neighborhood.

The size and shape of the original homes, the shape of roofs, the size and placement of windows, the location of the front door, the type of foundation, the building materials, the location and details of porches all contribute to the existing character. It is therefore important to understand and be familiar with the historic patterns of design so that additions and new construction can work within the framework of established precedents.

The style and form descriptions that follow are not meant to be rigid parameters but are intended to provide property owners with a sense of the important design factors of a particular style. In the planning and design of alterations, additions, and new construction, they provide clear direction of the priority design characteristics. If an open porch across the entire facade is a character defining feature, an owner may conclude that enclosing the front porch is not going to be an acceptable idea.

If an owner is planning to construct a new building on an existing lot, the style guidelines will give guidance as to what it means to “fit in” to the existing character. Massing shape and size, roof forms, window patterns and sizes are articulated so that the designer can establish a framework within which to work.

Much of the information was compiled from a number of architectural style resources including: *A Field Guide to American Houses* by Virginia & Lee McAlester and *Identifying American Architecture* by John J.G. Blumenson.



A wide variety of architectural styles exist in the Oread Neighborhood.



Details are what help provide this area with its unique sense.

3.D. CHARACTER OF THE ARCHITECTURE

NATIONAL FOLK - GABLE FRONT



The National Register nomination of the Oread Historic District identifies the National Folk style of architecture as the single largest housing type found in the Oread Neighborhood. Homes of this type were built for working and middle class citizens. Simple in form, these vernacular homes were often adorned with ready-made details that were brought in by railroad.

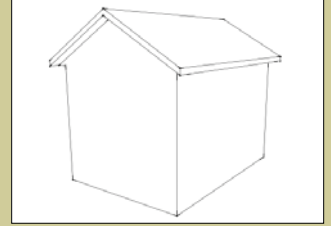
Two prominent sub-types that can be found in the Oread Neighborhood are the Gable-Front and the Gable-Front and Wing. The simple gable front is believed to have derived from the earlier Greek Revival Style, where the triangular gable end imitates the pediments of Greek temples.

In Oread, there are many types of style elements that are applied to the gable front form including Craftsman, Prairie, and Colonial Revival. Most of the detail appears in the porches and gables.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

a. Massing

The massing of the front elevation is a simple rectangle, taller than it is wide. The front wall extends to the gable roof line, generally with the same material. Sometimes decorative shingles are used in the gable. Gable front houses in Oread are both one and two stories.



b. Facades

The facades are generally symmetrical with the door offset from the center.



c. Roof Pitch

The roof is generally a very simple shape with one ridge running from front to back. The roof pitch is generally greater than 6:12.



d. Foundation

The foundation is raised above the ground level by 2 to 4 feet. The foundations are masonry either stone or brick.



e. Porch

A one story porch is usually applied across the front facade. The porch roof is hipped or shed.



f. Windows

Windows are wood, double hung, typically one-over one, but early houses are two-over-two.



3.D. CHARACTER OF THE ARCHITECTURE

NATIONAL FOLK - GABLE FRONT & WING



A variation of the gable front is created with the addition of a wing. An additional side-gabled wing is added at right angles to the gable front and set back from the projecting facade. This form creates an “L” and it is common to have a porch with a shed roof within the “L”. This sub-type is sometimes called an “L Plan”.

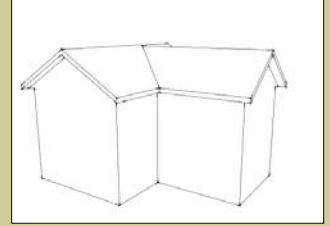
Gable Front and Wing, like the Gable-Front, often incorporate details from other styles. Common elements in this form are derived from the Queen Anne, Craftsman, and Second Empire styles.

Gable front with wings can be either one or two stories, small or large scale. The porches in Oread are generally one story, but a two-story porch is appropriate to the form.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

a. Massing

The front gable massing is rectangular, taller than it is wide. Generally in Oread Neighborhood the proportion is 3 to 4, width to height. The majority of houses of this sub-type are two stories, but both one and one-and-a-half can be found.



b. Roofs

The roof is generally a simple shape with one major ridge line that is consistent in both wings. The roof pitch is generally greater than 6:12.



c. Foundation

The foundation is raised above the ground level by 18 inches to 24 inches. The foundations are generally masonry either stone or brick.



d. Porch

A one story porch is usually placed in the "L". The porch roof is shed.



e. Front Door

The front door is located in the wing, under the porch and rarely on the front gable elevation.



f. Windows

Windows are wood, double-hung, typically one-over-one, but early houses are two-over-two.



3.D. CHARACTER OF THE ARCHITECTURE

LATE VICTORIAN - ITALIANATE



There are a number of “high style” Victorian residences within the Oread Neighborhood. These were the homes of the upper middle class and wealthy citizens. They can be distinguished by their more elaborate detailing and by an exuberant use of decorative masonry, ornate wood cornices and ornamental porches.

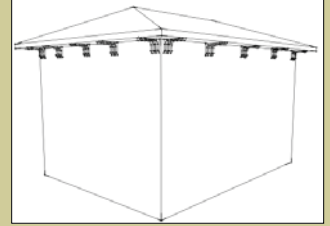
Italianate architecture began as part of the picturesque movement in England, a romantic movement away from the rigid classical revivals. It was thought to evoke images of rural Italian farmhouses, and coincided with a general fascination with foreign cultures introduced through a number of International Expositions that were held beginning in the 1850’s.

The simple block massing, low-pitched roofs, pronounced cornice and exuberant detailing above the doors and windows helps distinguish this beautiful style.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

a. Massing

The massing of Italianate residences in Oread are almost all two story, square or rectangular, boxed-shaped structures, although there are examples of center gables and towers.



b. Roofs

Roofs are low pitched and generally hipped, with large over-hanging eaves that are supported by decorative brackets that are often paired.



c. Symmetrical Facade

The houses are three part - a central front door with a window on each side. Italianate can also be five part - two windows are either side of a central door.



d. Cornices

The cornices are embellished with a large trim board, often with raised panels or moulding. Large eave brackets, are dominant features. The brackets are often found in pairs and are usually quite elaborate.



e. Windows

Windows in Italianate are almost always vertical in proportion. They are commonly one over one double hung, or two over two. Window tops are often rounded or have segmental arches.



f. Porches

Porches are generally one story and located across the entire front facade. Smaller porches centered on the front door are also appropriate for Italianate. Column, balustrade and spindles are often quite elaborate.



3.D. CHARACTER OF THE ARCHITECTURE

LATE VICTORIAN - QUEEN ANNE



Another popular sub-type of “high style” Victorian is Queen Anne. According to the National Register nomination for the Oread District, over thirty percent of the homes in the Oread Neighborhood are Queen Anne.

Queen Anne is an exuberant, creative style that was originally developed by Richard Norman Shaw in England around 1870. The American version that dominated Midwest towns and cities typically was constructed of wood lap siding and wood shingles, although brick, timbers and shingles were also used. The Queen Anne plan is asymmetrical and expressed on the outside with nooks, bay windows, towers, wrapping porches and a variety of building materials. An array of window types and sizes are incorporated into the facade.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

a. Massing

The massing is irregular and asymmetrical. Generally in Oread Neighborhood the massing is taller than it is wide. There is usually a front facing gable, turret or a projecting, round porch element. Queen Anne houses in Oread are both one and two stories.



b. Roofs

The roofs are generally steeply pitched with multiple cross gables. The ridge lines of various parts of the house are usually not aligned.



c. Change in Exterior Materials

A change of building materials is often used to differentiate the various wall surfaces. Wood siding and decorative wood shingles are typical materials used in the Oread Neighborhood.



d. Walls

Wall surfaces are not flat. Various devices such as bays, projections, overhangs, and towers are incorporated to avoid a flat wall.



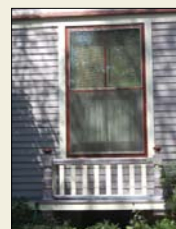
e. Porches

A one story porch is usually applied across the front facade. The porch roof is either hipped or shed. The wood posts, spindles and balustrades are often highly carved.



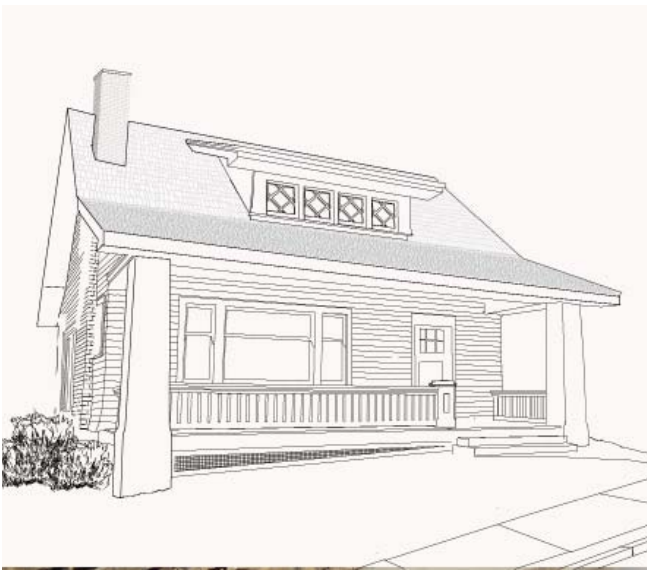
f. Doors and Windows

The front door is rarely centered on the house and is often located behind the front facade on a wing or projection. Windows are wood, double hung, typically two-over-two. Typically windows do not align with windows on another floor.



3.D. CHARACTER OF THE ARCHITECTURE

CRAFTSMAN



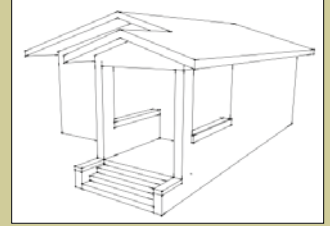
Gustav Stickley, began publishing a magazine in 1901 called *The Craftsman*, promoting the honesty of handcrafted goods, the use of genuine materials, and the straightforward expression of structural elements. In 1902 he started featuring house designs by various architects that promoted both Craftsman architecture and his own furniture products. The magazines' success rapidly spread the popularity of the Craftsman style. Pattern books and pre-cut lumber packages began to appear throughout the country helping make Craftsman the dominant residential architecture from 1905-1920.

Craftsman houses generally have a low-pitched roof with wide overhanging eaves, exposed rafters and roof beams and often use knee brackets to support the large overhangs. One story Craftsman are called "bungalows." In the Oread Neighborhood, it is common to find craftsman inspired detailing on a variety of housing types.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

a. Massing

The massing is rectangular and usually wider than it is tall. There are often one if not two front facing gables.



b. Roofs

The roofs are generally low-pitched. If there is a cross gable - the ridge lines are aligned. If the main roof is a cross gable, there is often a gabled porch or dormer.



c. Foundation

The foundation is raised above the ground level by 2-4 feet. The foundations typically have an articulated water-table at the level of the first floor joists.



d. Porches

A one story porch is usually featured on the front facade, and can be across the entire length or only across a portion of the front elevation. The porch roof is typically an extension of the main roof, or it can be a secondary gable. Porch columns are typically short, square and often rest upon massive piers or upon a solid porch balustrade.



e. Front Door

The front door is generally wide, often over 3 feet. Three windows above a projecting ledge with supports are a common pattern.



f. Windows

Windows are wood, double hung, typically one over one, but they may feature a more intricate pattern of mullions in the upper sash. The windows may be wider than typical windows.



3.D. CHARACTER OF THE ARCHITECTURE

FOURSQUARE



The Foursquare form is prevalent in the Oread Neighborhood. It was a very popular style from 1900 to 1920. The name is derived from the plan which typically has four rooms of equal size arranged in a square. Four more rooms of equal sizes are placed on the second floor. The result is a cube. Once elevated onto a foundation, the general proportion is taller than it is wide. The roof is pyramidal, often with a central gable facing the front of the house.

The Foursquare is a common form that allows for a variety of stylistic elements to be applied. It is most commonly associated with the Prairie style where it is known in some parts of country as the Prairie Foursquare or the American Foursquare. The Prairie style usually has a low pitched roof, deep eaves, contrasting caps on railing, and contrasting wood trim between stories. A one story porch with massive columns is placed across the full length of the front facade.

Other stylistic elements have been applied to foursquares in the Oread Neighborhood, including Italianate, Queen Anne, and Colonial Revival.

CHAPTER 3 CHARACTER OF THE OREAD NEIGHBORHOOD

a. Massing

The massing is two-story square, it is shaped like a cube. Generally a clear pattern of four rooms over four rooms can be discerned.



b. Roofs

Roofs are either pyramidal or hipped. The slope is dependent on the style. Italianate and Prairie are low pitched, Queen Anne and Colonial Revival usually have steep pitch.



c. Foundations

Foundations are commonly made of stone. Rough cut native limestone is typical for many of the Foursquare homes. Foundations are usually 18 to 36 inches above grade.



d. Porches

Porches are almost always present in Oread Foursquares. They are one story and added to the front facade. They are the full width of the facade. Wide steps lead to the porch. The steps are rarely narrower than 6 feet, and can be as wide as 10 feet.



e. Dormers

A large central dormer is often placed on the front facade, centered in the roof not necessarily aligned with windows or doors below. The dormer ridgeline is always below the ridgeline of the main house.



f. Windows and Doors

Due to the layout of the room there is typically a front door with side lights centered on one of the front rooms. A window (or pair of windows) is then centered on each of the rooms facing the street. The windows are still vertical in proportion but are wider than the Victorian style.





4

CHAPTER 4 : NEIGHBORHOOD WIDE DESIGN GUIDELINES

The Design Guidelines are intended to help maintain and enhance the character of the Oread Neighborhood by providing direction in the design and construction of structures within the entire neighborhood.

The guidelines laid out in Chapter 4 represent the overall character of Oread and therefore apply throughout the overlay districts. They represent the essence of the design character of the entire area.

The guidelines deal first with the larger issues of the Public Realm, then move to the design of the site/lot, and then finally to the design of the structure on the lot. This pattern of addressing how the design fits into the neighborhood then into the block will hopefully put the design of the structure into perspective.

Designs should be aligned with the patterns for the neighborhood and should be in context with other structures on the block. Then the architectural style and detailing can be discussed in a productive manner. Then architectural detail can be refined to achieve a context sensitive design.

The character of each District within Oread has subtle and not-so-subtle variations. Chapter 4 deals with the norm and Chapter 5 deals with the exceptions and specifics of each district.

The scope of the guidelines shall be equal to a building permit's proposed scope of work. Work conducted without a valid permit when a permit is required shall still be subject to the Guidelines. Projects not affecting the exterior appearance of an existing structure will not be subject to Design Guidelines review.

Nonconformities shall be subject to the applicable provisions of Article 15: Nonconformities of the Land Development Code.

4.A. PUBLIC REALM GUIDELINES



Public Realm is from the building face through the front yard and across the street to the other building face.



Stone curbs and brick streets should be retained.



Streetscape contain real grass and are planted with tall canopy trees.

A. PUBLIC REALM GUIDELINES

Goal: Maintain natural and built elements that are character defining elements including topography, vegetation and historic public works when possible. (Earth work can occur for subterranean development but upon completion the original topographic elevations are restored.)

A1. Street and Alley Patterns and Materials (including curbs)

- a. Improvements in the public right-of-way should retain stone curbs, stone steps and hitching posts.
- b. Improvements in the public right-of-way should retain historic materials, brick streets, and support rehabilitation.
- c. Street width should be maintained and not increased.
- d. The street grid shall remain intact. Streets should not be blocked to create cul-de-sacs.

A2. Sidewalks

- a. Public improvement projects should retain brick sidewalks.
- b. Herringbone or running bond patterns with a soldier course along edges should be used. If a pattern exists in the block, that pattern should be used.
- c. Brick sidewalks should be set on 4-5 inches of compact base materials (e.g. sand or limestone screenings).
- d. Brick sidewalks should shift to accommodate tree roots as opposed to cutting major tree roots.
- e. Sand should be used as fill material between bricks, not mortar.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

Streetscape (Rights-of-Way)

Parkways should be planted with living plant materials, grass or low growing ground cover (no more than 12 inches high).

A3. Street Trees

- a. Trees should match the character of existing street trees, and be compatible with approved trees on the City list. Typically these are canopy trees.
- b. Trees should be placed in the middle of the parkway or towards the sidewalk leaving room for people to get out of cars.
- c. Trees should be placed no closer than 5 feet from the curb and should adhere to city traffic guidelines for sight triangles at intersections.

A4. Front Yard Landscaping

- a. Front walk
 - i. Front walk should link the public sidewalk to the front door in a mostly direct route.
 - ii. The front walk should be at least 36 inches wide and no more than 60 inches wide.
 - iii. Materials for the walk should be concrete, brick or a relatively smooth paving stone and not gravel.
- b. Landscaping
 - i. Front yards (between the street and the front facade) should be mostly grass or low ground cover. Shrubs should be limited to foundation plantings.
 - ii. Front lawns should be graded down to the sidewalk without the use of a retaining wall when possible.
 - iii. Where space allows, front yards should be planted with canopy trees. Ornamental trees should be placed closer to the house.



A row of tall canopy trees creates a great sense of enclosure along this street.

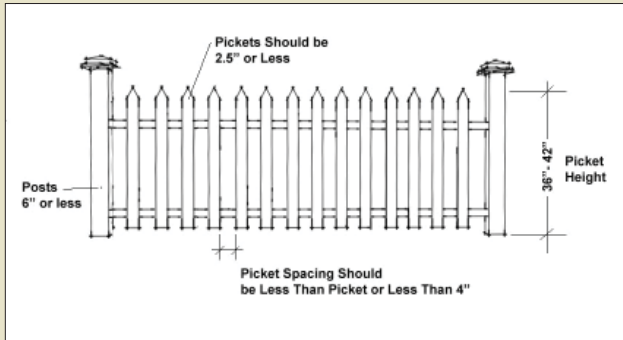


Front yards generally contain lawn with low growing plants.

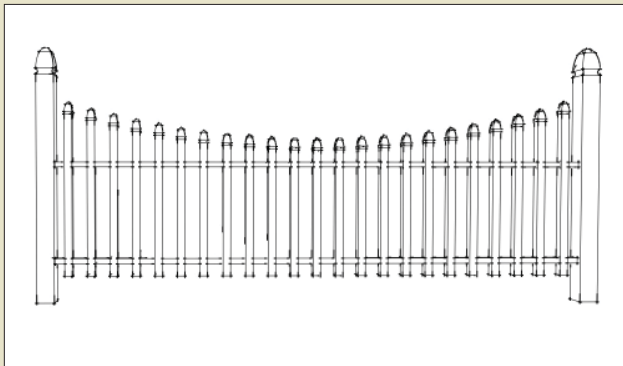


Yards are often graded down to the sidewalk without the use of retaining walls.

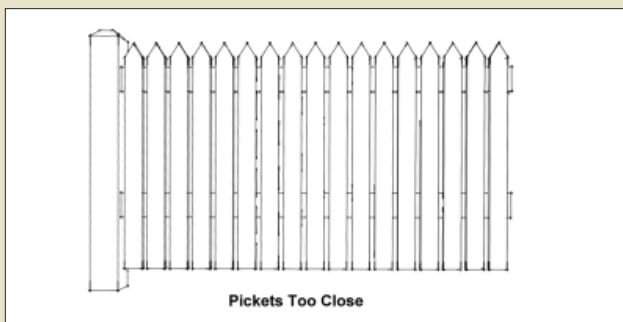
4.A. PUBLIC REALM GUIDELINES



Front yard wooden fences should be more open than closed and between 36" and 42" tall.



Wooden pickets should be vertical but height can vary.



Close pickets are only appropriate for rear yard fences.

A5. Fences and Retaining Walls

a. Front Yard Fences

- i. Fences should use materials such as:
 - (1) Masonry or stone
 - (2) Ornamental iron
 - (3) Woven wire in a historic pattern
 - (4) Wood picket
 - (5) Hedges
 - (6) Chain link and vinyl should not be used
- ii. Height
Fences should be between 36 and 42 inches tall.
- iii. Pickets
 - (1) Pickets should be vertical and should not occupy more than 50% of the fence panel.
 - (2) The pickets or materials should not be more than 2.5 inches wide at its widest point. Ornamental iron fences should be more delicate than these standards.
 - (3) Picket spacing should be less than a picket width or no more than 4 inches.
- iv. Posts
Posts should be no more than 6 inches wide.

b. Side Yard Fences

- i. Side yard fences may be 100% opaque and may be any material listed in A5, a.
- ii. Fences erected behind the front building line shall not be over 4 feet in height to the rear building line of the neighboring dwelling, or to the rear of the dwelling located on the same site as the fence, whichever is the greater distance from the front property line.

c. Rear Yard Fences

- i. Rear yard fences can be 100% opaque and can be any material listed in A5, a.
- ii. Rear yard fences may be 6 feet tall.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

d. Retaining Walls

- i. Retaining wall materials should be native Kansas limestone. Rough concrete modular units are also allowed and should be no larger than 6 inches high. Railroad ties or landscape timbers are out of character and should not be used.
- ii. Retaining walls should not be more than 30 inches in height.
- iii. When more height is needed, retaining walls should be broken into a series of shorter walls to avoid one massive wall and to allow a planting area between the walls.
- iv. Retaining walls should start back 5 feet from curb face when at street level.

A6. Other Elements

a. Mailboxes

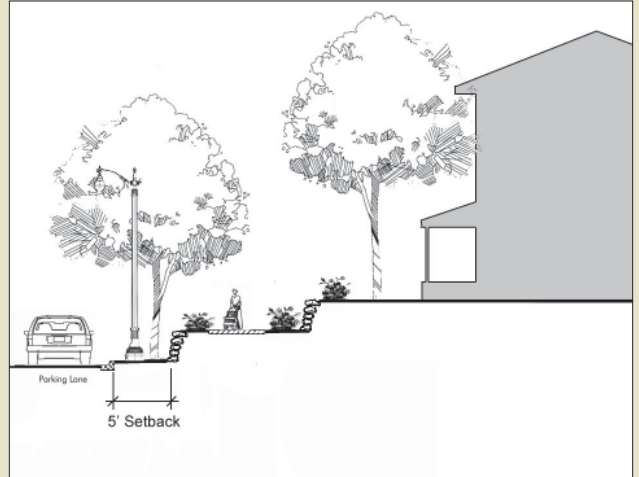
Cluster mailboxes, for multi-dwelling structures, that sit on pedestal bases should be set behind the front wall plane of the structure that is closest to the front lot line and should not be in the front yard.

b. Utility Service Boxes

Above ground utility service boxes that need to be in the front yard should be subterranean or earth tones, and/or screened with landscaping.

c. Solid Waste Collection

- i. Trash and recycling carts shall be screened with some type of landscaping or fencing so that the receptacles are not visible from the public right-of-way.
- ii. Where alley service is not available, trash carts should be stored in back of the front wall plane closest to the street.



Retaining walls should handle grade changes in stages and not all at once. Leave areas in front of retaining walls for plantings.



A good example of a stepped retaining wall in Oread.



Cluster mail boxes should be located behind the front wall of the structure.

4.A. PUBLIC REALM GUIDELINES



Rainwater collection should be located behind the front facade.



Landscaping along an alley.

d. Alley-Side Solid Waste Collection

i. Locations

Dumpster locations shall be close to the lot line and along the edge of the alley. Adjoining property owners should work together to establish combined locations.

ii. Screening

The dumpster should be screened by opaque fencing material that is compatible with the adjacent structure. Screening should be 6 feet in height.

iii. Surfacing

Dumpster pads shall be concrete, level and continuous with the alley surface to allow for drainage to alley. Curbs around the inside of the pad are recommended.

e. Rainwater Collection and Detention

Collection barrels and tanks should be located behind the front wall plane of the structure and not in the front yard.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

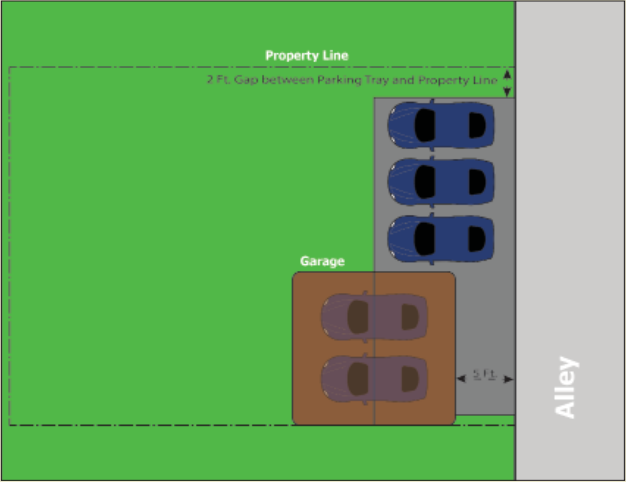


Diagram showing allowed parking configuration when accessing parking via alley.

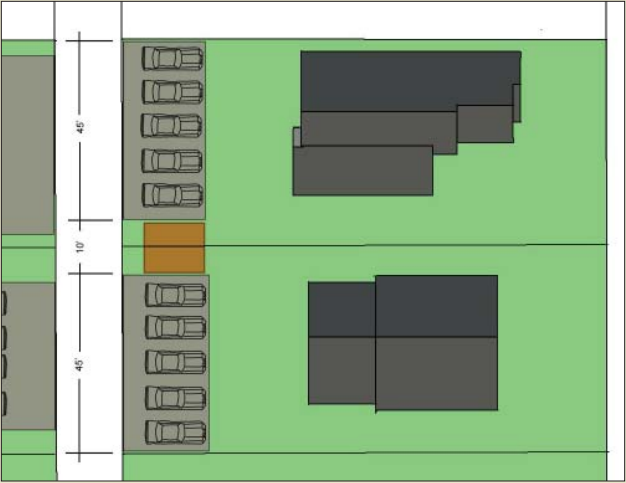


Diagram showing parking configuration to accommodate shared dumpster.

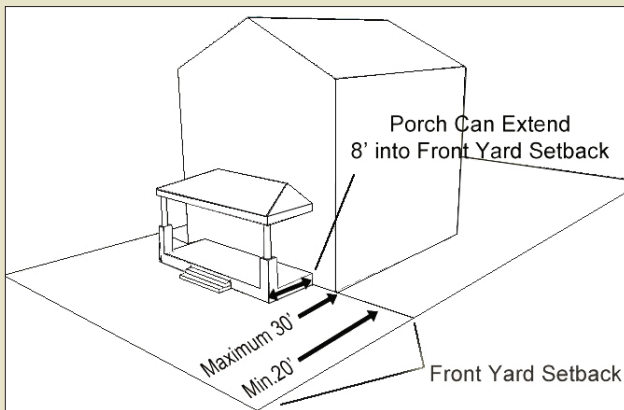


A good example of a dumpster screened with native stone walls.

4.B. SITE DEVELOPMENT GUIDELINES



Principal structures shall face the street.



Front yard setback should be between 20' and 30'.

B. DEVELOPMENT OF THE LOT AND SITE GUIDELINES

Goal: Maintain the established pattern of lot development. Combining lots into larger parcels is not appropriate, except as allowed in Chapter 5.B. Respect and maintain the traditional relationship of a structure to the street and to neighboring properties, the common orientation of structures, the established configuration of open space, and the placement of parking in the rear.

B1. Lot Consolidation

- i. Lots in all districts except District 2 shall not be combined as part of redevelopment to construct duplex and other higher-intensity forms of housing.
- ii. Combinations shall be permitted if they reestablish the historic 50 foot wide lot pattern that is traditionally seen within the neighborhood and do not remove historic structures.

B2. Front Yard Setbacks

- a. Principal Structure
 - i. The entry of the Principal structures shall face a street.
 - ii. Structures shall be constructed no closer than 20 feet to the front property line and should be no farther than 30 feet from the front property line.
 - iii. Portions of porches that extend beyond the principal structure shall not be included in setback calculations.
- b. Porch Encroachment
 - i. A porch may encroach up to 8 feet into the required front yard setback.
 - ii. The total area of any encroachment of the porch into a front yard or a side yard adjacent to a street shall not exceed 150 square feet.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

B3. Side Yard Setbacks

The combination of side yard setbacks should be no less than 15 feet, with a minimum of 5 feet on each side.

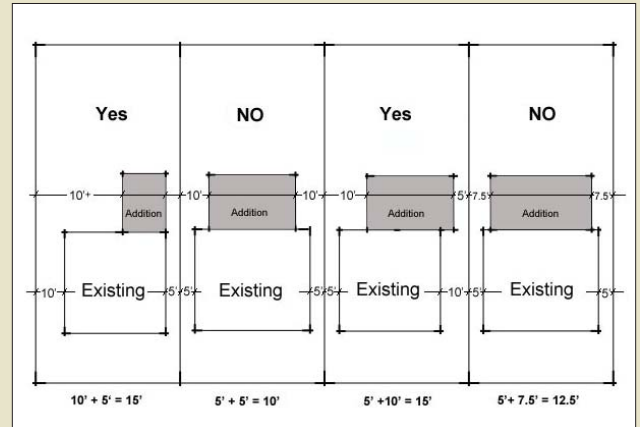
B4. Specific Lot Guidelines

a. Open Space

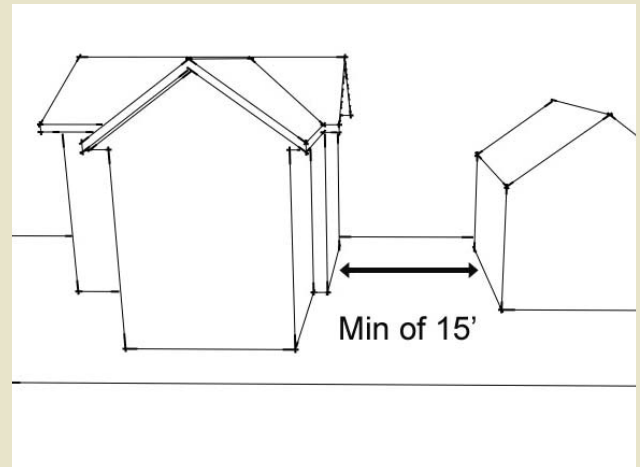
- i. There shall be a minimum of 15 feet of open space between the principal structure and an accessory structure or a parking pad.
- ii. Lots with no alley access may use the 15 feet of open space between the primary structure and the accessory structure for parking.

b. Parking Configuration and Location (when there are no alleys behind lots)

- i. No additional curb cuts shall be constructed if one already exists. To relocate a curb cut, the old curb cut shall be eliminated.
- ii. All parking areas shall be located behind the front facade.
- iii. Driveways to detached dwellings and duplex structures shall be no more than 12 feet wide but can flare out.
- iv. Parking behind the front facade surface may be permeable paving.
 - (1) Brick
 - (2) Geotech paving systems
 - (3) Permeable concrete
- v. Shared driveways are encouraged. Shared driveways to detached dwellings and duplex structures should not be wider than 12 feet.



Combined side yard setbacks should be 15' or greater with a 5' minimum.



A minimum of 15' open space should be maintained between the principal structure and accessory structure or parking.

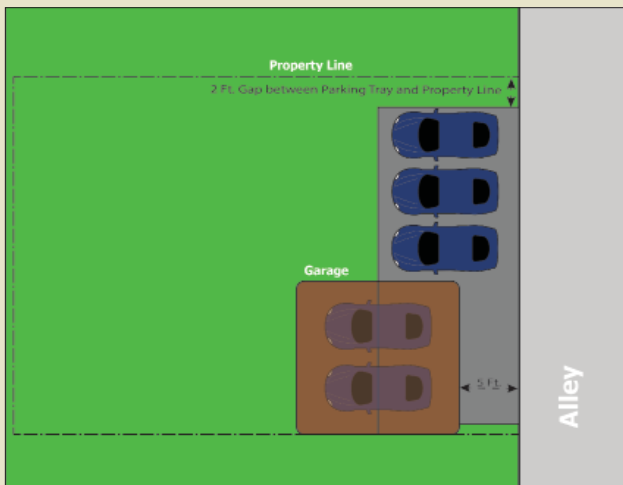


There shall be no parking in the front yards.

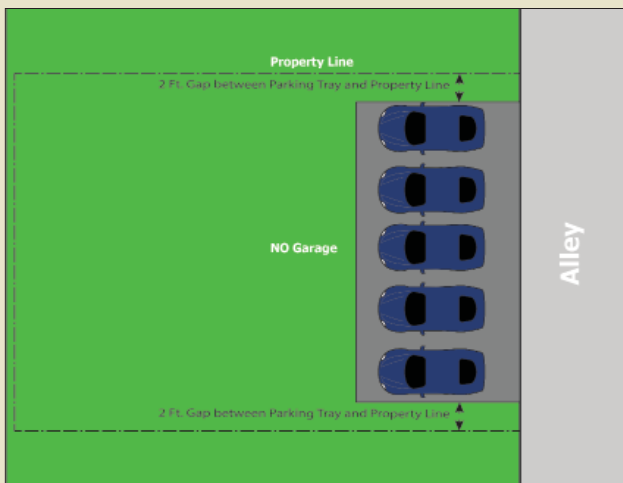
4.B. SITE DEVELOPMENT GUIDELINES



An acceptable example showing depth of parking stall should be no more than 25' from rear lot line.



Permitted parking layout with a garage.



Permitted parking layout with no garage.

c. Parking Configuration and Location (when parking is accessed off alleys.)

- i. The depth of uncovered parking spaces should be not more than 25 feet from the rear lot line.
- ii. Bumper blocks or a 6 inch curb shall be used.
- iii. The parking surface may be permeable pavement.
- iv. New curb cuts from the street shall not be permitted.
- v. Any new covered parking structure shall be a garage and not a carport, and shall not exceed more than 2 car capacity.
- vi. Parcels shall have only one garage.
- vii. Detached Dwellings and Duplexes may construct parking spaces in accordance with the diagrams.
- viii. All Multi-Dwelling Structures shall be required to construct a parking lot in accordance with the Land Development Code and the guidelines of this document.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

B5. Principal Building Orientation

- a. Front Door Location
 - i. The front door should face the street to maintain the pattern of the neighborhood.
 - ii. When multiple units are within the same building, one door shall face the street.
 - iii. When more than one door faces the street one door should be more dominant than the other.
- b. Common Green Space
 - i. In multi-unit structures and rowhouses, the green space should be between the front facade and the street.
 - ii. In courtyard housing, the common greenspace should be in the courtyard.



Front doors shall face the street.



Common greenspace can be shared by all tenants.

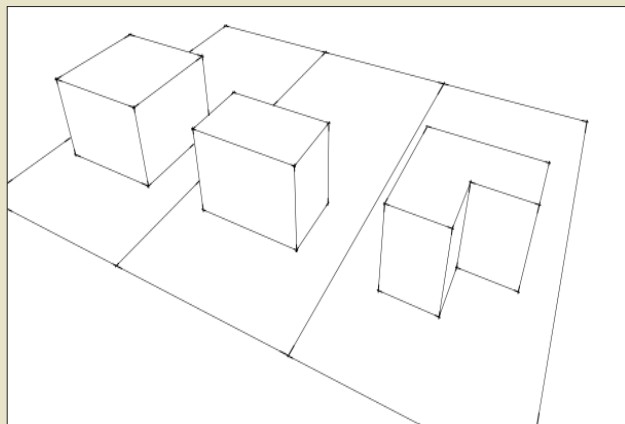


A good example of how common greenspace can be planting areas, rather than lawns.

4.C. ARCHITECTURAL GUIDELINES



Recognize and respect historic elements.



Three basic massing forms, square, rectangle, and "L" plan.



Exposed foundation wall shall be brick or stone.

C. ARCHITECTURAL GUIDELINES

Goal: New construction, additions and remodels should be compatible with the neighborhood, reinforcing the existing patterns and enhancing the established character.

C1. General

- a. New construction in the Oread Neighborhood should recognize and respect the historic elements and patterns that exist within the neighborhood. The design of new structures should respond to the character of existing structures, using them as a source of inspiration for new designs.
- b. New construction should avoid trying to create an artificial history by duplicating styles and designs that are not associated with the neighborhood. For example, while the Spanish Revival style is appropriate within the period of significance (1877-1945), because it is not found in significant amounts in the neighborhood, it is not appropriate for Oread.

C2. Massing and Form

- a. The main form of the principle structure should be a simple square, a rectangle or an "L-Plan" with an uncomplicated roof form, that has minimal roof plane changes.
- b. The proportions of the front facade shall be taller than it is wide.

C3. Foundations

- a. Height
First floor elevation shall be a minimum of 18 inches above grade on the front of the principle structure.
- b. Materials
 - i. Exposed concrete shall be clad in brick or stone.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

C4. Roofs

a. Form

Primary roof form shall either be a gable end, cross gable, hipped, pyramidal, or gambrel.

b. Dormers

Dormers shall be designed so that there is a relationship in type and alignment to windows on the main building.

- i. Dormers may be front facing and centered, but shall not occupy more than 40% of the roof plane. In other words, dormers shall not be so large as to appear to be adding an additional story to a structure.
- ii. Dormers on the side shall not occupy more than 60% of the roof plane.
- iii. New dormers shall not exceed the height of the original roof.

c. Roof Pitch

Primary roof line shall be between 5:12 to 12:12 in slope depending on the style of the house.

d. Roof Materials

Roof materials shall be one of the following:

- i. Dimensional asphalt shingle roofs that emulate wood shingles.
 - ii. Real clay tiles
 - iii. Slate tiles
 - iv. Terra-cotta tiles
 - v. Standing seam metal roofs with a double munch or double lock seam, no more than 1.5 inches high and no more than 18 inch wide pans.
 - vi. Grade A, smooth machine cut, real wood shingles treated with fire retardant. Shingles should be about 3/8 inch thick by about 5 inches wide.
- #### e. Eaves and Overhangs
- Overhangs shall be provided and shall be between 16 inches and 36 inches to provide shade over windows in summer months.



Primary roof form shall either be a gable end, cross gable, hipped, pyramidal, or gambrel.



New dormers shall not exceed height of the original roof.

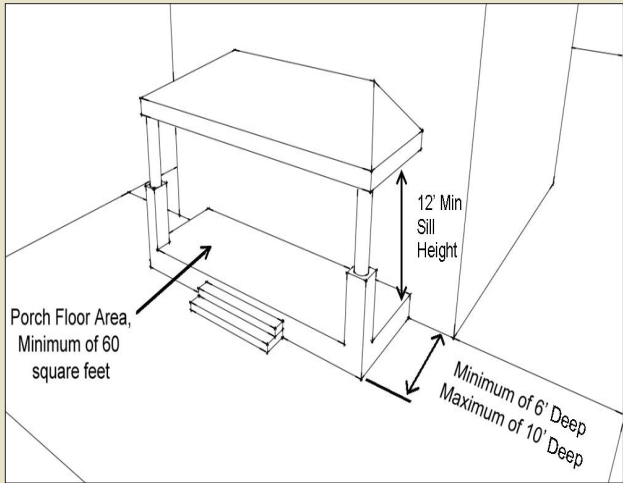


Clay tile roof.

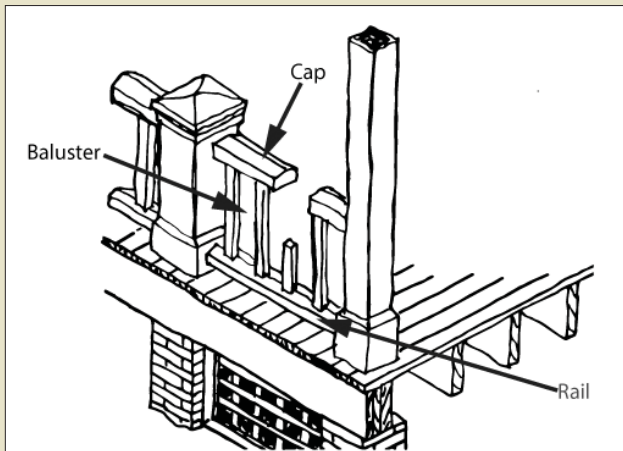
4.C. ARCHITECTURAL GUIDELINES



Eaves should extend by 18" to 24" from the wall.



Porches can be 6' to 10' deep with a minimum deck area of 60 square feet.



Porch balustrades should be 50% or more opaque material with no more than 2" of spacing between balusters.

C5. Porches (Front and Street Side)

a. Porches

All ground floor dwelling units shall have a porch.

b. Location

Porches shall be located at and accessible from the first floor level of the structure.

c. Porch Size

Porches should cover at least half of the first floor facade facing the street horizontally and should have a minimum deck surface area of 60 square feet.

d. Depth of Porch

The minimum depth of the porch shall not be less than 6 feet and the maximum depth of the porch should not exceed 10 feet.

e. Porches Bulk

Porches should appear to be "added on to the building" rather than cut out of the building. Porches should have their own roof that isn't integral to the rest of the building.

f. Porch Roofs

100% of the front porch shall be covered by a roof or a pergola.

g. Porch Railings and Balusters

- i. All porches should have a railing.
- ii. Railings should not exceed 36 inches in height measured from the floor of the porch.
- iii. Baluster spacing should be continuous between columns.
- iv. Balusters should be composed of 50% or more opaque material.
- v. Spacing between balusters should be no more than 2 inches.
- vi. Balustrades should include a cap or rail on top.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

h. Porch Steps

Front stairs shall appear as one set of stairs even if there is more than one housing unit in the structure.

i. Open Porch

The front porch shall be open and not enclosed by any materials except screens.

j. Height of Porch Floor

The floor of the front porch shall be raised above grade and no lower than one step below the first floor.

k. Porch Roof Height

The sill of a roof or trellis should be set at the second story/floor.

l. Porch Columns

Porch columns should match the proportion, detailing, and size of the porch massing they are supporting. Porch columns shall have capitals and bases. Capitals and bases shall be proportional to the size of the column and shall be detailed in a manner to match the overall architectural style and period of the structure, providing a balanced and finish look. The horizontal spacing between columns is typically no greater than $1\frac{1}{2}$ times the height of each column, and not less than $\frac{3}{4}$ of the column height. Fiberglass columns shall not be permitted.

m. Porch Foundation

- i. The porch shall be supported by columns or foundation walls affixed to the ground. The columns should be no less than 16 inches by 16 inches.
- ii. The space between the foundation columns shall be infilled with wood siding, lattice, brick or stone.

n. Painted Wood

All exposed wood used for porches should be painted, not stained. The



Porch details should be commensurate with the load they support, such as this example.



The load being supported by the columns should be expressed in the design of the structure. Columns and their bases should be evident.

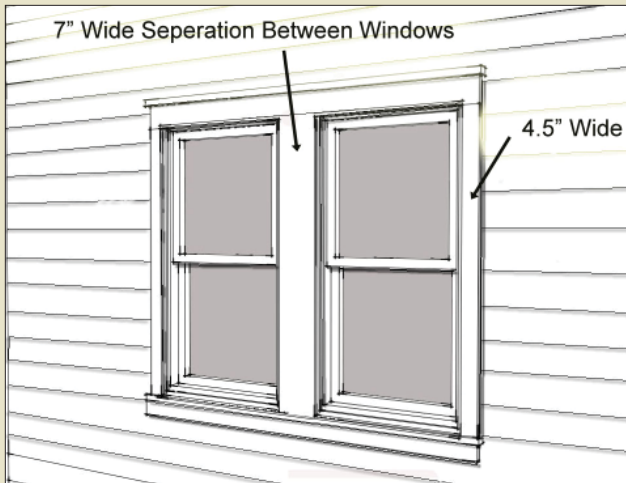


A good example illustrating how front doors shall be prominent and face the street.

4.C. ARCHITECTURAL GUIDELINES



A good example of how windows centered in bay.



Leave 7" of space between grouped windows with 4.5" trim around windows.



Wood siding and wood shingles are common wall cladding.

exception is that floor decking can be stained.

C6. Fenestration

a. Front Doors

- i. Front door shall be prominent and face the street.
- ii. Front door should contain glass in character with the style of the house. Side lights and transom lights may be used.
- iii. Doors into separate units in the same buildings should be situated so that one door is prominent.

b. Windows

- i. Operable windows should be single or double-hung windows.
- ii. Windows should comprise 35% of the front facade.
- iii. Windows shall be made of wood or aluminum-clad wood. The profiles and jamb conditions shall resemble the original wood windows in detailing and profile thickness.
- iv. Windows facing the street shall have all the same sill and/or head height on each floor of the structure.
 - (1) Accent or feature windows are an exception.
 - (2) Windows along stair cases should follow the pitch of the stairs.
- v. Windows should maintain a 2:1 height to width ratio.
- vi. Windows should be laid out symmetrically on each wall plane that faces the street.
- vii. Smaller windows should be grouped together in lieu of using one large single pane window.
- viii. Grouped windows should be separated by a no less than a 5 ½ inch wide trim piece.
- ix. Window should have trim that represents the architectural style of the structure.
- x. Shutters should be the size as the window they are adjacent to so if

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

they were to be closed they would cover the windows. Single or double shutters may be used.

C7. Building Wall Materials

- a. Wood
 - i. Horizontal wood or cementitious wood siding shall have a 4 to 7 inch exposure and a smooth finish, not a wood-grained texture.
 - ii. Wood shingles should be in a vertical pattern with 3-7 inch reveal.
 - iii. Wood shingles should be primarily used as second story cladding, on attic dormers, gable ends and porch roof gables.
 - iv. Grooved panel siding should not be used.
- b. Brick & Block
 - i. Brick should be no larger than 2 2/3 inch tall X 8 inch long with mortar joints no larger than 1/2 inch.
 - ii. Concrete masonry units (CMU) or concrete block shall not be used as an exposed exterior material.
 - iii. Brick should not be used on upper floors unless brick is found on the floors below.
- c. Stone
 - i. Synthetic stone should not be used, except in lintel applications.
 - ii. Stone should be in the scale of other stone found in the neighborhood and should have the appearance that they could have been laid by hand.
- d. Combinations

Creative combinations of the above may be used to reflect natural textures, so long as they meet the overall objective of conveying a sense a permanence, human scale and proportion.

C8. Architectural Details and Features

- a. Columns, lintels, sills, rafters, door



Brick is a common wall material.

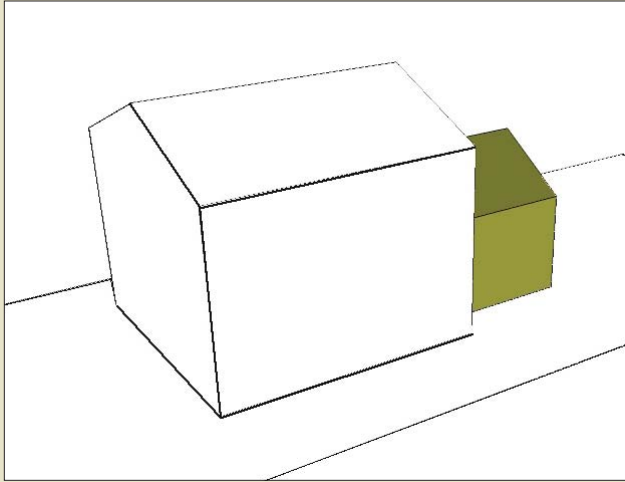


Use of native stone is appropriate for walls or accents or both. Use of synthetic stone can be appropriate for accents.



Wood shingles used on gable end.

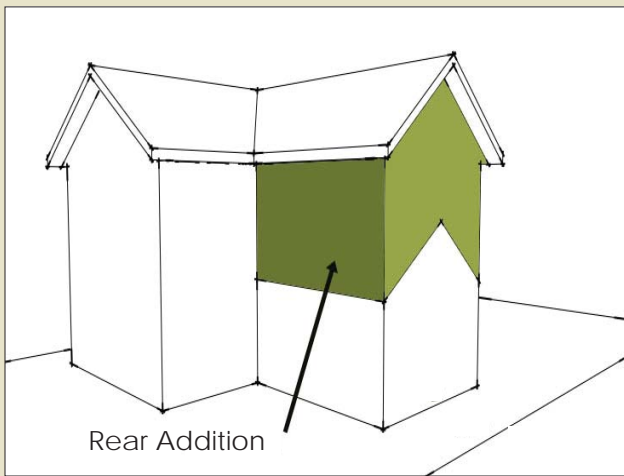
4.C. ARCHITECTURAL GUIDELINES



Additions should be placed inconspicuously on the least character-defining elevation.



Place additions away from public streets.



Second story additions shall not project past the existing first story wall.

surrounds, decorative gable-ends, etc., should be used to add visual interest to a structure.

- b. Appropriately scaled details should be used.
- c. Details should be consistent with the design and style of the building.

C9. Additions

- a. Additions in General
 - i. Additions may include projecting bays, bay windows, dormers, a room or an entire wing.
 - ii. New additions shall not be so large as to overwhelm the original structure because of location, size, height or scale.
 - iii. New additions shall not obscure or demolish character defining features of the original structure.
 - iv. Building additions should be in keeping with the original architectural character, mass, scale, and materials.
- b. Location of Additions
 - i. Additions shall be located inconspicuously on the least character-defining elevation.
 - ii. Additions should be to the rear of the existing structure.
 - iii. Additions should be recessed behind the front facade by a minimum of 5 feet if it is not possible to place the addition to the rear.
 - iv. Additions shall avoid filling in the spaces between projecting elements that create a flat or projecting front facade.
- c. Second Story Additions
 - i. Second story additions shall comply with the minimum interior or combined side yard setbacks:
 - ii. Additions shall not project past the existing first story wall.

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

d. Massing of Addition

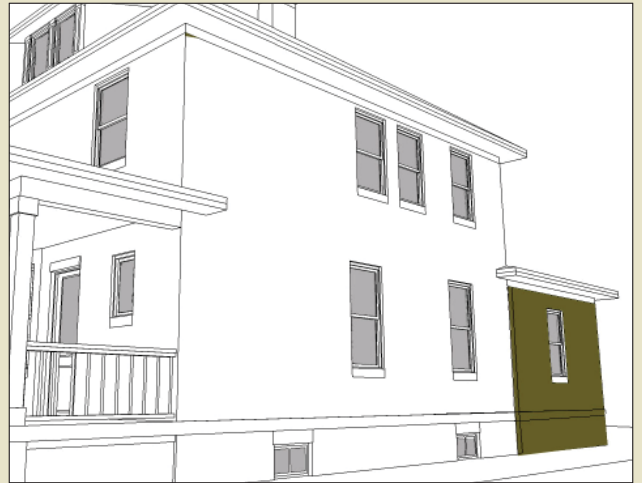
- i. Design of additions shall be compatible with the style of the primary structure.
- ii. The addition should be relatively the same scale as, or subordinate to, the original house.
- iii. Additions that exceed the height of the original structure should be subordinate in massing and scale.
- iv. Roof forms, pitches, and overhangs that are similar to the original structure should be used.
- v. Window types, shapes, and proportions should be similar to those of the original structure.

e. Exterior Materials

- i. The selection of exterior materials shall be compatible with the primary building.
- ii. See C7 for materials.



A acceptable example of how additions should be subordinate to the primary structure.

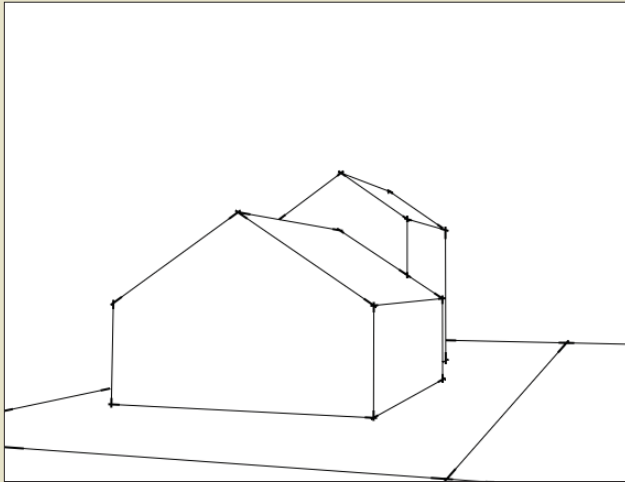


Additions should relate in scale to other elements of the house. The addition is a similar scale to the porch.



Here an unacceptable addition overwhelms the original building.

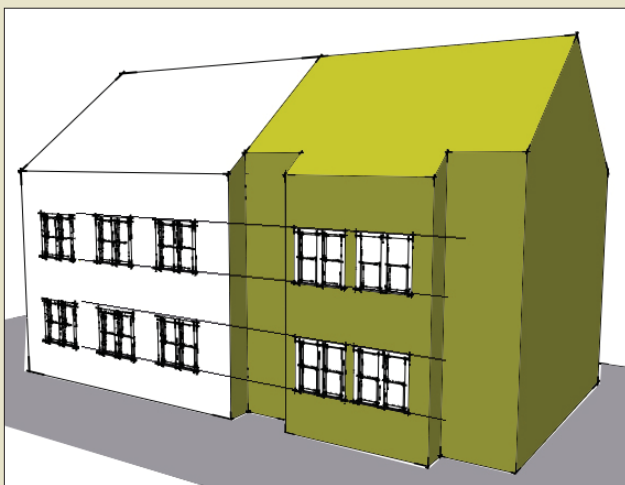
4.C. ARCHITECTURAL GUIDELINES



Roofs on additions should match the pitch of the main body of the house.



Additions should have the same size eaves and windows and should be in the same proportions.



Match window types, shapes, and proportions similar to those of the original structure.

- f. Distinguish New From Existing Structures
 - i. An addition shall be discernible from, yet compatible with, the existing structure.
 - ii. Differentiation should be achieved through a break in roofline, cornice height, wall plane, change in materials, siding profile, or window type.
 - iii. A vertical change should be established between the original portions of the house and the addition to avoid one long wall plane. This change should run from the foundation through to the roof line.

C10. Decks and Balconies

- a. Decks and balconies shall be on the rear of the structure and recessed from the side elevation a minimum of 2 feet.
- b. Decks shall not be constructed above the first story.
- c. Balconies constructed above the first floor/story shall be limited to no more than 60 square feet.

C11. Fire Escapes

CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

- a. Ramps, lifts, and accessible entrances should be designed in such a way to avoid damage to character-defining features of a building.
- b. Fire escapes should be located at the rear of the structure.
- c. Fire escapes should be metal.

C12. Energy Efficiency

- a. Construction of any new structures or alterations to existing structures should be done in such a way as to maintain character while maximizing energy efficiency.
- b. Maximizing energy efficiency should not be motivation to demolish a historic, contributing, or potentially contributing structure, or to change a structure in such a way that its historic features are modified or removed.

C13. Architectural Barriers and Accessibility

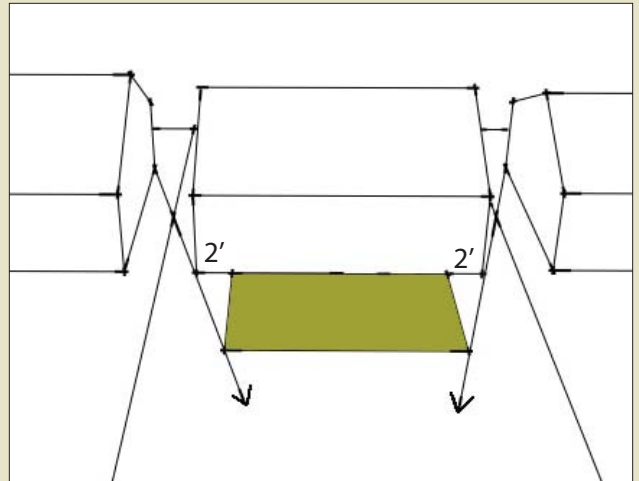
- a. Accessibility to properties should be achieved with careful and creative design solutions when needed or required.
- b. Ramps, lifts, and accessible entrances should be designed in such a way to avoid damage to character-defining features of a building.

Mechanical Equipment

- c. New mechanical equipment should be located a minimum of 10 feet behind the front facade of the structure.
- d. When mechanical equipment must be attached to the exterior wall, historic exterior wall material should be minimally affected. For masonry walls, all attachments shall anchor into the mortar rather than the masonry unit.
- e. Rainwater collection systems should be located behind the primary facade. They should use traditional materials such as

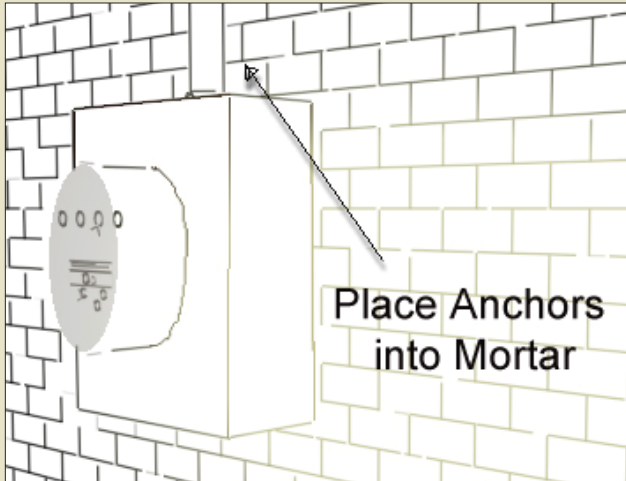


An unacceptable example showing why all decks should be on the rear of the structure and not visible from the front yard.



Decks should be recessed a minimum of 2' from the side elevation.

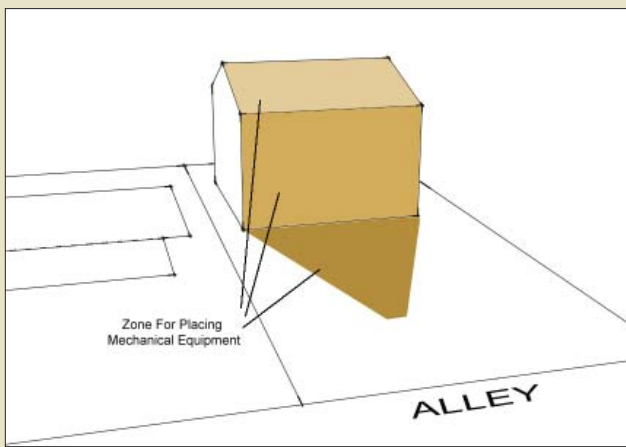
4.C. ARCHITECTURAL GUIDELINES



Place anchor in a way that minimizes damage to the structure.



Photovoltaic and solar thermal panels should be in scale with the existing structure's roof line and should be in same plane as the roof line.



All mechanical utilities should strive to be located to not be visible from the street.

metal and wood.

- f. Photovoltaic and solar thermal installations shall be designed to be in scale with the existing structure's roofline, and shall not damage historical architectural features or materials. These roof systems shall be on the same plane as the roof.
- g. Photovoltaic, solar thermal and satellite dishes (external systems) should be located on ancillary/secondary structures or new additions.

C14. Exterior Lighting

- a. Exterior lighting should be incorporated into renovations or new construction.
- b. Lights shall only shine directly onto the property to which they are attached. They shall not shine onto an adjacent property.
- c. Lights should be shielded as to not glare on adjacent properties, so that "dark skies" are maintained and light does not bleed beyond the property lines. Lights shall only shine directly onto the property to which they are attached.

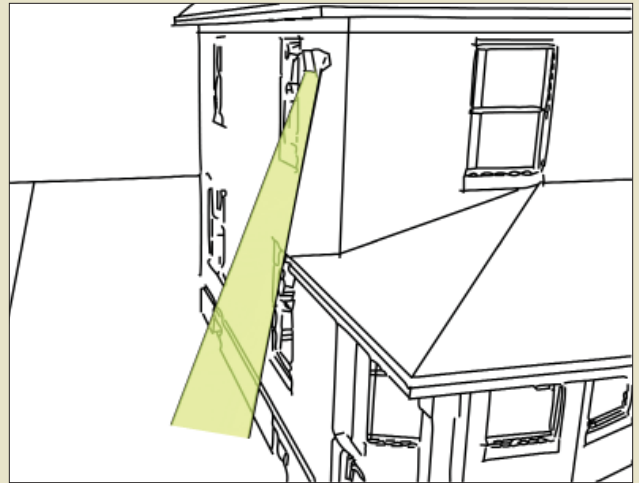
CHAPTER 4 NEIGHBORHOOD WIDE DESIGN GUIDELINES

They shall not shine onto an adjacent property.

- d. Lighting may be included on buildings, along pathways and walkways, along the edge of the alleys and parking, and on porches.
- e. Constant level of light should provide reasonably good visibility. Harsh bright spots and shadows should be avoided.



Low voltage lighting along paths and on porches is appropriate.



Exterior lighting shall not shine onto adjacent property.

4.D DEMOLITION OF EXISTING STRUCTURES



Rehabilitation of an existing structure.



Character defining features.

D. DEMOLITION OF EXISTING STRUCTURES

Goal: Demolition of historic structures is rarely positive for a neighborhood because it destroys the relationships between the structures, landscape features, and open space, and as a result the overall character of the area is diminished. Demolition removes the opportunity for a future owner to rehabilitate the existing structure.

- D1. Character-defining structure(s) shall not be demolished.
- D2. Features that define the character of a listed property shall be retained.
- D3. When removal of a character-defining feature or structure is necessary, a new feature or structure that is compatible with the district shall be installed.
- D4. Plans for compatible replacement of features or structures shall accompany a request for demolition of character-defining features or structures.
- D5. Open space, such as a parking lot or park, shall not be created by demolition of any character-defining structure(s).
- D6. Character-defining structure(s) shall not be demolished and replaced with a historic building from off site.
- D7. Principal and Accessory Structures that are 50 years old or older at the time of demolition application shall be reviewed by the Historic Resources Commission in accordance with the Secretary of the Interior Standards to make a determination on demolition. Structures which are not 50 years old or older at the time of application may be approved by staff.



5

CHAPTER 5: DISTRICT SPECIFIC DESIGN GUIDELINES

The Oread Neighborhood contains a number of unique districts.

Each of these districts have their own special character. Chapter 4 provides guidelines that were applicable neighborhood wide and apply to all districts. Chapter 5 will address the unique character of each district.

The intent of Chapter 5 is to help manage changes in each specific district, so that over time the design character of each district within the Oread Neighborhood will remain distinct.

Chapter 5 addresses the guidelines that are exclusive to Districts 1, 2 and both Historic Districts. District 3 will use the Chapter 4 guidelines for residential development and will refer to Chapter 6 for the commercial guidelines. If Chapter 5 is silent on any given subject, then it may be assumed that the guidelines of Chapter 4 apply.

The Historic District Guidelines are specifically written for the restoration, rehabilitation, and renovation of existing buildings within a historic district. They address additions to contributing and non-contributing buildings, and they address some particular aspects of new construction within an historic district.

It is recommended that Chapter 4 be read prior to Chapter 5, so that a comprehensive understanding of the design guidelines is achieved before embarking on a new project.

5.A. DISTRICT 1



District 1 Boundary Map.



Residential structures are spaced farther apart in District 1.



Residential structures are of a more modest scale in District 1.

A. DISTRICT 1- LOW DENSITY

The following design guidelines are unique to District 1 and only apply to this distinctive area known as Stadium North.

District 1 is a unique district within the Oread Neighborhood. Primarily zoned RM12D, the residential structures are spaced farther apart and are generally of a more modest scale than other parts of the neighborhood. There is a strong desire by neighborhood residents to protect the feel and character of the district by maintaining the scale of the existing houses.

The intent of these specific guidelines is to retain the scale of the original detached dwellings in the neighborhood, while allowing for the density permitted by the Land Development Code. The streetscape, the size and location of structures and the feel of the neighborhood from the public realm should remain unchanged.

Historically, lots within District 1 were drawn to encourage the construction of single-family residential structures along the streets. Any alterations to the lots shall be done to maintain or restore the historic pattern and use of district.

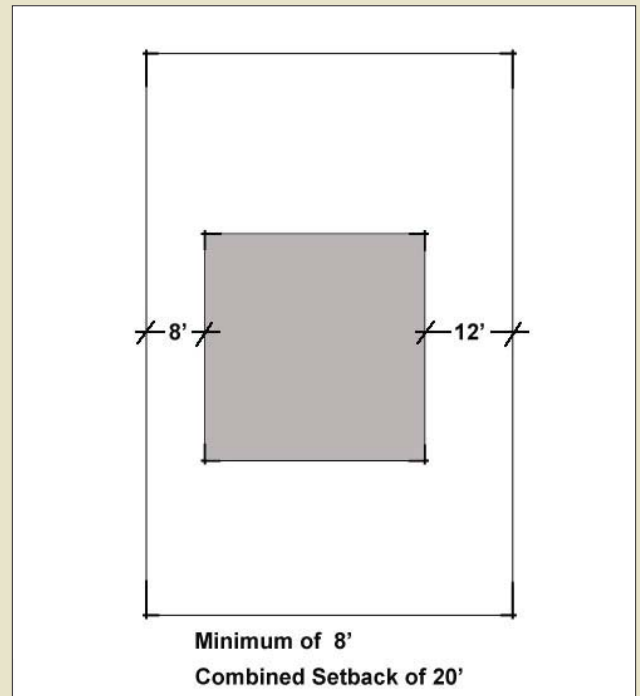
Lots presently platted and meeting the requirements for duplex development under the provisions of the Land Development Code, a lot having a minimum of 7,260 square feet, may maintain the ability to construct this type of use. Within District 1, lots shall not be created or modified to accommodate the construction of duplexes or other higher intensity residential dwellings.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

A1. Sideyard Setbacks

a. Primary Structure

The minimum combined side yard setback for District 1 shall be 20 feet. The minimum per side shall be 8 feet. This allows for flexibility in locating the house on the lot and retains the diversity of the existing pattern.

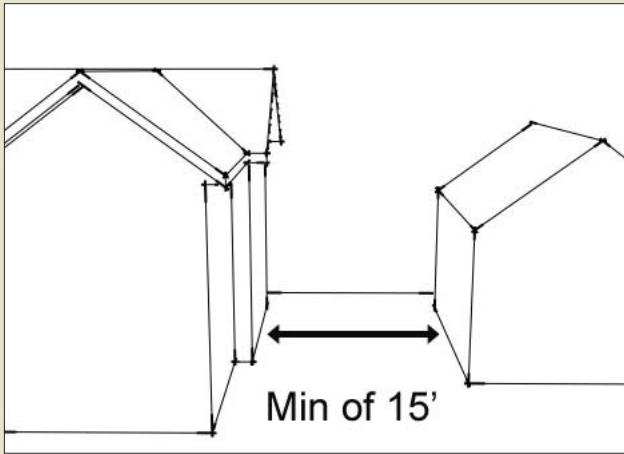


Total side yard setback should be 20' wide. Minimum is 8'.

5.A. DISTRICT 1



A good example of maintaining open space.



Maintain at least 15' of open space between structures.

A2. Open Space

- a. Open space shall be defined as the space between a primary structure and the alley.
- b. Open space shall not be used for parking.
- c. There should be a minimum of 15 linear feet between the garage, parking pad, and the primary structure.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

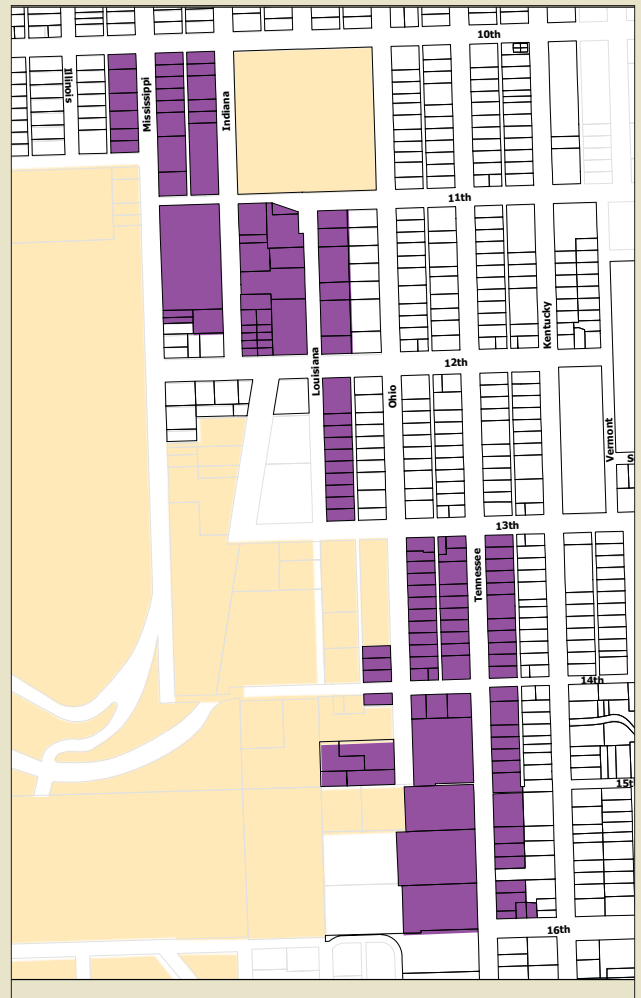
B. DISTRICT 2 - HIGH DENSITY

The following design guidelines are unique to District 2 and only apply to this distinctive area adjacent to the eastern edge of the University of Kansas.

District 2 is a unique district within the Oread Neighborhood. Primarily zoned RM32, this area has seen a number of the original homes demolished and replaced with apartment buildings. Many of these apartments date from the 1960's and 1970's.

The intent of these specific guidelines is to ensure that all new buildings are compatible with the scale and character of the Oread Neighborhood while allowing for a greater density of development.

Portions of the Oread Neighborhood are zoned Mixed Use (MU). The Oread Design Guidelines are intended to supplement the provisions of the Land Development Code's MU Zoning. If the provisions of the Land Development Code's Mixed Use District (MU) are inconsistent with the Oread Design Guidelines, or if they are in conflict, provisions of the Oread Design Guidelines shall control.



District 2 Boundary Map.



Oread Hotel.

5.B. DISTRICT 2



Structure constructed after the “Period of Significance”.



Lot consolidation for larger structures.



Parking should be accessed through the alley.

B1. Demolition of Existing Structures

- a. Demolition of an existing structure may be appropriate in District 2 if the structure is not listed on the National Register of Historic Places, State or Local listings.
- b. Demolition may be appropriate if the structure was constructed after the “Period of Significance” of the Neighborhood, 1877-1945, as discussed in Chapter 3.

B2. Lot Consolidation

- a. Lot consolidation shall not involve alley closures or street closures.
- b. Lot consolidation shall not involve the interruption of the established street grid pattern or the creation of a cul-de-sac.
- c. Lot consolidation, which includes demolition, may be appropriate if it consistent with the guidelines in the Demolition of Existing Structures section above.

B3. Access Standards

- a. Parking should be accessed through the alley.
- b. Where an alley is not present, parking and driveway access shall be consistent with the rest of the block.
- c. Driveways shall be at a residential scale.
- d. Entrances to garages, and subterranean garages should be accessed through the alley where feasible.

B4. Parking Standards

- a. Parking shall not be located in the front yard.
- b. Parking lots should be screened along the public street.

B5. Service Standards

Services, including all utility access and above ground equipment, trash carts, and dumpsters should be located on alleys, where alleys exist.

B6. Frontage Standards for Multi-Dwelling Structures

- a. Each ground floor residential unit that is adjacent to the street shall be oriented with front entrances to the street accessed through a front door that faces the street.

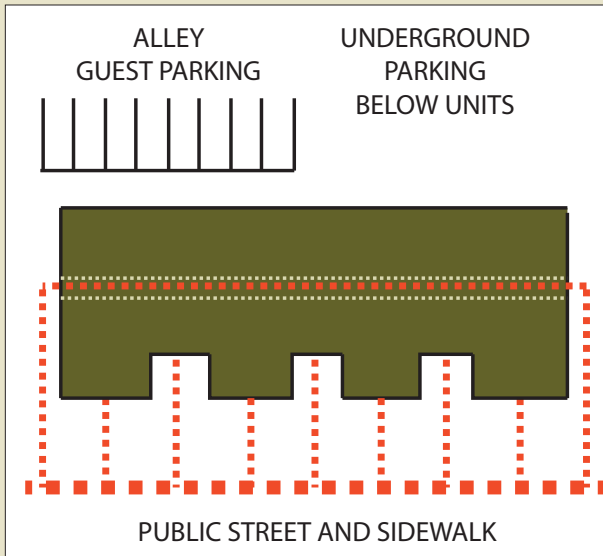


Parking shall not be located in the front yard.



Multi-dwelling structure has individual front doors.

5.B. DISTRICT 2



"House scale" elements and masses relate to the neighborhood.



Apartments shall step down to smaller scale residences.

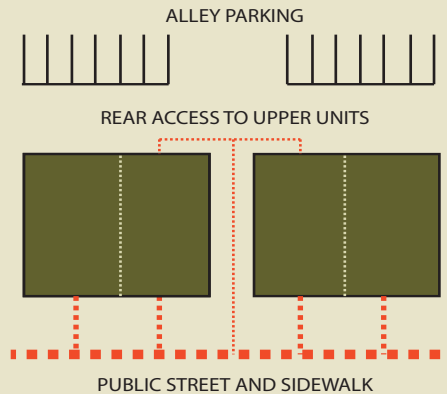
B7. Building Size and Massing Standards

- The facade of large buildings shall be no greater than 40 feet wide after which the building shall be set back a distance of at least 8 feet for the next 20 feet, before returning to the initial setback. This is to reflect the size of a typical Oread house and the side yard spacing between houses. These offsets should be expressed from the foundation through the roof line.
- New construction should be composed of "house scale" elements and masses that relate to the scale of the original neighborhood. Porches, dormers, and projecting bays can help achieve this standard.
- Windows should be vertical in proportion. The scale should be similar to historic windows which are generally 5 feet minimum in height. Windows may be paired.
- Roofs should be pitched.
- New structures should step down to within 1 story of adjacent structures.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

DUPLEX / TRIPLEX / FOURPLEX

Attached dwellings giving a large single-dwelling appearance.



Stories: 2 - 2.5

Parking:

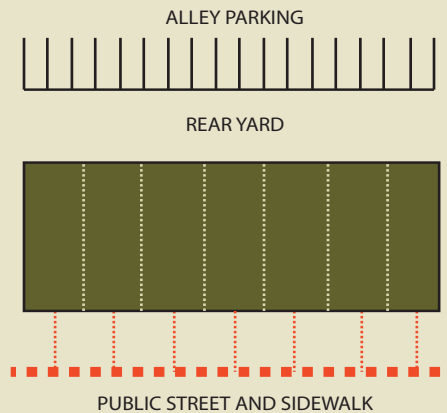
In individual garages to rear or garages via alley.

Porches/Yards:

Porches and stoops front the street. Private patios to the rear.

ROW HOUSES

Attached dwellings, multi level, typically in rectangular form.



Stories: 2 - 3

Parking:

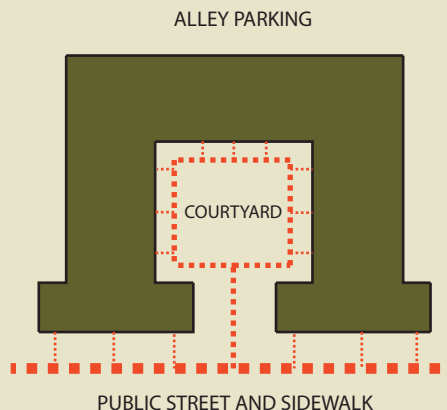
In individual garages at rear or garages via alley.

Porches/Yards:

Private yards in front and/or rear.

COURTYARD HOUSING

Attached dwellings around a central interior courtyard.



Stories: 2 - 3

Parking:

In individual garages to rear or underground garages

Porches/Yards:

Units facing streets have porches and all others have stoops fronting the interior court.

5.C. HISTORIC DISTRICTS

C. OREAD AND HANCOCK HISTORIC DISTRICTS

Work within the Oread and Hancock Historic Districts shall follow the Neighborhood Wide Design Guidelines of Chapter 4 as well as the more stringent guidelines for Historic Districts that are set forth below. The Design Guidelines are based upon the Secretary of Interior's Standards for Preservation, Rehabilitation, Restoration, or Reconstruction, as appropriate.

These Standards can be found on the National Park Service website.

These standards apply to all properties and new construction within the Historic Districts.

For additional guidance, the National Park Service publishes the Interpreting the Standards Bulletins and Preservation Briefs, available from the National Park Service website.



Oread Neighborhood Historic District Boundary Map.



Hancock Historic District Boundary Map.

C1. Demolition

- a. New construction shall meet the standards of section C6.
- b. Historic structures should not be demolished
- c. If beyond repair, a cost analysis and structure analysis and economic analysis will be required as part of the demolition application process

C2. Respect and Retain Original Historic Style and Features

- a. Historic features, including character-defining elements, original scale and massing, shall be retained.
- b. Stylistic elements that were not originally present shall not be added.
- c. Alterations that have no historic basis and that seek to create the appearance of a different architectural period or a false sense of history shall be avoided.
- d. Non-historic alterations that detract from original historic style should be removed.



A good example of details that are well proportioned to the structure.



Preserve original details when possible.



When replacing elements make sure they match the originals in proportion, scale, mass and materials.

5.C. HISTORIC DISTRICTS



C3. Repair or Replace?

A project should demonstrate that the least intrusive treatment option that is feasible is being used. The Secretary's Standards lay out a prioritization for the treatment of historic materials:

- a. When to Preserve:
Deteriorated historic features and architectural elements shall be repaired rather than replaced.
- b. When to Rehabilitate:
If an original architectural feature has become deteriorated beyond repair, the replacement shall match the historic feature in size, scale, profile, and finish.
 - i. Synthetic or composite replacement materials shall match the original in size, scale, profile, and finish.
 - ii. Compatible recycled historic materials may be used provided that the replacement material is compatible with the historic style and character of the resource.
- c. When to Replace:
 - i. Replaced elements shall match the original in size, scale, profile, and finish.
 - ii. An entire missing building should not be reconstructed.
 - iii. Missing architectural features may be restored using photographs, historic architectural drawings, or physical evidence as a guide. Physical evidence may include other matching elements that remain on the building or a "ghost" showing where the missing element historically was attached.
- d. When to Construct New:
 - i. New construction shall not demolish or significantly alter an existing contributing resource.
 - ii. New construction may be appropriate on an empty lot or to the rear of a contributing resource.

C4. Restoration, Rehabilitation, Renovation of Contributing Structures

- a. Facade
 - i. Original elevations of the structure that are visible from the public right-of-way shall be maintained.
 - ii. Appearance, character, arrangement of architectural features, design or materials shall not be changed except to restore a structure to its original appearance.
 - iii. Architectural features shall not be added to a structure if they were never present.
- b. Building Walls
 - i. Damaged walls shall be repaired with like materials that match in color and texture with the weathered material of the original structure.
 - ii. Only those sections that are deteriorated beyond repair should be replaced.
 - (1) Traditional building materials shall be replaced with traditional building materials. For example stucco ages differently than Exterior Insulation and Finish Systems (EIFS) and over time the EIFS looks significantly different than stucco.
 - (2) Natural building materials that age naturally and weather well should be used. For example synthetic stone does not age in the same manner as real stone. Over time the synthetic stone does not retain the characteristics of real stone.



Retain the appearance, character and arrangement of architectural features, such as in this example.



Unlike this example, walls should be repaired with like materials that match in color, texture, and construction method



A good example of using traditional building materials.

5.C. HISTORIC DISTRICTS



An unacceptable example of vinyl siding concealing original features.



Masonry should not be painted.

- iii. Aluminum, vinyl or other synthetic siding shall not be used as a replacement for a primary building material. Artificial siding materials can cause irreparable damage to underlying materials and structural members.
- iv. Unpainted masonry shall not be painted. Moisture may become trapped between the paint and masonry, causing deterioration of the underlying materials.
- v. Simulated modern materials that attempt to emulate traditional materials should not be used.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

b. Roofs

- i. The original shape and slope of the roof as seen from the street shall be maintained. The shape and slope of the original roof is an important character defining feature of the structure.
- ii. Roofing materials should duplicate the appearance and profile of the original materials.
 - (1) The color of the new roofing material should be comparable to the color of the original material.
 - (2) The existing roofing may be replaced with roofing materials that historically would have been appropriate for the building form and style.
- iii. Original decorative roof elements, such as exposed rafter ends, bargeboards, or cornices shall be maintained.
- iv. Decorative roof elements that were not historically present should not be added.
- v. Gutters should be made of copper, galvanized steel or aluminum. Gutters should be half-round or ogee in shape.

c. Dormers

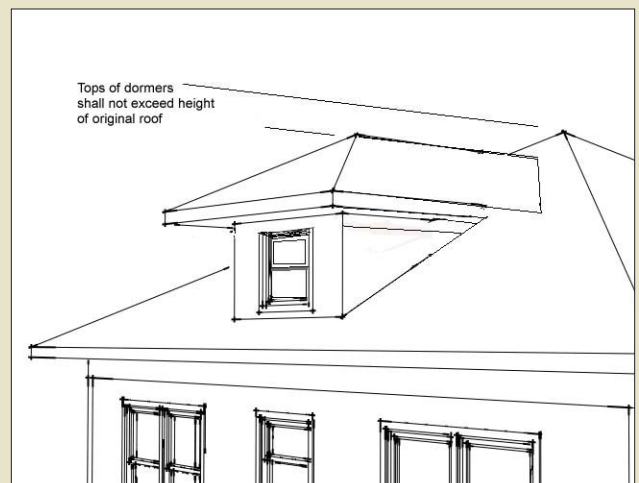
- i. Dormers shall be appropriately sized so as not to overwhelm or dramatically change the character of an historic structure.
- ii. Dormers should be centered over windows on a lower level.
- iii. Dormers should be constructed of lighter materials such as wood, never of brick or stone.
- iv. Windows should be placed in the center of the dormers at least 6 inches from the side edge.



These fiberglass shingles are approximately the scale of the original roofing shingles.



Maintain original decorative roof elements such as these cast roofing tiles.



Keep dormers below the crest of the original roof line.

5.C. HISTORIC DISTRICTS



Retain original porch details such as the balustrade, columns, and porch skirting.



Balustrades are composed of a series of balusters that collectively make a screen that is slightly more opaque than transparent.



If a front porch is screened, it shall be constructed so that the primary architecture elements, such as columns and balustrades are readily visible from the street.

e. Porches

- i. Original front porch elements such as columns, balustrades and decorative trim shall be retained.
- ii. Damaged elements shall be repaired using materials that match the original.
- iii. Only those elements deteriorated beyond repair should be replaced.
- iv. Sufficient documentation should be used to accurately reproduce missing original porch elements. Paint shadows of original profiles, original porch features, examples that remain, or old photos can assist in determining historical designs.
- v. Porch elements that were not historically present shall not be added.
- vi. Front porches should not be enclosed.
- vii. Screened front porches shall be constructed so that the primary architecture elements, such as columns and balustrades are readily visible from the street.
- viii. The addition of screen material should be reversible and should not damage any historic features.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

f. Doors

- i. Original door openings on the front elevation shall not be enlarged or enclosed.
- ii. Doors should not be moved or added.
- iii. Original door openings that have been enclosed may be restored.
- iv. Original doors, jambs, decorative door surrounds, sidelights, and transoms should be retained and repaired using materials that match the original.
- v. Doors and surrounds that have deteriorated beyond repair should be replaced with door surround, sidelight, or transom that match the style, materials, and finish of the original door.
- vi. Inappropriate doors and surrounds shall be replaced with door surround, sidelight, or transom that match the style, materials, and finish of the original door.
- vii. If there is no documentary evidence of the original door, the replacement should reflect the style and period of the house.
- viii. Solid wood doors with recessed panels and frames, and glass lights should be used.



Location, style and size of doors are important character defining features.



Retain original doors, door surrounds, side lights and transoms, like this example.



A good example of a solid wood door with glass lights.

5.C. HISTORIC DISTRICTS



Window placement and patterns are character defining features.



Retain and repair original windows, window surrounds and screens using materials that match the original.

g. Windows and Screens

- i. Original window openings on the front elevations shall not be enlarged or enclosed.
- ii. Window openings on the front elevations should not be moved or added.
- iii. Original window openings that have been enclosed should be restored.
- iv. Original windows, window surrounds, and screens shall be retained or repaired using materials that match the original.
- v. When original windows or screens are deteriorated beyond repair, replacement windows shall maintain the same size, profile, configuration, finish and details as the original windows.
- vi. Replacement materials:
 - (1) Windows should be made of wood.
 - (2) Aluminum clad windows may be used.
 - (3) Solid PVC windows shall not be used.
- vii. Replacement window profiles and jamb conditions should match the original wood windows in detailing and profile thickness.
- viii. The jamb should be recessed from the front facade at the same depth as the original windows.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

- ix. Divided light windows should match the size and pattern of the original window.
 - (1) True divided lights or dimensional mullions that emulate the historic shape should be used.
 - (2) Dimensional mullions shall be placed on the outside of the glass.
 - (3) Window mullions shall not be inserted inside the glass.
- x. Interior storm windows should be used to maintain the historic exterior appearance of the window.
- xi. Storm windows should be installed so that they do not damage historic jambs and surrounds.

h. Chimneys

- i. Original chimneys shall be retained.
- ii. New chimneys should not be visible on the front of the house as seen from the street.



A good example of painted, wooden storm windows that are appropriate for this application.

Acceptable



Unacceptable



Divided light windows should match the size and pattern of the original windows.



Original chimneys should be retained. If adding a chimney, do this on the back of the building so it's not visible from the street.

5.C. HISTORIC DISTRICTS



A good example of how character-defining features on the front of the building shall be preserved.



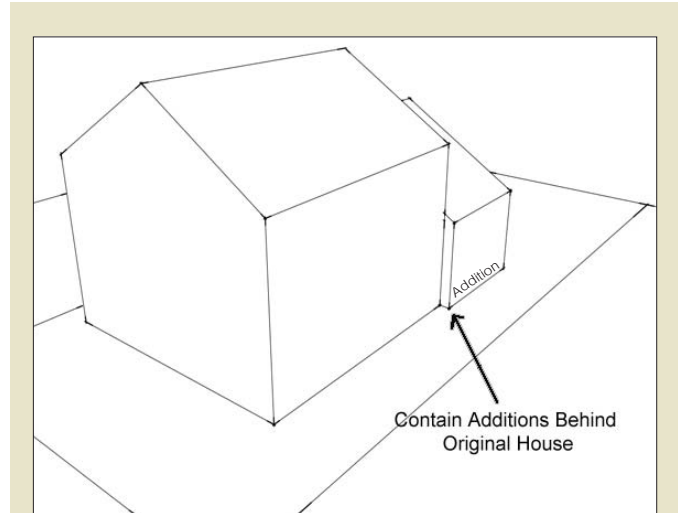
An unacceptable addition at the rear of the property.

C5. Additions to Contributing Structures

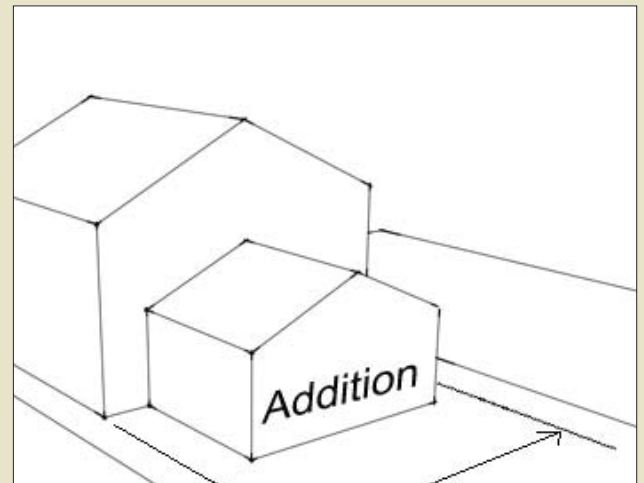
- a. Location and Height
 - i. All character-defining features on the front of the building shall be preserved.
 - ii. Additions shall not project in front of or align to the original front facade.
 - iii. The roof form of the addition shall not be visible above the ridgeline of the original roof when the front of the historic building is viewed from the street.
 - iv. Additions should not overwhelm or overshadow the existing building.
 - v. Additions should appear subordinate and be located as inconspicuously as possible.
 - vi. Additions should reinforce the original historic character, and should not destroy significant historic features.
 - vii. An addition should complement the scale and massing of the original historic building.
 - viii. Features on the sides of the house that are easily seen from the street should be retained.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

- ix. Original building material should be retained when constructing an addition.
- x. Additions should be located behind the original rear facade of the historic building.
- xi. Additions should not be wider than the original house.
- xii. Two-story additions to one-story homes should be built at the rear of the historic building to preserve the original one-story character.
- xiii. The roof height of the addition should be as low as possible to minimize visual impact.
- xiv. The addition should be set back behind the ridgeline of the original roof if the original roof is side-gabled, hipped, or pyramidal in form.
- xv. The minimum setback between the front facade and the addition should be a minimum of 15 feet if the original structure is front-gabled or has a flat roof form.
- xvi. Additions should be compatible in size, scale, massing and materials to the original structure and the adjacent historic structures.

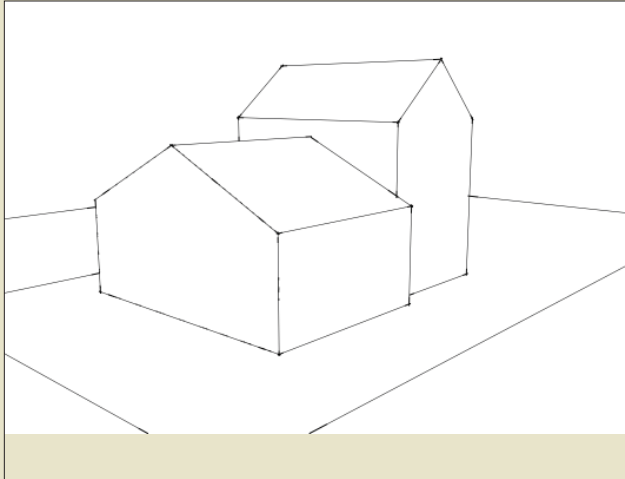


Additions should not be wider than the original house.



Roof height should be as low as possible.

5.C. HISTORIC DISTRICTS



Make the addition compatible with the original (not a replicate) and at the same time it should be vaguely obvious that it's an addition.



Contemporary additions may be appropriate.

b. Design and Style of New Additions

- i. Additions shall be compatible with the historic structure, but should not replicate or give a false sense of history.
- ii. A contemporary design may be appropriate when:
 - (1) The addition is not visible from the street, or
 - (2) The addition does not overwhelm or obliterate the historic building or its architectural features.
- iii. Contemporary designs should respect the scale, forms, and patterns of the original construction.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

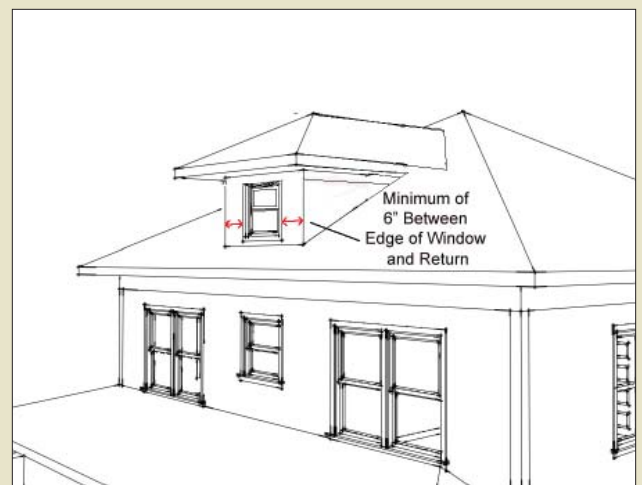
- iv. Additions that are visible from the street shall:
 - (1) Have windows that are the same proportion to the walls,
 - (2) Floor-to-floor heights that are similar,
 - (3) Fenestration patterns, and bay divisions compatible with those on the existing house.
- c. Dormers
 - i. Dormers shall be appropriately sized so as not to overwhelm or dramatically change the character of an historic structure.
 - ii. Dormers should be centered over windows on a lower level.
 - iii. Dormers should be constructed of lighter materials such as wood, never of brick or stone.
 - iv. Windows should be placed in the center of the dormers at least 6 inches from the side edge.
 - v. Dormers should not be located on the main front façade .



Dormers should be appropriately scaled so not to overwhelm or dramatically change the character.



Center dormers on lower level windows.

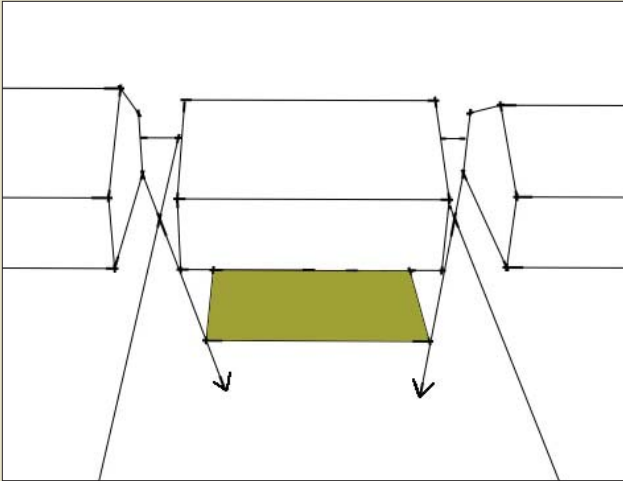


Leave at least 6" between the edge of the window and the outer wall surface.

5.C. HISTORIC DISTRICTS



A good example of how a slight recess helps distinguish the addition from the original.



Back porches and decks should not be visible from the street.

c. Exterior Walls

- i. Additions that are visible from the street (either from the front or from the side) shall be compatible with the exterior wall materials and the architectural styles of the original house.
- ii. Additions should be differentiated from the original structure by varying:
 - (1) Width and profile of wood siding
 - (2) Shape and depth of brick
 - (3) Size of mortar joints
 - (4) Texture
 - (5) Color
 - (6) Slight recess
 - (7) Change in materials
 - (8) Vertical trim board
 - (9) Other appropriate method

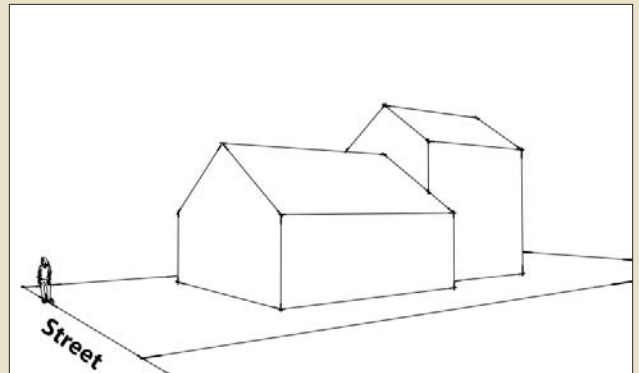
d. Porches & Decks

- i. Front porches shall not be added to contributing structures if one never existed.
- ii. Back porches and decks should not be visible from the street when the house is viewed from the public right-of-way.

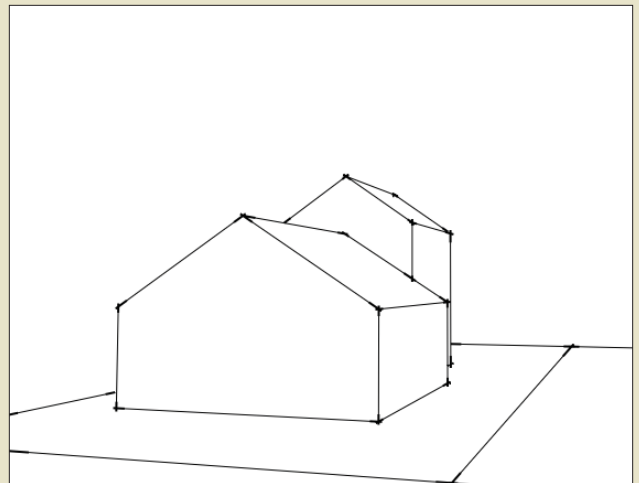
CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

e. Roofs

- i. The ridgeline of the addition should not be visible from the street.
- ii. New roofs should be simple in style and should be compatible with the roof of the contributing structure.
- iii. The slope of the new roof should match the slope of the existing house.
- iv. Roof materials should match or be similar to the roof on the existing house in color, scale, and texture.



The roof of the addition should not be visible from the street.



The slope of the new roof should match the slope of the existing.

5.C. HISTORIC DISTRICTS



When constructing an addition the windows should emulate the windows of the existing structure in terms of proportions, pattern, configuration, profile and finish.



False mullions inserted between the glass in windows with no profile are not appropriate.



Wood screens are appropriate for windows.

f. Windows and Screens

- i. Windows should emulate the windows of the existing structure in terms of fenestration pattern, size, configuration, profile and finish.
- ii. Windows with false mullions inserted between the glass in windows with no profile should not be used.
- iii. Windows with mullions attached to the outside of glass, both interior and exterior, should have a spacer between the glass.
- iv. Anodized or dark coated metal screen should be used to minimize their visual presence.
- v. Window materials:
 - (1) Windows should be made of wood.
 - (2) Aluminum clad windows may be used.
 - (3) Solid PVC windows shall not be used.

- g. Exterior Doors
 - i. Exterior doors should be compatible with those of the contributing structure.
 - ii. Hollow core doors should not be used.

C6. Non-Contributing Structures

- a. When a structure is designated “non-contributing” it does not necessarily indicate that the building has no inherent value, nor does it imply that the building does not play a role in defining the character of the neighborhood. A building can be designated non-contributing because:
 - i. Of its age.
 - ii. It has received unsympathetic remodeling.
 - iii. The structure does not have any character defining features of the neighborhood.
 - iv. It is ill sited.
 - v. It is of a style or design that is foreign to the district.
- b. Some non-contributing buildings can be renovated and become contributing buildings.



Doors in an addition should complement those of the existing house.



Inappropriate alterations and additions can be removed.

5.C. HISTORIC DISTRICTS



Modifying newer building to make them look old is not appropriate.



An inappropriate example of new infill being incompatible with the neighborhood.



Infill construction should incorporate proportions, materials and architectural styles typical of the historic districts.

- c. Alterations and Additions to Non-contributing Structures
 - i. Inappropriate alterations or additions to a historic non-contributing building may be removed to return the building to its historic appearance, based upon physical or photographic evidence.
 - ii. Historic elements and features should not be added to a non-contributing building to make it appear older or to change styles.
 - iii. When a non-contributing building is being modified or remodeled the New Construction guidelines (see below) should be followed so that the building might be made more compatible with the neighborhood.

C7. New Construction

- a. Designed in Context with the Historic District
 - i. New construction should be compatible with the primary design characteristics of the district.
 - ii. Siting lot, as well as the massing, proportions, roofs, fenestration patterns, materials and architectural styles should reflect what is historically found in the District.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

- b. New construction styles should be based on the architectural styles outlined in Chapter 3 of this document.
- c. Contemporary design and style may be appropriate in the historic district if the building respects the scale, massing, proportions, patterns, and materials prevalent among contributing houses within the District.
- d. Exterior Walls
 - i. The exterior wall material of new construction should be constructed of materials that can be found on contributing buildings within the existing historic district.
 - ii. Primary building materials such as wood, brick and stone should be used in comparable ways that they were used on historic buildings.



New construction should be based on the architectural styles outlined in Chapter 3.



Vary shapes and textures to add interest to exterior walls.

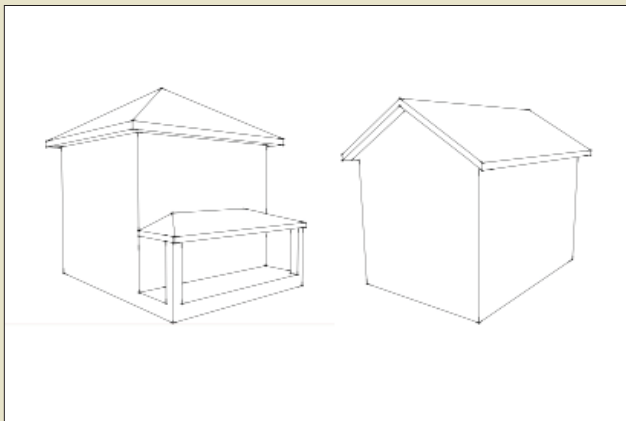
5.C. HISTORIC DISTRICTS



Porches are common character defining features among contributing buildings, such as in this good example.



A good example of how roof details such as dormers, eaves and soffits should also specifically relate to the architectural styles of the neighborhood.



Roof forms should relate to the architectural style of the new construction.

- e. Porches
 - i. New construction should have a front porch.
 - ii. Porch columns, railings, balustrades and detailing should relate to the architectural style of the new building.
- f. Roofs
 - i. Roofs should reflect the character of the roofs on contributing houses within the district.
 - ii. Roof form and materials should relate to the architectural style of the new construction.
 - iii. Roof details such as dormers, eaves and soffits should specifically relate to the architectural style of the new construction.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

g. Windows

- i. Windows in new construction should reflect the patterns of windows in historic buildings within the District.
- ii. The depth of the windows from the wall surface should be similar to those in historic buildings within the Districts.
- iii. The style of windows in new construction should relate to the architectural style of the new construction.
- iv. False mullions attached to or inserted between the glass in windows should have a spacer between the glass.
- v. Screen frames should be made of wood.
- vi. Anodized or dark coated metal screen should be used to minimize their visual presence.
- vii. Recycled windows from a compatible historic period may be used.



Windows in new construction should reflect the patterns of windows in contributing buildings.



Screen frames should be made of wood with bronze or dark metal screen.



Using recycled windows is appropriate.



Doors shall be visible from the street.



Garages and accessory structures shall be detached and located in the rear yard.



Exterior materials should be wood or cementitious board.

h. Doors

- i. Front doors shall be visible from the street.
- ii. Solid wood doors with or without lights should correspond to the style and design of the house.
- iii. Doors with panels and recesses may be used.
- iv. Flat unadorned doors shall not be used on the primary facade.
- v. Recycled doors from a compatible historic period may be used.

i. Garages & Accessory Buildings

- i. Garages and accessory structures should be detached.
- ii. Garages and accessory structures shall be located in the rear yard.
- iii. Garages and accessory structures should be compatible in scale for the property and subordinate to the main structure.
- iv. Garages and accessory structures should be sited to reflect and correspond to the historic patterns of the block on which they are located.
- v. Exterior materials should be wood or cementitious board.

CHAPTER 5 DISTRICT SPECIFIC DESIGN GUIDELINES

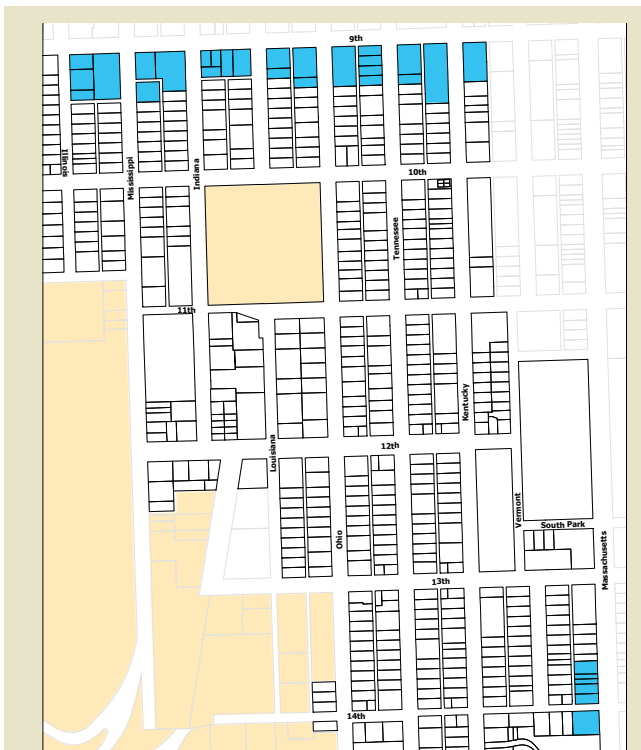
- iv. Garage doors should reflect the overall character of the district.
- v. Garages doors should be made of wood or painted metal to resemble the wooden doors common to the neighborhood.



Garage doors should reflect the style of the main house and character of the district.



CHAPTER 6: COMMERCIAL AND MIXED-USE



District 6 Boundary Map.

The goal of commercial and mixed use design guidelines is twofold: 1) to create a pleasing and attractive urban design setting where businesses can succeed; and, 2) maintain the existing character of the residential neighborhood. The commercial uses should be scaled to serve the neighborhood, providing business opportunities for convenience that would be primarily accessed on foot.

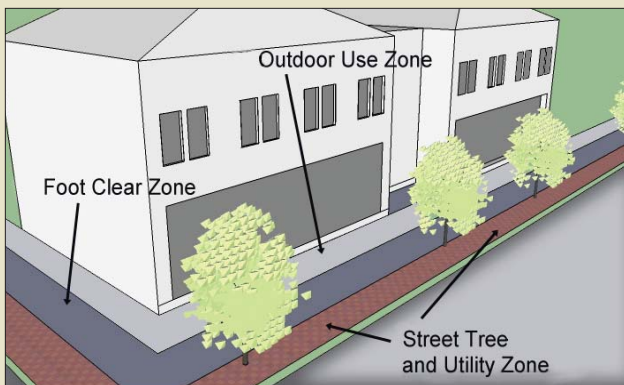
These guidelines are intended to be used only if there is commercial on the ground floor. If the first floor is not commercial then the guidelines in Chapter 4 should be followed.

Portions of the Oread Neighborhood are zoned Mixed Use (MU). The Oread Design Guidelines are intended to supplement the provisions of the Land Development Code's MU Zoning. If the provisions of the Land Development Code's Mixed Use District (MU) are inconsistent with the Oread Design Guidelines, or if they are in conflict, provisions of the Oread Design Guidelines shall control.

6.A. COMMERCIAL PUBLIC REALM



Mixed use structure integrating residential and commercial uses within the same structure.



The sidewalk space shall be composed of a "Street and Furniture Zone", "Clear Zone", and "Outdoor Zone".



A good example of how corner buildings should locate at the lot lines.

A. PUBLIC REALM

A1. Zones

The area in front of the structure shall be composed of a "Street Tree and Furniture Zone", a "Clear Zone" and an "Outdoor Use Zone." See illustration.

A2. Trees

- a. Redevelopment should encourage canopy trees in the "tree zone."
- b. Tree spacing should strive to meet the rhythm and placement of trees along the rest of the street.

A3. Surfacing

- a. Surfacing should include a mixture of different material to create visual interest in the Public Realm.
- b. Materials may include:
 - i. Concrete
 - ii. Brick
 - iii. Tree grates
 - iv. Planting beds
 - v. Permeable paving
 - vi. Slate
 - vii. Stone

A4. Landscaping

- a. All plant material shall be real and living.
- b. Planting beds should contain lower growing vegetation to maintain views of first floor storefronts.

B. SITE DESIGN

B1. Setback

The front of the building shall be located in order to accommodate the Public Realm.

B2. Corner Buildings

Corner buildings should locate at the lot lines.

C. BUILDING DESIGN

C1. Massing

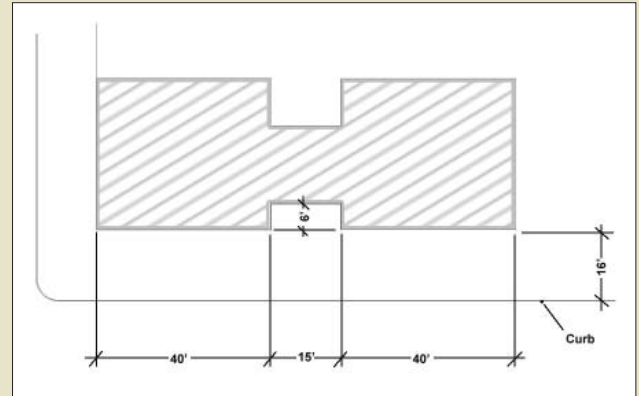
The front facade should be a distance of 40 feet after which the building should be set back a distance of at least 6 feet for 15 feet and then it can return back to the original facade (see figure). The goal here is to emulate the typical building and side yard setback rhythm found in nearby residential fabric. These offsets should be expressed from the foundation through to the roof line.

C2. First Floor

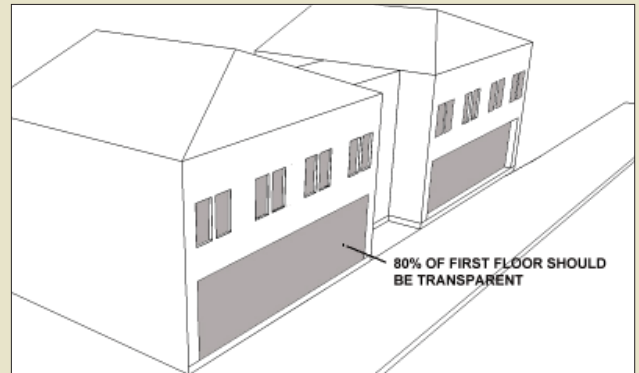
- a. The top of the foundation should be differentiated from the rest of the building with a "water table" or a change of materials between the foundation and upper floors.
- b. Foundations should be covered in real stone or brick, laid so that it appears to be load bearing.
- c. The first floor should be taller relative to other floors in the structure.
- d. The door to the first floor commercial space should be parallel to, visible from, and directly accessible to the front sidewalk.
- e. The first floor commercial spaces should be 70% or more clear glass.
- f. Smoked, frosted or tinted glass shall not be used.

C3. Upper Floors

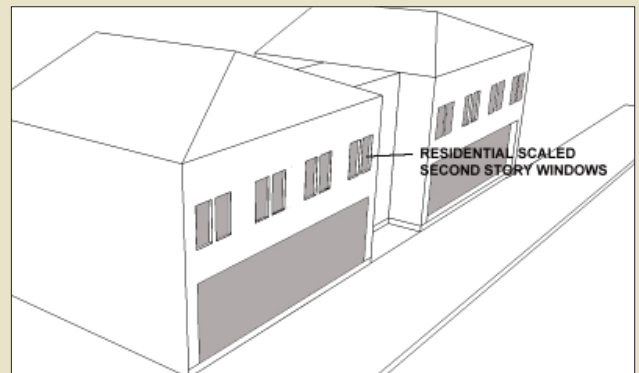
- a. The upper floors should appear to be more residential in nature.
- b. Windows should be taller than they are wide.



The building should be composed of 40' bays separated by a 15' wide recess.



First floor should be at least 80% clear, non frosted and non tinted glass.



Upper floors should be more residential in appearance and about twice as tall as wide.

6.A. COMMERCIAL PUBLIC REALM



An unacceptable example of varying building materials.



Roof lines should be more residential in nature.



Parking lots should be behind the building and incorporate permeable paving.

- c. Group individual windows if more light is desired.
- d. Upper floor windows should comprise between 20% and 30% of the facade.

C4. Materials

- a. Materials should avoid being monolithic in appearance and should be of human scale.
- b. Materials should be:
 - i. Stone
 - ii. Brick
 - iii. Tile
 - iv. Wood
 - v. Non-Corrugated Metal
 - vi. Glass
- c. Plastic, fiberglass and other substitute materials should not be used because they do not age or weather gracefully. Cementitious wood siding is acceptable.
- d. Highly reflective, shiny materials should not be used.

C5. Roof Line

- a. The roof should appear to be more residential in nature.
- b. Flat roofs with cornices typical of downtown commercial buildings should not be used with the exception of the structures along Massachusetts Street.

D. PARKING LOCATION

- D1. All parking should be behind the structure or in the interior of the block.
- D2. Parking should be accessed via the alley and not via additional curb cuts.
- D3. Parking should use permeable paving.

E. SIGNAGE

All provisions of the City of Lawrence's sign ordinance Chapter V, Article 18 shall apply. In addition, the following standards shall apply:

E1. Types

The following types of signs shall be permitted:

- a. A-Frame
- b. Projecting
- c. Wall
- d. Surface mounted
- e. Works of art

E2. Amount and Size of Signage

- a. Businesses may have a total of three different types of signs.
- b. The total amount of signage per business shall not exceed 50 square feet.
- c. No one sign shall exceed 20 square feet.

E3. Awnings

- a. Signs may be placed on an awning.
- b. Awnings shall not be back lit.
- c. Plastic is not an acceptable material.

E4. Projecting Signs

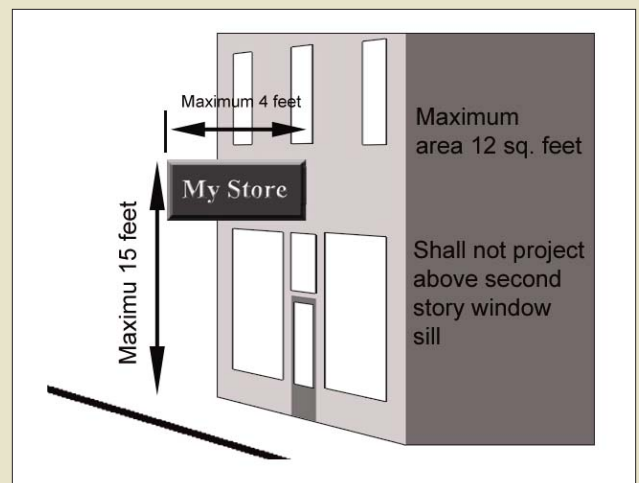
- a. Projecting signs may have two faces but only the square footage of one side shall count towards total sign square footage allocation.
- b. The maximum area shall not exceed 12 square feet.
- c. The sign shall not project more than 4 feet from the structure.



Signage.



A good example of awning signs.



Projecting signs.

6.A. COMMERCIAL PUBLIC REALM



Pedestrian scale projecting signs.

- d. The height at the top of the sign shall not exceed 15 feet from the sidewalk.
- e. The sign shall not project above a second story window sill.

E5. Lighting of Signs

- a. Gooseneck reflectors and lights shall be permitted on surface mounted signs, wall signs, and works of art.
- b. Lights shall be equipped with proper glass lenses concentrating the illumination upon the area of the sign or work of art so as to prevent glare upon the street or adjacent property.
- c. Internally illuminated and back-lighted signs shall not be used.

E6. Ground mounted signs shall not be used.

PLANNING COMMISSION REPORT
Regular Agenda - Public Hearing Item

PC Staff Report
3/21/2016

ITEM NO. 7 TEXT AMENDMENT FOR ADOPTING THE *OREAD NEIGHBORHOOD DESIGN GUIDELINES* (JSC)

TA-12-00171: Consider a Text Amendment to the Land Development Code, Chapter 20 of the Code of The City of Lawrence, Kansas to include the *Oread Neighborhood Design Guidelines*. *Initiated by City Commission on 8/28/2012.*

HISTORIC RESOURCES COMMISSION RECOMMENDATION: Staff recommends that the Historic Resources Commission forward a recommendation for approval to the Lawrence-Douglas County Metropolitan Planning Commission and the Lawrence City Commission for adoption of the proposed Oread Neighborhood Design Guidelines and establishment of the *Oread Neighborhood Design Overlay*, as outlined in this staff report and the attached *Oread Neighborhood Design Guidelines* document.

PLANNING COMMISSION RECOMMENDATION: Staff recommends adopting the *Oread Neighborhood Design Guidelines* and approval of the revised text for Article 3 and forwarding of the proposed guidelines and text amendment to Chapter 20, Articles 3 to the City Commission with a recommendation for approval and adoption.

Reason for Request: The City Commission initiated this amendment on August 28th, 2012 to begin the process of creating Overlay Districts for the areas initially identified in the adopted *Oread Neighborhood Plan*, and to initiate text amendments to various Land Development Code sections incorporating the design guidelines and ensuring consistency with the code.

RELEVANT GOLDEN FACTOR:

- Conformance with the Comprehensive Land Use Plan is the relevant factor that applies to this request. Adoption of a new regulatory tool to use with the existing Land Development Code is an implementation step in the Oread Neighborhood Plan, which is incorporated into Chapter 14: Specific Plans in *Horizon 2020: The Comprehensive Plan for Lawrence and Unincorporated Douglas County*.

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

- A summary of written comments regarding the Oread Neighborhood Design Guidelines is attached.

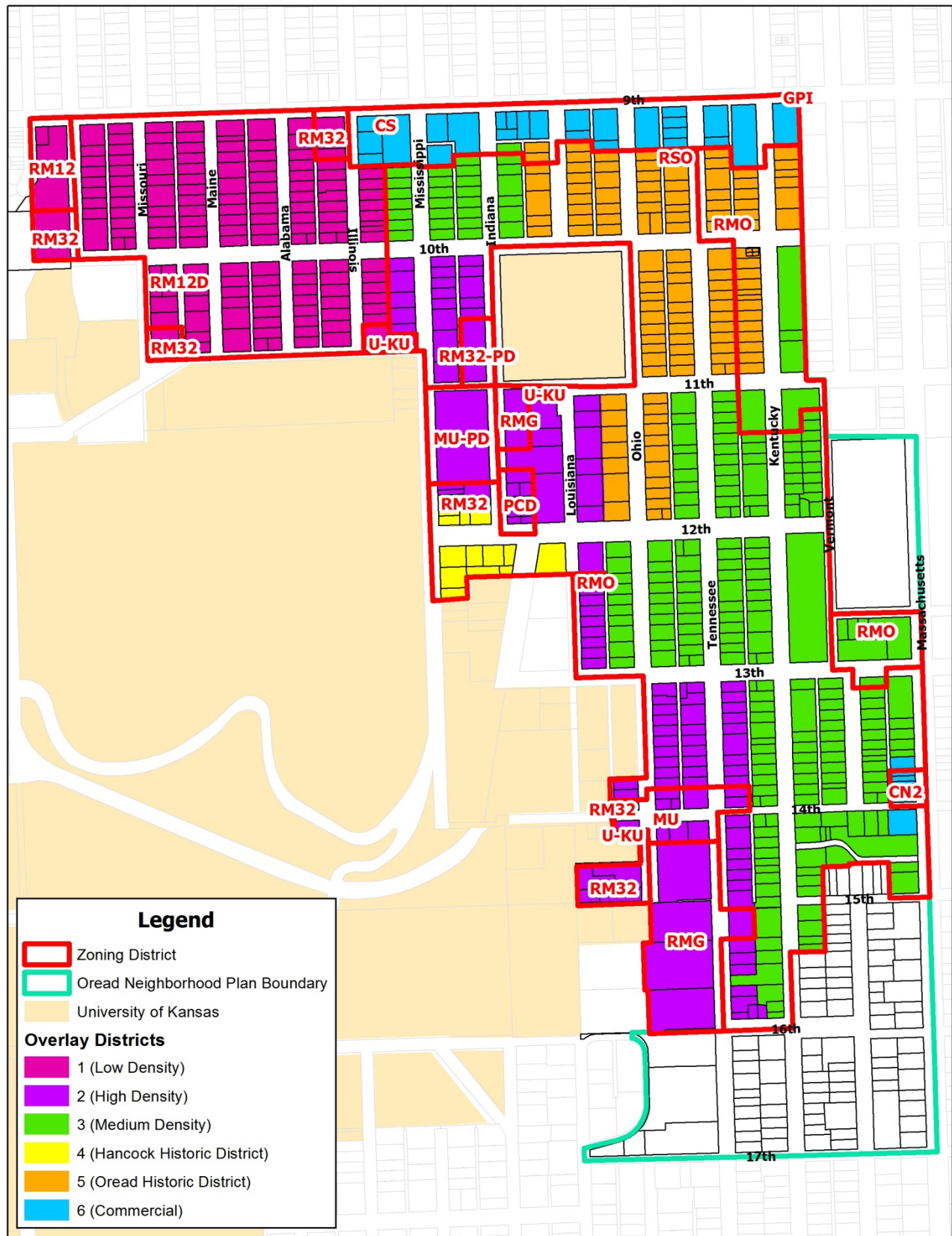


Figure 1: Proposed Overlay Districts with Current Base Zoning

OVERVIEW OF PROPOSED AMENDMENT

The proposed text amendment would incorporate the relevant code language that would assist in implementing and reconciling the differences between the Land Development Code and the *Oread Neighborhood Design Guidelines*. The portion of the neighborhood, including the Overlay Districts, is shown in Figure 1. The properties that are within one of the six districts would have the –UC Overlay applied to them. The properties, as shown in the defined areas of Figure 1, are zoned with a base zoning district that would also include an Urban Conservation Overlay (-UC) District, known as the “Oread Neighborhood Design Guidelines Overlay District.”

The initial recognition of the overlay districts is to create location-specific guidance regarding land use regulations and requirements, such as density and dimensional standards, parking standards, and use standards. The overlay districts are intended to: encourage development that conforms to the size, orientation and setting of existing buildings in a neighborhood or area; reduce the need for zoning variances for development that conforms to the size, orientation and setting of existing buildings in a neighborhood or area; provide building setbacks, lot dimensions and related physical characteristics; foster development that is compatible with the scale and physical character of original buildings in a neighborhood or area through the use of development/design standards and guidelines; and conserve the cultural resources, historic resources and property values within an identified neighborhood or area.

The establishment of the *Oread Neighborhood Design Guidelines* was an action step of the *Oread Neighborhood Plan*, which was adopted by the City Commission in September 2010. In spring 2011, city staff applied for and received a Historic Preservation Fund grant to draft design guidelines for the overlay districts identified in the plan, which was accepted by the City Commission on June 7, 2011. Staff subsequently issued a request for proposals for a consultant for the project which resulted in the selection of Urban Development Services with Ann Benson McGlone. Staff began working with the consultant in January 2012. The first visit by the consultants occurred in February 2012 where they met with staff, design professionals, The Oread Residents Association representatives, and Oread Neighborhood Association representatives.

The first round of public meetings occurred on April 13th and April 14th 2012. Over 400 letters were mailed to property owners, an email was sent to the *Oread Neighborhood Plan* email listserv, newspaper ads were published advertising the meetings, and information was posted on the City of Lawrence website. The April 13th meeting was held at the Lawrence Public Library where approximately 30 people attended where the consultants were introduced, the general concepts of design guidelines were discussed, and a community input activity was completed. This activity was used to identify preferences and what items/issues are important in the neighborhood. The information from this activity was later used as the goals of the guidelines. The April 14th workshop at the Carnegie Building was held for attendees to help identify development patterns within the neighborhood.

The next public meeting was held on May 3rd at the Lawrence Public Library where approximately 20 people attended. The consultants provided a summary and the findings of the April meetings, and showed pictures to understand what type of density and development the attendees would find acceptable in the neighborhood. Over 400 letters were mailed to property owners, an email was sent to the *Oread Neighborhood Plan* email listserv, and information was posted on the City of Lawrence website for this meeting. The 4th public meeting was held on August 2nd, 2012 at the Lawrence Public Library where approximately 15 people attended. The consultants introduced details of the draft design guidelines, though the actual document was not provided.

In January 2013, a joint Planning Commission/Historic Resources Commission subcommittee was established to review the draft of the Oread Design Guidelines, in order to prepare them for public consumption and review. Each commission appointed two members to this subcommittee. The Subcommittee's review process was completed in November 2015, after which a draft will be available for the public in December 2015.

While the adopted neighborhood plan initially called for five districts, through review and consultation in the various drafts of the guideline document, a sixth district was created to provide clarity on commercially zoned properties that are along the edges and could potentially be redeveloped within the Oread Neighborhood as part of potential mixed-use development.

While the *Oread Neighborhood Design Guidelines* contain a significant number of urban design and preservation standards, only a portion would have a direct effect on the Land Development Code.

The following is a summary listing of the proposed changes based on their designation in the *Oread Neighborhood Design Guidelines*:

1. Article 3: Overlay Zoning Districts – to include "Oread Neighborhood Design Guidelines" via Incorporation by Reference.
2. All Districts
 - 2.1. Lot Consolidation
 - 2.2. Front Setback – Article 6: Density & Dimensional Standards
 - 2.3. Side Setback – Article 6: Density & Dimensional Standards
 - 2.4. Open Space Requirements – Article 6: Density & Dimensional Standards
 - 2.5. Parking Configuration – Article 9: Parking, Loading and Access
 - 2.6. Building Orientation – Article 6: Density & Dimensional Standards
 - 2.7. Additions – Article 6: Density & Dimensional Standards
3. District 1 – Low Density
 - 3.1. Lot Dimensions – Article 6: Density & Dimensional Standards
 - 3.2. Lot Consolidation
 - 3.3. Side yard setbacks – Article 6: Density & Dimensional Standards
 - 3.4. Open Space Requirements – Article 6: Density & Dimensional Standards
4. District 2 – High Density
 - 4.1. Lot Consolidation
 - 4.2. Access Standards – Article 9: Parking, Loading and Access
 - 4.3. Parking / Service Standards – Article 9: Parking, Loading and Access
 - 4.4. Frontage Standards – Article 6: Density & Dimensional Standards
 - 4.5. Adjacent Height Stepdowns – Article 6: Density & Dimensional Standards

5. Districts 4 & 5 – Hancock & Oread Historic Districts

5.1. Additions – Article 6

6. District 6 – Commercial District

6.1. Setbacks – Article 6

6.2. Parking Standards – Article 9

Section 20-308(d)(3)(i),a: Description of General Development Pattern

District 1: Located just north of the University of Kansas, this district is predominately comprised of single-family residences with intermittent duplexes, triplexes, and quadraplexes. This district is less dense (6 or less dwelling units per acre) than most parts of Oread neighborhood, with smaller scale homes, larger yards, and houses spaced farther apart. The many one story homes in this area provide it with its unique character. The district is composed of a wide variety of architectural styles constructed over a long period. This was the last area of the neighborhood to develop; therefore, Post-War Minimal Traditional houses are unique to this area. More recent apartments exist on the fringes, mainly on the western side of the district.

District 2: This district is located adjacent to the east side of the University of Kansas Main Campus. It has the highest density (16+ dwelling units per acre) of any Oread district. The proximity to campus has made it a prime location for apartments, multi-dwelling residential, and mixed-use developments. The architectural character of the district is diverse compared to the other districts. There are large Greek houses, Scholarship Halls, and multi-dwelling complexes that are “campus” scale in massing and design. Interspersed are substantial pockets of older homes, some converted to congregate or multi-dwelling uses. The character of this area is distinctly different with larger scale structures, fewer street trees, and hilly terrain. Off-street parking is more visible due to a lack of alleys. Apartment complexes in this district typically were constructed in the 1970s and 1980s, and now are approaching 30 to 40 years in age, offering re-development potential and opportunities for improvements in the neighborhood.

District 3: District 3 is located primarily on the eastern edge of the Oread Neighborhood, and serves as the basis for the design guidelines for all 5 residential districts. The other district guidelines within Chapter 5 of the Guidelines are modifications to this standard tailored to their own distinctive characteristics. Most off-street parking is accessed from alleys, so driveways are rare. The homes sit back from the street. The original single-family homes, constructed between the 1880s and the 1900s, are generally two stories and represent a wide array of styles and sizes. Many have been converted to multi-dwelling uses. Others have been replaced with modern apartments. The density is approximately 7 to 15 dwelling units per acre. Older, commercial buildings and more recent professional buildings can be found towards Massachusetts Street.

District 4: Hancock Historic District is a unique area of the Oread Neighborhood located on a substantial rise that overlooks the University of Kansas. The Hancock Historic District was added to the National Register of Historic Places in 2004. This small district boasts a beautiful and historic public improvement project that adds to the allure of the area: a pair of curved stone steps ascends from Mississippi Street to W. 12th Street in the grand fashion of the City Beautiful Movement. At the top of the stairs on W. 12th Street is a planter of native stone that forms an esplanade in the center of

the street until it intersects with Indiana Street. Most of the lots are larger than those found in the rest of neighborhood, creating a more landscaped setting for most of the homes.

District 5: The Oread Historic District was listed in the National Register of Historic Places in 2007. The Oread Historic District was listed to the Lawrence Register of Historic Places in 1991. It is an area with some of the oldest homes in Lawrence. There are excellent examples of the Italianate, Queen Anne, Craftsman, and 20th Century Revival styles side by side with the more vernacular National Folk and Foursquare homes. They range in size from grand to simple. The streets are lined with tall, deciduous trees. On the west side of the streets, the houses are set back with elevated front yards. Dominant porches and shade-dappled yards add to the appeal of this area. On the east side of the streets, the homes are placed closer to the street with shallow front yards that are relatively flat. Parking is off the alleys; therefore, driveways are rare. Some streets are still paved with the original brick, and hitching posts and stone steps can still be found.

District 6: The guidelines defined commercial district which presently are found fronting W. 9th Street and on the western half of the intersection of 14th Street and Massachusetts Street. The commercial uses were typically scaled to serve the adjacent neighborhoods, providing business opportunities for convenience that would be primarily accessed on foot. These proposed guidelines are intended to be used only if there is commercial on the ground floor. If the first floor is not commercial then the pertinent residential district guidelines would be applicable.

A full copy of listing the proposed changes is attached to this staff report. Text to be deleted is shown with ~~strikeout~~ and proposed text is shown in underlined font.

CRITERIA FOR REVIEW & DECISION-MAKING

Section 20-1302(f) provides review and decision-making criteria on proposed text amendments. It states that review bodies shall consider at least the following factors:

1) Whether the proposed text amendment corrects an error or inconsistency in the Development Code or meets the challenge of a changing condition.

Staff Response: The proposed text amendments address a changing condition. With the adoption of the Oread Neighborhood Plan in 2010, and the subsequent direction from City Commission in 2012, this text amendment would integrate the *Oread Neighborhood Design Guidelines* into the relevant sections of the Land Development Code. Provisions within the *Oread Neighborhood Design Guidelines* would revise certain aspects, such as parking, building envelope, and lot requirements that are integral parts of the guideline document. With the adoption of the guidelines, there would be inconsistencies created between the *Oread Neighborhood Design Guidelines* and the Land Development Code. This text amendment would help reduce these conflicts and inconsistencies.

2) Whether the proposed text amendment is consistent with the Comprehensive Plan and the stated purpose of this Development Code (Sec. 20-104).

Staff Response: The *Oread Neighborhood Design Guidelines* is a key recommendation of the Oread Neighborhood Plan (Section 4.2), which is incorporated into Chapter 14: Specific Plans within *Horizon 2020*. This is consistent with the language and intent of Section 20-104 of the Land Development

Code.

PROFESSIONAL STAFF RECOMMENDATION

Staff recommends adopting the *Oread Neighborhood Design Guidelines* and forwarding a recommendation of approval to the Lawrence City Commission of this text amendment to the Land Development Code, Chapter 20 of the Code of The City of Lawrence, Kansas to incorporate the *Oread Neighborhood Design Guidelines*.

ARTICLE 3. OVERLAY ZONING DISTRICTS

- 20-301 The Districts
- 20-302 ASO, Airspace [Overlay District](#)
- 20-303 FP, Floodplain Management Regulations [Overlay District](#)
- 20-304 HD, Historic District Overlay
- 20-305 HL, Historic Landmark Designation
- 20-306 PD, Planned Development [Overlay District](#)
- 20-307 TC, Major Transportation Corridor Overlay
- 20-308 UC, Urban Conservation [Overlay District](#)
- 20-309 Incorporation By Reference of “The Downtown Design Guidelines 2009 Edition”
- 20-310 Incorporation By Reference of “Design Guidelines 8th & Penn Neighborhood Redevelopment Zone” (2006, Revised October 4, 2011)
- 20-311 [Incorporation By Reference of “Oread Neighborhood Design Guidelines 2016 Edition”](#)

20-301 THE DISTRICTS

(a) [Overlay Districts](#)

[Overlay Districts](#) are tools for dealing with special situations or accomplishing special zoning goals. As the name implies, [Overlay Districts](#) are "overlaid" on [Base District](#) classifications to alter the [Base District](#) regulations. [Overlay Districts](#) are shown on the [Official Zoning District Map](#) as suffixes to the applicable [Base District](#) classification. For example, a CD-zoned [Parcel](#) that is included in the Urban Conservation [Overlay Districts](#) would be shown on the map as CD–UC.

(b) **Districts Established**

The following [Overlay Zoning Districts](#) are included in this Development Code:

District Name	Map Symbol
Overlay Districts	
Airspace Overlay	-ASO
Floodplain Overlay	-FP
Historic District Overlay	-HD
Historic Landmark Designation	-HL
Planned Development Overlay	-PD
Major Transportation Corridor Overlay	-TC
Urban Conservation Overlay	-UC

20-302 ASO, AIRSPACE [OVERLAY DISTRICT](#)

(a) **Purpose**

The ASO, Airspace [Overlay District](#), is intended to:

- (1) prevent the creation and establishment of hazards to life and property in the vicinity of any [Airport](#) owned, controlled or operated by the City of Lawrence;
- (2) protect users of the [Airport](#); and

- (3) prevent any unreasonable limitation or impairment on the use and expansion of the [Airport](#) and the public investment therein.

(b) Authority

The regulations of this District are adopted under the authority granted by K.S.A. Sections 3-701 through 3-713.

(c) Applicability

The Airspace [Overlay District](#) regulations apply to all land or water area lying within the established [Airport](#) control Instrument Approach Zones, Non-Instrument Approach Zones, Transition Zones, Horizontal Zones and Conical Zones as shown on the Airspace Control Zones overlay map.

(d) Effect

The Airspace [Overlay District](#) is a zoning classification that establishes additional restrictions and standards on those uses permitted by the [Base District](#). In the event of conflict between the Airspace [Overlay District](#) regulations and the regulations of the [Base District](#), the [Overlay District](#) regulations govern. In all other cases, both the [Overlay](#) and [Base District](#) regulations apply.

(e) Sub-zones Established

In order to carry out the provisions of this District, the following Airspace Zones are established within the Airspace District. The Airspace [Overlay District](#) and the Airspace Zones shall be shown on the [Official Zoning District Map](#).

(1) Instrument Approach Zone

The Instrument Approach Zone is established at each end of all runways used for instrument landings and takeoffs. The Instrument Approach Zones have a width of 1,000 feet at a distance of 200 feet beyond the end of each instrument runway, widening thereafter uniformly to a width of 16,000 feet at a distance of 50,200 feet beyond each end of the runway, its centerline being the continuation of the centerline of the runway.

(2) Non-Instrument Approach Zone

The Non-Instrument Approach Zone is established at each end of all runways used for non-instrument landings and takeoffs. The Non-Instrument Approach Zone has a width of 500 feet at a distance of 200 feet beyond the end of each non-instrument runway, widening thereafter uniformly to a width of 2,500 feet at a distance of 10,200 feet beyond each end of the runway, its centerline being the continuation of the centerline of the runway.

(3) Transition Zone

The Transition Zone is established adjacent to each instrument and non-instrument runway and approach zone as indicated on the [Official Zoning District Map](#). Transition Zones symmetrically located on either side of runways have variable widths as shown on the [Official Zoning District Map](#). Transition Zones extend outward from a line of 250 feet on either side of the centerline of a non-instrument runway for the length of such runway plus 200 feet on each end; and 500 feet on either side of the centerline of an instrument runway for the length of such runway plus 200 feet on each end; and are parallel and level with such runway centerlines. The Transition Zones along such runways slope upward and outward one (1) foot vertically for each seven (7) feet horizontally to the point where they intersect the surface of the Horizontal Zone. Further, Transition Zones are established adjacent to both Instrument and Non-

Instrument Approach Zones for the entire length of these Approach Zones. These Transition Zones have variable widths, as shown on the [Official Zoning District Map](#). Such transition zones flare symmetrically with either side of the runway Approach Zones from the base of such zones and slope upward and outward at the rate of one (1) foot vertically for each seven (7) feet horizontally to the points where they intersect the surfaces of the Horizontal and Conical Zones. Additionally, Transition Zones are established adjacent to the Instrument Approach Zone where it projects through and beyond the limits of the Conical Zone, extending a distance of 5,000 feet measured horizontally from the edge of the Instrument Approach Zones at right angles to the continuation of the centerline of the runway.

(4) Horizontal Zone

A Horizontal Zone is that area within a circle with its center at the [Airport Reference Point](#) and having a radius of 7,000 feet. The Horizontal Zone does not include the Instrument and Non-Instrument Approach Zones or the Transition Zones.

(5) Conical Zone

A Conical Zone is the area that commences at the periphery of the Horizontal Zone and extends outward a distance of 5,000 feet. The Conical Zone does not include the Instrument Approach Zone and Transition Zones.

(f) Height Limitations

No [Structure](#) may be erected, altered, or maintained in any Airspace Zone to a [Height](#) in excess of the [Height](#) limit established for such Zone, except as otherwise provided in this section. The following [Height](#) limitations are hereby established for each of the Airspace Zones:

(1) Instrument Approach Zone

One foot in [Height](#) for each 50 feet in horizontal distance beginning at a point 200 feet from and at the centerline elevation of the end of the instrument runway and extending to a distance of 10,200 feet from the end of the runway; thence one foot in [Height](#) for each 40 feet in horizontal distance to a point 50,200 feet from the end of the runway.

(2) Non-Instrument Approach Zone

One (1) foot in [Height](#) for each 20 feet in horizontal distance beginning at a point 200 feet from and at the centerline elevation of the end of the non-instrument runway and extending to a point 10,200 feet from the end of the runway.

(3) Transition Zone

One (1) foot in [Height](#) for each seven (7) feet in horizontal distance beginning at any point 125 feet normal to and at the elevation of the centerline of non-instrument runways, extending 200 feet beyond each end thereof, and 500 feet normal to and at the elevation of the centerline of the instrument runway, extending 200 feet beyond each end thereof, extending to a [Height](#) of 150 feet above [Airport](#) elevation. In addition to the foregoing there are established [Height](#) limits of one (1) foot vertical [Height](#) for each seven (7) feet horizontal distance measured from the edges of all Approach Zones for the entire length of the Approach Zones and extending upward and outward to the points where they intersect the horizontal or conical surfaces. Further, where the Instrument Approach Zone projects through and beyond the Conical Zone, a [Height](#) limit of one (1) foot for each seven (7) feet of horizontal distance shall be maintained beginning at the edge of the Instrument Approach Zone and extending a

distance of 5,000 feet from the edge of the Instrument Approach Zone measured normal to the centerline of the runway extended.

(4) Horizontal Zone

[Height](#) may not exceed 150 feet above the [Airport](#) elevation.

(5) Conical Zone

One (1) foot in [Height](#) for each 20 feet in horizontal distance beginning at the periphery of the Horizontal Zone, extending to a [Height](#) of 400 feet above the [Airport](#) elevation.

(g) Performance Standards

Notwithstanding any other provision of this section, no use or [Development Activity](#) may occur on land within any Airspace [Overlay District](#) that:

- (1) creates electrical interference with radio communications between the [Airport](#) and aircraft, including radio and television transmitting towers or studios and large radiation or X-ray equipment;
- (2) includes aboveground storage of petroleum or any other explosive material.
- (3) emits smoke or odor; emits smoke or odor;
- (4) contains lights or signals that may be confused with [Airport](#) navigational lights;
- (5) results in glare to pilots approaching, leaving or circling the [Airport](#) or that impairs visibility in the District;
- (6) provides private airfields or runways for the use of aircraft other than those used in the principal [Airport](#) in the District; or
- (7) otherwise endangers the landing, taking-off, or maneuvering of aircraft.

(h) Nonconformities

- (1) The regulations set forth in this section do not require the removal, lowering, or other change of any [Structure](#) not conforming to these regulations or otherwise interfere with the continuance of any nonconforming use, except as provided in Section 20-302(h)(2) and 20-302(i)(6).
- (2) The City may require, upon 30-days written notice, any person, firm, association, or corporation owning and maintaining any nonconforming pole or pole line upon the roads and highways immediately adjoining the [Airport](#) to remove, lower, change, or alter said nonconforming pole or pole line. Prior to the removal, lowering, or changing of the pole or pole line, the [Owner](#) or [Owner](#) of the [Airport](#), shall pay said person, firm, association or corporation the reasonable and necessary expense of removing, lowering or changing said pole or pole lines; or in lieu thereof shall execute good and sufficient bond with corporate surety thereon as security for the payment of the reasonable and necessary expense of removing, lowering or changing such pole or pole lines. The reasonable

and necessary expense of removing, lowering or changing said pole or pole lines may include, among other items of expense, the actual cost of:

- (i) constructing underground conduits and the construction of such wires and equipment in such conduits; and
- (ii) rerouting wires together with the poles, cross arms, and other equipment connected thereto, together with the cost, if any, of new right-of-way made necessary by such rerouting.

(i) Permits

(1) Future Uses

Except as specifically provided by the exceptions stated in Section 20-302(i)(4), no material change may be made in the use of land and no [Structure](#) may be erected, altered, or otherwise established in any [Airspace Overlay District](#) unless a permit has been applied for and granted. Each application for a permit shall indicate the purpose for which the permit is desired, with sufficient particularity to permit it to be determined whether the resulting use or [Structure](#) would conform to the regulations set forth in this section. If such determination is in the affirmative, the permit shall be granted.

(2) Existing Uses

No permit may be granted that would allow the establishment or creation of an [Airport Hazard](#) or permit a nonconforming use, or [Structure](#) to be made or become higher, or become a greater hazard to air navigation than it was on the [Effective Date](#), or the [Effective Date](#) of any amendments hereto, or than it is when the application for a permit is made. Except as provided herein, all applications for permits shall be granted.

(3) Nonconforming Uses

Before any nonconforming [Structure](#) may be replaced, substantially altered or repaired, rebuilt, or increased in [Height](#), a permit shall be obtained authorizing such replacement, alteration, change or repair.

(4) Exceptions

- (i) In the area lying within the limits of the Horizontal Zone and the Conical Zone, no permit will be required for any [Structure](#) less than 75 feet in vertical [Height](#) above the ground, except where, because of terrain, land contour or topographic features, such [Structure](#) would extend above the [Height](#) limits prescribed for such Zones.
- (ii) In the areas lying within the limits of the Instrument and Non-Instrument Approach Zones but at a horizontal distance of not less than 4,200 feet from each end of the runways no permit will be required for any [Structure](#) less than 75 feet in vertical [Height](#) above the ground, except where, because of terrain, land contour or topographic features, such [Structure](#) would extend above the [Height](#) limits prescribed for the Instrument or Non-Instrument Approach Zone.
- (iii) In the areas lying within the limits of the Transition Zones beyond the perimeter of the Horizontal Zone, no permit will be required for any [Structure](#) less than 75 feet in vertical [Height](#) above the ground except where such [Structure](#), because of terrain, land contour or

topographic features would extend above the [Height](#) limit prescribed for such Transition Zones.

- (iv) Nothing contained in any of the foregoing exceptions will be construed as permitting or intending to permit any construction, or alteration of any [Structure](#) in excess of any of the [Height](#) limits established by this section.

(5) Variances

Any person desiring to erect any [Structure](#) or increase the [Height](#) of any [Structure](#), or otherwise use his property in violation of the [Airspace Overlay District](#) regulations, may apply to the City Commission of the City of Lawrence for a variance from the zoning regulations in question. Such variances will be allowed where a literal application or enforcement of the regulations would result in practical difficulty or Unnecessary Hardship and the relief granted would not be contrary to the public interest but do substantial justice and be in accordance with the spirit of this section: Provided, that any variance may be allowed subject to any reasonable conditions that the City Commission may deem necessary to effectuate the purposes of this section.

(6) Hazard Marking and Lighting

Any permit or variance granted may, if such action is deemed advisable to effectuate the purposes of this section and be reasonable in the circumstances, be so conditioned as to require the [Owner](#) of the [Structure](#) in question to permit the City of Lawrence, at its own expense, to install, operate, and maintain thereon such markers and lights as may be necessary to indicate to flyers the presence of an [Airport](#) Hazard.

(j) Administration and Enforcement

For the purposes of this section and pursuant to K.S.A. 3-707, the Lawrence/Douglas County Metropolitan [Planning Commission](#) will be the [Airport](#) Zoning Commission for the City of Lawrence and will have responsibility for administering and enforcing the regulations set forth in this section.

- (1) In particular, the [Airport](#) Zoning Commission shall review all permit applications and determine if such should be granted. If an application is found to conform to all the [Airspace Overlay District](#) regulations, the [Airport](#) Zoning Commission shall grant the permit.
- (2) Applications for permits and variances shall be made to the [Planning Director](#) upon forms furnished by the [Planning Director](#).
 - (i) Applications for permits shall be submitted at least 35 days prior to a regular meeting of the [Planning Commission](#).
 - (ii) Applications for variances shall be submitted at least 35 days prior to a regular meeting of the City Commission.

(k) Conflicting Regulations

In the event of conflict between the [Airspace Overlay District](#) regulations and any other regulations applicable to the same area, whether the conflict be with respect to the [Height](#) of [Structures](#), use of land, or any other matter, and whether such other regulations were adopted by the City of Lawrence or any other unit of local government, the more stringent limitation or requirements as to [Airport](#) Hazards will govern and prevail.

20-303 FP, FLOODPLAIN MANAGEMENT REGULATIONS OVERLAY DISTRICT

The FP, Floodplain Management Regulations are implemented as an [Overlay District](#). The established regulatory provisions affecting land in the FP District are set out in Article 12, Floodplain Management Regulations.

20-304 HD, HISTORIC DISTRICT OVERLAY

Historic Districts designated in accordance with the provisions of Chapter 22 of the City Code shall be submitted to the [Planning Commission](#) as a recommended Zoning Map Amendment and processed in accordance with Section 20-1303. If approved by the City Commission, the HD [Overlay District](#) shall be shown on the [Official Zoning District Map](#) with the map symbol “– HD” and shall be governed by the relevant provisions of Chapter 22.

20-305 HL, HISTORIC LANDMARK DESIGNATION

Historic Landmarks designated in accordance with the provisions of Chapter 22 of the City Code shall be shown on the [Official Zoning District Map](#) with the map symbol “– HL” and shall be governed by the relevant provisions of Chapter 22.

20-306 PD, PLANNED DEVELOPMENT OVERLAY DISTRICT

See Section 20-701 for purposes, standards and procedures for the PD District.

20-307 TC, MAJOR TRANSPORTATION CORRIDOR OVERLAY**(a) Purpose**

The TC, Major Transportation Corridor [Overlay District](#), is intended to protect properties adjacent to the transportation corridors from the noise, activity, light and dust of vehicular traffic by requiring [Building Setbacks](#) and Landscaping along the corridors.

- (1) SLT/K10-TC, South Lawrence Trafficway Corridor [Overlay District](#), is designed to create an aesthetically pleasing corridor along the South Lawrence Trafficway, in keeping with the SLT policy of providing a park-like setting. The SLT Corridor [Overlay District](#) does not affect land use regulations or development standards of the [Base Districts](#) except as specifically specified in this section.
- (2) 23rd/K10-TC, 23rd Street/K10 Corridor [Overlay District](#) (section reserved)
- (3) Iowa/US 59-TC, Iowa Street/US Highway 59 Corridor [Overlay District](#) (section reserved)
- (4) 6th/US 40-TC, 6th Street/ US Highway 40 Corridor [Overlay District](#) (section reserved)
- (5) N. 2nd/US 24,40 & 59 -TC, N. 2nd Street/US Highways 24, 40 & 59 Corridor [Overlay District](#) (section reserved)
- (6) 31st Street Corridor [Overlay District](#) (section reserved)

(b) Boundaries

- (1) SLT/K10-TC: The boundaries of the SLT/K10 [Overlay District](#) are shown on the [Official Zoning District Map](#). The SLT [Overlay District](#) extends 500 feet on either side of the centerline of the SLT/K10 right-of-way within the city limits of Lawrence.
- (2) 23rd Street/K10-TC: The boundaries of the 23rd Street/K10 Corridor [Overlay District](#) are shown on the [Official Zoning District Map](#). (Section reserved for adoption of boundaries based on adopted 23rd Street Corridor Study)
- (3) Iowa/US 59-TC: (Section reserved for adoption of a major corridor plan and boundaries for this [Overlay District](#))
- (4) 6th Street/US 40-TC: (Section reserved for adoption of a major corridor plan and boundaries for this [Overlay District](#))
- (5) N. 2nd Street/US 24,40 & 59-TC: (Section reserved for adoption of a major corridor plan and boundaries for this [Overlay District](#))
- (6) 31st Street TC: (Section reserved for adoption of a major corridor plan and boundaries for this [Overlay District](#))

(c) Development Standards

- (1) SLT/K10-TC Development Standards: All [Significant Development Projects](#) within the SLT/K10 [Overlay District](#) shall be required to meet the following minimum standards:
 - (i) All new [Structures](#) and parking Lots shall be set back a minimum of 50 feet from the right-of-way line of the South Lawrence Trafficway/K10;
 - (ii) Improvements to existing [Structures](#) shall be set back a minimum of 50 feet, or the distance of the [Setback](#) of the existing [Structure](#), whichever is less; and
 - (iii) On properties lying directly adjacent to the South Lawrence Trafficway or its service roads, [Structures](#) shall be Screened from the Trafficway by continuous landscape [Screening](#) that meets the following standards:
 - a. Large [Shade Trees](#), a minimum 3-inch [Caliper](#), planted 40 feet on center;
 - b. [Ornamental Trees](#), a minimum 2-inch [Caliper](#), planted 20 feet on center;
 - c. Large [Evergreen](#) Trees, a minimum of 6 feet in [Height](#), planted 30 feet on center;
 - d. Landscape [Screening](#) may be mixed in combinations of the above requirements;
 - e. The landscape [Screening](#) shall be placed within 50 feet of the boundary of the property that lies nearest to the Trafficway; and

- f.** Required Landscaping shall be installed, maintained and replaced in accordance with Section 20-1010.
- (2)** 23rd Street/K10 Highway-TC Development Standards: All **Significant Development Projects** within the 23rd Street/K10 Transportation Corridor **Overlay District** shall be required to meet the following standards:
- (i)** Section held for **Setback** standards for new **Structures**.
 - (ii)** Section held for **Setback** standards for existing **Structures**.
 - (iii)** Section held for Landscaping standards.
- (3)** Iowa/US 59 Highway Development Standards: All **Significant Development Projects** within the Iowa/US 59 Highway **Overlay District** shall be required to meet the following standards:
- (i)** Section held for **Setback** standards for new **Structures**.
 - (ii)** Section held for **Setback** standards for existing **Structures**.
 - (iii)** Section held for Landscaping standards.
- (4)** 6th Street/US 40 Highway Development Standards: All **Significant Development Projects** within the 6th Street/US 40 Highway **Overlay District** shall be required to meet the following standards:
- (i)** Section held for **Setback** standards for new **Structures**.
 - (ii)** Section held for **Setback** standards for existing **Structures**.
 - (iii)** Section held for Landscaping standards.
- (5)** N. 2nd Street/US 24, 40 & 59 Highways: All **Significant Development Projects** within the N. 2nd Street/US 24, 40 & 59 Highways **Overlay District** shall be required to meet the following standards:
- (i)** Section held for **Setback** standards for new **Structures**.
 - (ii)** Section held for **Setback** standards for existing **Structures**.
 - (iii)** Section held for Landscaping standards.
- (6)** 31st Street TC Development Standards: All **Significant Development Projects** within the 31st Street Corridor **Overlay District** shall be required to meet the following standards:
- (i)** Section held for **Setback** standards for new **Structures**.
 - (ii)** Section held for **Setback** standards for existing **Structures**.
 - (iii)** Section held for Landscaping standards.

(d) Interpretation

- (1) The provisions of this Section shall not be interpreted to deprive the [Owner](#) of any existing property or of its use or maintenance for the purpose to which that property is then legally devoted.
- (2) In the event that a governmental taking or acquisition for right-of-way, [Easement](#) or other governmental use would reduce a [Setback](#) that previously complied with this Section, that reduction in [Setback](#) shall not be deemed to constitute a violation of this Section.

20-308 URBAN CONSERVATION [OVERLAY DISTRICT](#)

(a) Purpose

The UC, Urban Conservation [Overlay District](#), is intended to:

- (1) encourage development that conforms to the size, orientation and setting of existing [Buildings](#) in a neighborhood or area;
- (2) reduce the need for zoning variances for development that conforms to the size, orientation and setting of existing [Buildings](#) in a neighborhood or area;
- (3) provide [Building Setbacks](#), [Lot](#) dimensions and related physical characteristics;
- (4) foster development that is compatible with the [Scale](#) and physical character of original [Buildings](#) in a neighborhood or area through the use of Development/Design Standards and guidelines; and
- (5) conserve the cultural resources, historic resources and property values within an identified neighborhood or area.

(b) Selection Criteria

A UC District shall be a geographically defined area that has a significant concentration, linkage or continuity of sites that are unified by physical development, architecture or historical development patterns. To be eligible for UC zoning, the area shall comply with the following criteria:

- (1) the general pattern of development, including Streets, Lots and [Buildings](#), shall have been established at least 25 years prior to the [Effective Date](#);
- (2) the area shall possess built environmental characteristics that create an identifiable setting, character and association;
- (3) the designated area shall be a contiguous area of at least five (5) acres in size. Areas of less than five (5) acres may be designated as an UC [Overlay District](#) only when they abut an existing five (5) acre or greater UC [Overlay District](#).

(c) Establishment of District

UC [Zoning Districts](#) are established in accordance with the Zoning Map Amendment procedures of Section 20-1303, except as modified by the following provisions:

- (1) an application to establish a UC District may be initiated by the [Historic Resources Commission](#), the [Planning Commission](#) or the City Commission;
- (2) applications may also be initiated by petition when signed either by the [Owner](#) of at least 51% of the area within the proposed UC District or by at least 51% of total number of [Landowners](#) within the proposed District;
- (3) the [Historic Resources Commission](#) and the [Planning Commission](#) shall hold public hearings, and submit written recommendations to the City Commission, regarding each application to establish a UC District;
- (4) the [Historic Resources Commission](#) is responsible for reviewing UC zoning applications for compliance with the selection criteria of Section 20-308(b) and for recommending development/design standards and guidelines for the District;
- (5) the [Planning Commission](#) is responsible for reviewing UC applications for its planning and zoning implications; and
- (6) the City Commission is responsible for making a final decision to approve or deny the [Overlay District Zoning](#).

(d) Procedure

Upon receipt of an application for UC zoning or upon initiation of a UC zoning application by the City Commission, [Planning Commission](#) or [Historic Resources Commission](#), the following procedures apply:

- (1) unless otherwise expressly stated, the zoning map amendment procedures of Section 20-1303 apply;
- (2) public hearings on UC zoning applications shall be held by the [Historic Resources Commission](#) and the [Planning Commission](#) prior to consideration by the City Commission; and
- (3) the [Historic Resources Commission](#) shall make a recommendation that UC District zoning be approved, approved with conditions or denied. The [Historic Resources Commission's](#) recommendation shall be submitted to the [Planning Commission](#) and City Commission. The item shall be placed on the [Planning Commission](#) agenda after receipt of the [Historic Resources Commission's](#) recommendation. The recommendation shall be accompanied by a report containing the following information:
 - (i) an explanation of how the area meets or does not meet the selection criteria contained in Section 20-308(b);
 - (ii) in the case of an area found to meet the criteria in Section 20-308(b):
 - a. a description of the general pattern of development, including Streets, Lots and Buildings in the area; and
 - b. Development/Design Standards to guide development within the District;

- (iii) a map showing the recommended boundaries of the UC District;
and
- (iv) a record of the proceedings before the [Historic Resources Commission](#);

(e) Allowed Uses

UC District Classifications do not affect the use of land, [Buildings](#) or [Structures](#). The use regulations of the [Base District](#) control.

(f) Development/Design Standards

In establishing a UC District, the [Historic Resources Commission](#) or [Planning Commission](#) are authorized to propose, and the City Commission is authorized to adopt, by ordinance, District-Specific Development and Design Standards (referred to herein as “Development/Design Standards”) to guide development and redevelopment within UC Districts:

- (1) when Development/Design Standards have been adopted, all Alterations within the designated UC District shall comply with those standards. For the purposes of this section, “Alteration” means any [Development Activity](#) that changes one or more of the “Exterior Architectural Features” of a [Structure](#), as the latter term is defined in Chapter 22 of the City Code;
- (2) when there are conflicts between the Development/Design Standards of the [Base District](#) and adopted UC District Development/Design standards, the UC Development/Design Standards will govern;
- (3) the Development/Design Standards will be administered by City staff in accordance with adopted administrative policy.

(g) Appeals

- (1) Notwithstanding the procedure set forth in Section 20-1311, a person aggrieved by a decision of the City staff, determining whether the Development/Design Standards have been met, may file a written appeal with the [Historic Resources Commission](#). The appeal shall be filed within ten (10) [Working Days](#) after the decision has been rendered.
- (2) A person aggrieved by a decision of the [Historic Resources Commission](#), determining whether the Development/Design Standards have been met, may file a written appeal with the City Commission. The appeal shall be filed within ten (10) [Working Days](#) after the decision has been rendered.
- (3) the City Commission is the final decision-making authority in determining whether a proposed project meets the adopted Development/Design Standards.
- (4) the Board of Zoning Appeals has no authority to grant interpretations, exceptions or variances from the adopted Development/Design Standards.
- (5) within thirty days after the City Commission’s final decision, in passing upon an appeal pursuant to this Section, any person aggrieved by the decision may file an action in District Court to determine the reasonableness of the decision.

(h) UC Districts Established

The following UC Districts are established:

Conservation District Name	Boundaries
Downtown Urban Conservation Overlay District	See Ord. No. 7395
8 th & Pennsylvania Urban Conservation Overlay District	See Ord. No. 8053
Oread Neighborhood Design Overlay District	See Ord. No. <TBD>

(i) UC District Development/Design Standards Established

The following UC District Development/Design Standards and Administrative Policies are established:

Conservation District Name	Development Standards and Administrative Policies
Downtown Urban Conservation Overlay District	Downtown Design Guidelines 2009
8 th and Pennsylvania Urban Conservation Overlay District	Design Guidelines 8 th and Penn Neighborhood Redevelopment Zone (2006, Revised October 4, 2011)
Oread Neighborhood Design Overlay District	Oread Neighborhood Design Guidelines (March 2016)

20-309 INCORPORATION BY REFERENCE OF “THE DOWNTOWN DESIGN GUIDELINES 2009 EDITION”

The “Downtown Design Guidelines December 16, 2008 Edition” prepared compiled, published and promulgated by the City of Lawrence, Kansas is hereby adopted and incorporated by reference as if fully set forth herein, and shall be known as the “Downtown Design Guidelines 2009 Edition”. At least one copy of said text amendments shall be marked or stamped as “Official Copy as Adopted by Ordinance No. 8363 and to which shall be attached a copy of this ordinance, and filed with the City Clerk, to be open to inspection and available to the public at all reasonable business hours. The police department, municipal judge, and all administrative departments of the City charged with the enforcement of the ordinance shall be supplied, at the cost of the city, such number of official copies of such “Downtown Design Guidelines, 2009 Edition” marked as may be deemed expedient.

20-310 INCORPORATION BY REFERENCE OF “DESIGN GUIDELINES 8TH AND PENN NEIGHBORHOOD REDEVELOPMENT ZONE” (2006, REVISED OCTOBER 4, 2011)

The “Design Guidelines 8th and Penn Neighborhood Redevelopment Zone” (2006, Revised October 4, 2011) prepared compiled, published and promulgated by the City of Lawrence, Kansas is hereby adopted and incorporated by reference as if fully set forth herein. At least one copy of “Design Guidelines 8th and Penn Neighborhood Redevelopment Zone” (2006, Revised October 4, 2011) shall be marked or stamped as “Official Copy as Adopted by Ordinance No. 8675” and to which shall be attached a copy of Ordinance No. 8675, and filed with the City Clerk,

to be open to inspection and available to the public at all reasonable business hours. One additional marked and stamped copy of the "Design Guidelines 8th and Penn Neighborhood Redevelopment Zone" (2006, Revised October 4, 2011) shall, at the cost of the City of Lawrence, Kansas, be made available to the Lawrence-Douglas County Metropolitan Planning Office of the City of Lawrence, Kansas.

20-311 INCORPORATION BY REFERENCE OF "OREAD NEIGHBORHOOD DESIGN GUIDELINES 2016 EDITION"

The "Oread Neighborhood Design Guideline, 2016 Edition" prepared compiled, published and promulgated by the City of Lawrence, Kansas is hereby adopted and incorporated by reference as if fully set forth herein, and shall be known as the "Oread Neighborhood Design Guideline, 2016 Edition". At least one copy of said text amendments shall be marked or stamped as "Official Copy as Adopted by Ordinance No. <TBD> and to which shall be attached a copy of this ordinance, and filed with the City Clerk, to be open to inspection and available to the public at all reasonable business hours. One additional marked and stamped copy of the "Oread Neighborhood Design Guidelines" (2016 Edition) shall, at the cost of the City of Lawrence, Kansas, be made available to the Lawrence-Douglas County Metropolitan Planning Office of the City of Lawrence, Kansas.

PLANNING COMMISSION REPORT
Regular Agenda – Public Hearing Item

PC Staff Report
3/21/16

ITEM NO. 8A-8F REZONING TO APPLY THE URBAN CONSERVATION OVERLAY DISTRICT (-UC) TO 190.8 ACRES WITHIN THE OREAD NEIGHBORHOOD; OREAD NEIGHBORHOOD DESIGN GUIDELINES (JSC)

Z-12-00172: Oread Design Guidelines District 1 (Low Density), 38.1 Acres
RM12, RM12D, RM32, U-KU to RM12-UC, RM12D-UC, RM32-UC, U-KU-UC

Z-12-00175: Oread Design Guidelines District 2 (High Density), 43.7 Acres
MU, MU-PD, PCD, RM32, RM32-PD, RMG, RMO, U-KU to MU-UC, MU-PD-UC, PCD UC, RM32-UC, RM32-PD-UC, RMG-UC, RMO-UC, U-KU-UC

Z-12-00177: Oread Design Guidelines District 3 (Medium Density), 63.5 Acres
CS, RM32, RMO to CS-UC, RM32-UC, RMO-UC

Z-12-00173: Oread Design Guidelines District 4 (Hancock Historic District), 4.8 Acres
RM32 to RM32-UC

Z-12-00174: Oread Design Guidelines District 5 (Oread Historic District), 28.9 Acres
CS, RM32, RMO, RSO to CS-UC, RM32-UC, RMO-UC, RSO-UC

Z-16-00058: Oread Design Guidelines District 6 (Commercial), 11.9 Acres
CN2, CS, RM32, RMO to CN2-UC, CS-UC, RM32-UC, RMO-UC

STAFF RECOMMENDATION: Staff recommends approval of rezoning of 190.8 acres to apply the –UC (Urban Conservation Overlay District), and forwarding these items to the City Commission with a recommendation for approval based on the findings of fact found in this staff report.

KEY POINTS

- The *Oread Neighborhood Plan* was adopted by the City Commission in 2010.
- The *Oread Neighborhood Plan* is incorporated into *Horizon 2020*, Chapter 14: Specific Plans.
- These rezoning requests were initiated by the City Commission on August 28th, 2012.
- The implementation of the Urban Conservation Overlay District would implement the *Oread Neighborhood Design Guidelines*, which was a recommendation of the *Oread Neighborhood Plan*.
- These applications do not change the base zoning districts; they only apply the –UC overlay to the existing districts.

ASSOCIATED CASES/OTHER ACTION REQUIRED

TA-12-00171: Consider a Text Amendment to the Land Development Code, Chapter 20 of the Code of The City of Lawrence, Kansas to include the *Oread Neighborhood Design Guidelines*. Initiated by City Commission on 8/28/2012.

OTHER ACTION REQUIRED:

- City Commission approval of rezonings and adoption of ordinances.
- Publication of rezoning ordinance.

PLANS AND STUDIES REQUIRED

- | | |
|---|----------------------------|
| • <i>Traffic Study</i> | Not required for rezoning. |
| • <i>Downstream Sanitary Sewer Analysis</i> | Not required for rezoning. |
| • <i>Drainage Study</i> | Not required for rezoning. |
| • <i>Retail Market Study</i> | Not required for rezoning. |

PUBLIC COMMENT

General inquiries from the public regarding scope of the proposed rezoning requests. Staff has clarified for callers that the current requests are for the accompanying text amendment and the rezoning cases. Please see attached list.

GENERAL INFORMATION:**Project Summary**

The establishment of the *Oread Neighborhood Design Guidelines* is an action step of the *Oread Neighborhood Plan*, which was adopted by the City Commission in September 2010. In the spring of 2011, Planning staff received a Historic Preservation Fund grant to draft design guidelines for the overlay districts identified in the plan. The grant was accepted by the City Commission on June 7th, 2011. Staff subsequently issued a request for proposals for a consultant, which resulted in the selection of Urban Development Services with Ann Benson McGlone to help begin developing the guidelines.

Staff began working with the consultant in January 2012. The first visit by the consultants occurred in February 2012 where they met with staff, design professionals, The Oread Residents Association representatives, and Oread Neighborhood Association representatives.

The first round of public meetings occurred on April 13th and April 14th, 2012. Over 400 letters were mailed to property owners, an email was sent to the *Oread Neighborhood Plan* email listserv, newspaper ads were published advertising the meetings, and information was posted on the City of Lawrence website. The April 13th meeting was held at the Lawrence Public Library, where approximately 30 people attended, at which the consultants were introduced, the general concepts of design guidelines were discussed and a community input activity was completed. This activity was used to identify preferences and what items/issues were important in the neighborhood. The information from this activity was later used as the goals

of the guidelines. The April 14th workshop at the Carnegie Building was held for attendees to help identify archetypal development patterns within the neighborhood.

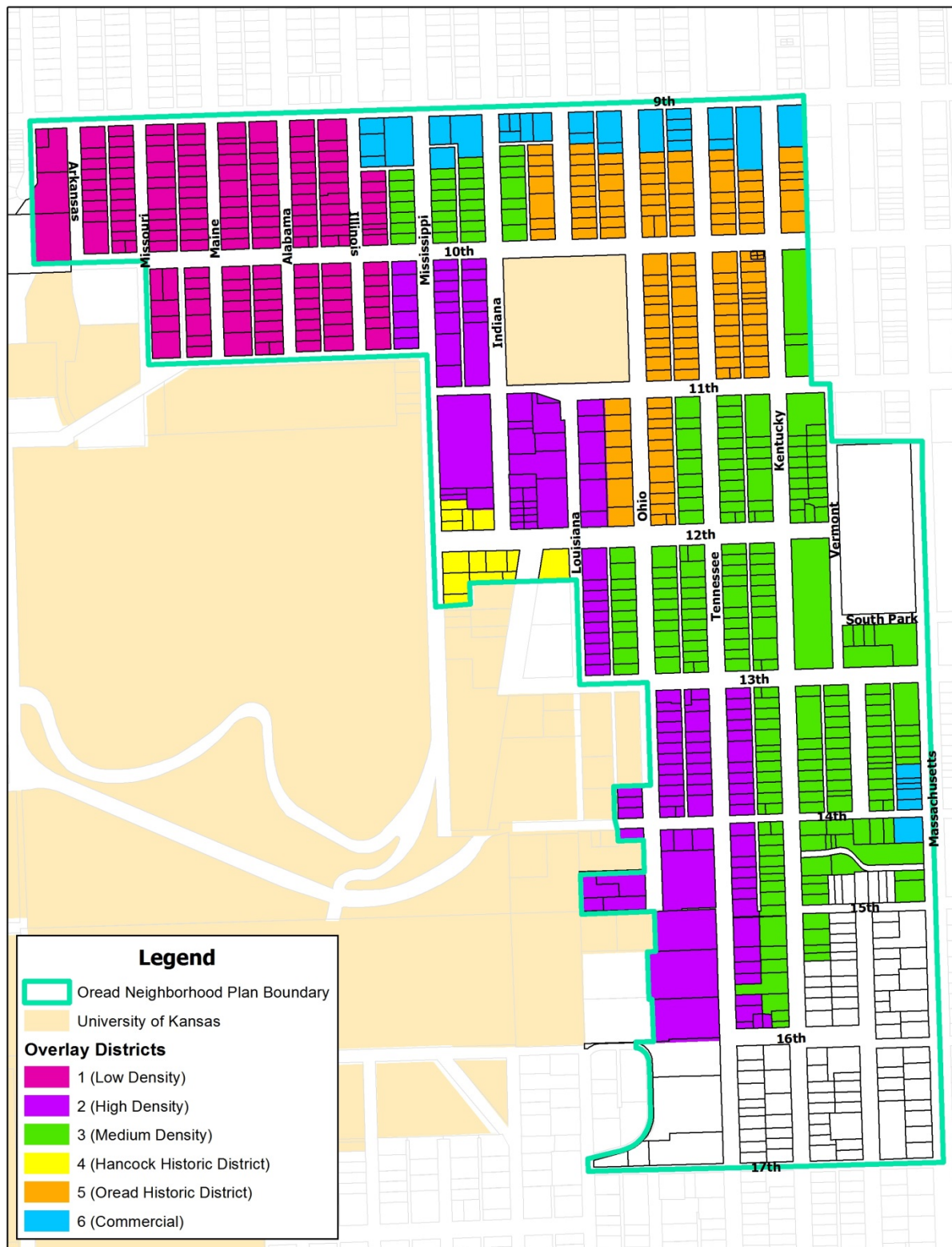


Figure 1: Proposed Oread Neighborhood Design Guideline Districts

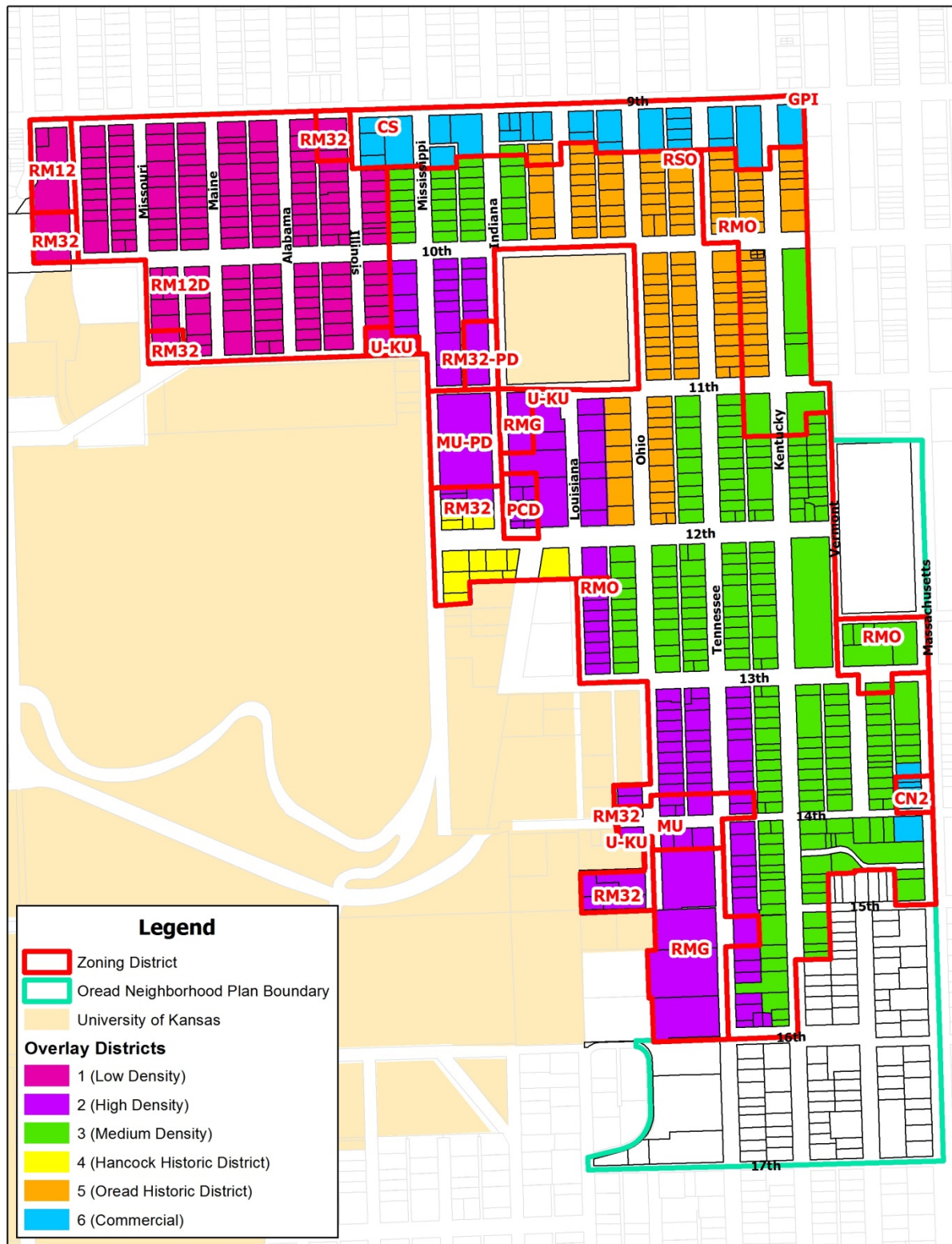


Figure 2: Proposed Oread Neighborhood Design Guideline Districts with Current Zoning

The next public meeting was held on May 3rd, 2012 at the Lawrence Public Library, at which approximately 20 people were in attendance. The consultants provided a summary and the findings of the April meetings, and showed examples to demonstrate what type of density and development the attendees would find acceptable in the neighborhood.

The fourth public meeting was held on August 2nd, 2012 at the Lawrence Public Library where approximately 15 people attended. The consultants introduced details of the draft design guidelines, though the actual document was not provided.

In January 2013, a joint Planning Commission/Historic Resources Commission subcommittee was established to review the draft of the *Oread Neighborhood Design Guidelines*, in order to prepare them for public consumption and review. Each commission appointed two members to this subcommittee. The Subcommittee's review process was completed in November 2015, after which a draft was made available for the public in December 2015. On January 13th, 2016 a public meeting was held at the Lawrence Public Library, at which approximately 25 people attended, to present the guidelines and receive feedback.

The *Oread Neighborhood Design Guidelines* are built on a strong history of community planning. Beginning in 1979, the neighborhood created the original *Oread Neighborhood Plan*. In 1998, the neighborhood participated in the citywide *Horizon 2020: The Comprehensive Plan for Lawrence and Unincorporated Douglas County*. The Hancock Historic District was listed in the National Register of Historic Places in 2004, and in 2007, the Oread Historic District was placed on the National Register of Historic Places. An updated *Oread Neighborhood Plan* was adopted in 2010.

These design guidelines are based on the goals, policies, and implementation strategies outlined in the 2010 *Oread Neighborhood Plan*. Listed on the following page are those policies and strategies that are specific to the overlay district design guidelines.

REVIEW & DECISION-MAKING CRITERIA

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

The initial *Oread Neighborhood Plan* was adopted by the City Commission in 1979, and the revised neighborhood plan was adopted in 2010. The revised neighborhood plan was incorporated into *Horizon 2020* Chapter 14: Specific Plans (CPA-9-21-09 / Ordinance [8496](#)) on September 28th, 2010.

Only the implementation strategies of the *Oread Neighborhood Plan* were the consideration of creating an overlay district. These rezonings, and the associated Text Amendment, begin to attain one of the key implementation strategies for this neighborhood plan.

Staff Finding – The request to rezone the properties within the identified overlay districts with the –UC (Urban Conservation Overlay) District conforms to Chapter 14: Specific Plan and the *Oread Neighborhood Plan*.

2. ZONING AND LAND USES OF NEARBY PROPERTY, INCLUDING OVERLAY ZONING

Current Zoning and Land Use: CN2 (Neighborhood Commercial District); Mass. St. Music, Footprints Shoe Store.

CS (Commercial Strip District); various retail and commercial uses along W. 9th Street.

GPI (General Public and Institutional Use District); Lawrence Carnegie Building and Lawrence Community Building.

MU (Mixed Use District); Bullwinkles and residential uses.

MU-PD (Mixed Use District – Planned Development Overlay); HERE Kansas Student Housing.

PCD (Planned Commercial District); Oread Hotel.

RM12 (Multi-Dwelling Residential District, 12 Dwelling Units per Acre); residential uses.

RM12D (Multi-Dwelling Residential District, 12 Dwelling Units per Acre - Duplex); residential uses.

RM32 (Multi-Dwelling Residential District, 32 Dwelling Units per Acre); residential uses.

RM32-PD (Multi-Dwelling Residential District, 32 Dwelling Units per Acre – Planned Development Overlay); residential uses.

RMG (Multi-Dwelling Residential – Greek Housing District); Alpha Epsilon Pi, Alpha Sigma Chi, Beta Theta Pi, Kappa Alpha Theta, Pi Kappa Phi, and Alpha Gamma Delta.

RMO (Multi-Dwelling Residential – Office District); residential and office uses.

RSO (Single-Dwelling Residential – Office District); residential and office uses.

U-KU (University District – KU); vacant parcel.

Surrounding Zoning and Land Use: To the north: RS5 (Single-Dwelling Residential – 5,000 square feet) District and CS (Commercial Strip) District. Existing uses include various retail establishments, and existing residential dwellings.

To the west: U-KU (University District - KU); The

University of Kansas Main Campus, Gertrude Sellards Pearson and Corbin Residence Halls.

To the east: CD (Downtown Commercial) District, GPI (General Public and Institutional Use) District, OS (Open Space) District, CN2 (Neighborhood Commercial) District, RS5 (Single-Dwelling Residential – 5,000 square feet) District, and RS7 (Single-Dwelling Residential – 7,000 square feet) District. Existing uses include, Downtown Lawrence, South Park, Community Building, and existing residential dwellings

To the south: RS5 (Single-Dwelling Residential – 5,000 square feet) District, and RM32 (Multi-Dwelling Residential – 32 Dwelling Units) District. Existing residential dwellings.

Staff Finding: The zoning surrounding the proposed Urban Conservation Overlay District is compatible with the zoning within the proposed district. The rezoning cases would not alter the existing base zoning presently established. Adoption of the overlay district would be compatible with the surrounding zoning.

3. CHARACTER OF THE AREA

Neighborhood-Wide

Historically, the Oread neighborhood has contained a mixture of uses with a dominant residential character via a variety of structure types. The history of the neighborhood traces back to just after the American Civil War as Lawrence was beginning to grow around the 1870s. Initially, the neighborhood appealed to upper-middle class residents, where they looked for space to construct the larger homes that still make up a significant portion of the neighborhood today.

In addition to its proximity to Downtown Lawrence, the Oread Neighborhood's proximity to the University of Kansas helped drive the initial growth and development of the neighborhood. The proximity of KU clearly influenced the development patterns of the neighborhood. Many of the neighborhood residents between 1874 and 1899 were university students. The first campus dormitory was not built until 1923, so several homes in the neighborhood were operated as congregate living. Families with university age students also moved into the neighborhood so their children could attend KU and live in close proximity.

As the University of Kansas began to take on greater importance in the local economy after 1900, the town's growth was slow while University enrollment increased dramatically. This led to a number of commercial properties to develop around KU's Main Campus. This created a concern among the nearby property owners, as well as city officials, resulting in the first Lawrence Planning Commission and zoning ordinance to be created in June 1926. The 1926 Zoning Ordinance allowed for apartments, congregate living, and fraternity and sorority houses within the Oread Neighborhood. As a result, a large number of students lived in the area immediately east and northeast of the University. Therefore, the Oread Neighborhood was increasingly been associated with student housing since the turn of the 20th Century, with many

structures being converted to private congregate living uses. In addition to students, the neighborhood remained popular for University faculty as well. After the turn of the 20th Century, important Lawrence business leaders also moved into the Oread neighborhood and constructed new residences, while the neighborhood continued to attract middle class families.

The Oread Neighborhood was still attractive for families during the early 20th Century. In addition to electricity, water and sewer, the streets and even some alleys were paved, curbing was introduced, and stone or brick sidewalks were installed. During the 1930s, and again in the post-war boom of the 1940s & 1950s, many of the larger homes were converted to apartments, cooperatives, or Greek houses. However, by the 1970s, new residents with an interest in rehabilitating historic homes began moving into the neighborhood. The growing appreciation for historic buildings resulted in several residences being rehabilitated and returned to single-family use. The new residents of the Oread Neighborhood worked to establish Lawrence's preservation ordinance, and nominated the east side of the 1000 block of Ohio Street and the west side of the 1000 block of Tennessee Street as the first local historic district in 1991.

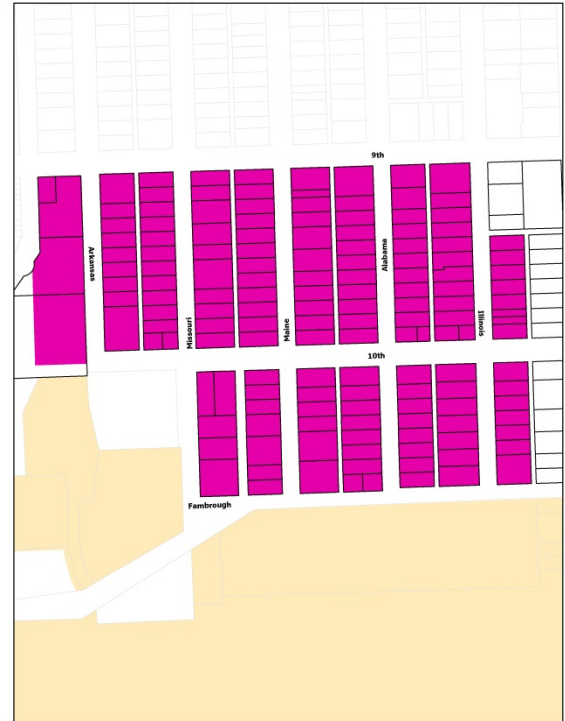
Over the course of time, the Oread Neighborhood developed in a distinct and unique way that is visible in the five districts that were identified in the 2010 *Oread Neighborhood Plan*. Each of the districts is unique in its architecture, as well as in the forces they experience in today's environment. In the *Oread Neighborhood Design Guidelines*, these districts are defined by their key distinctive aspects. Through the guidelines development process, a sixth district containing commercial properties at the edges of the neighborhoods were also identified.

The following is a brief synopsis of each of the six proposed districts to provide more insights into their exact foundations and specific histories.

Z-12-00172: District 1

Located just north of the University of Kansas, this district is predominately comprised of single-family residences with intermittent duplexes, triplexes, and quadraplexes. This district is less dense (6 or less dwelling units per acre) than most parts of Oread neighborhood, with smaller scale homes, larger yards, and houses spaced farther apart. The many one story homes in this area provide it with its unique character.

The district is composed of a wide variety of architectural styles constructed over a long period. Within this district you will find Victorian Queen Annes, Craftsman Bungalows, and 20th Century Revival Tudors. This was the last area of the neighborhood to develop; therefore, Post-War Minimal Traditional houses are unique to this area. More recent apartments exist on the fringes, mainly on the western side of the district.



Z-12-00177: District 3

District 3 is located primarily on the eastern edge of the Oread Neighborhood, and serves as the basis for the design guidelines for all 5 residential districts. The other district guidelines within Chapter 5 of the Guidelines are modifications to this standard tailored to their own distinctive characteristics.

Most off-street parking is accessed from alleys, so driveways are rare. The homes sit back from the street with simple, elegant façades. Wide, shaded porches add to the character. The original single-family homes, constructed between the 1880s and the 1900s, are generally two stories and represent a wide array of styles and sizes. Many have been converted to multi-dwelling uses. Others have been replaced with modern apartments. The density is approximately 7 to 15 dwelling units per acre.

Older, commercial buildings and more recent professional buildings can be found towards Massachusetts Street.

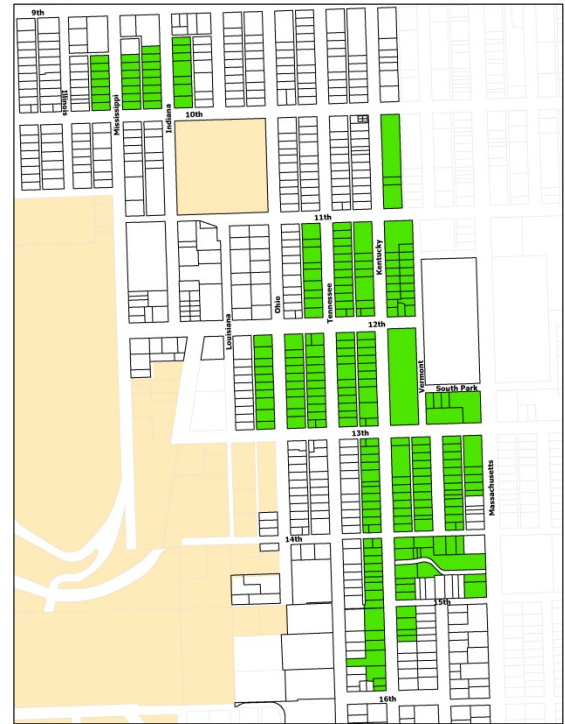


Figure 5: District 3 (Medium Density)

Z-12-00173: District 4

Hancock Historic District is a unique and charming area of the Oread Neighborhood located on a substantial rise that overlooks the University of Kansas. The Hancock Historic District was added to the National Register of Historic Places in 2004.

This small district boasts a beautiful and historic public improvement project that adds to the allure of the area: a pair of curved stone steps ascends from Mississippi Street to W. 12th Street in the grand fashion of the City Beautiful Movement. At the top of the stairs on W. 12th Street is a planter of native stone that forms a charming esplanade in the center of the street until it intersects with Indiana Street.

The homes in this district are mature and stately, perched on top of the hill overlooking the campus. Most of the lots are larger than those found in the rest of neighborhood, creating a more landscaped setting for most of the homes. The variety of styles, mostly 20th Century revivals, adds to the overall character in this district.

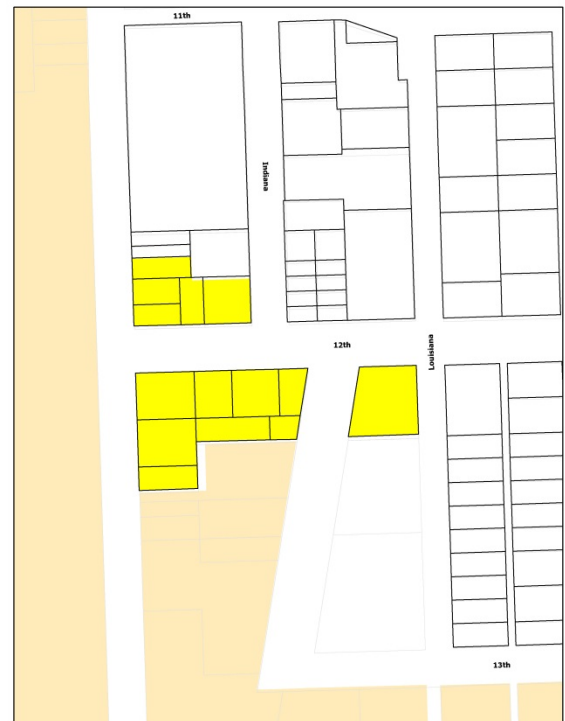


Figure 6: District 4 (Hancock Historic)

Z-12-00174: District 5

The Oread Historic District was listed in the National Register of Historic Places in 2007. The Oread Historic District (1000 block, east side Ohio Street, and west side Tennessee Street) was listed to the Lawrence Register of Historic Places in 1991. It is a charming and distinguished area with some of the oldest homes in Lawrence. There are excellent examples of the Italianate, Queen Anne, Craftsman, and 20th Century Revival styles side by side with the more vernacular National Folk and Foursquare homes. They range in size from grand to simple.

The streets are lined with tall, deciduous trees. On the west side of the streets, the houses are set back with elevated front yards. Dominant porches and shade-dappled yards add to the appeal of this area. On the east side of the streets, the homes are placed closer to the street with shallow front yards that are relatively flat. Parking is off the alleys; therefore, driveways are rare. Remnants of the historic past remain. Some streets are still paved with the original brick, and hitching posts and stone steps can still be found.

**Figure 7: District 5 (Oread Historic)****Z-16-00058: District 6**

The guidelines defined commercial district mainly fronts along W. 9th Street and on the western half of the intersection of 14th Street and Massachusetts Street.

The goal of commercial and mixed-use design guidelines is to create a pleasing and attractive urban setting where businesses can succeed, while maintaining the existing character of the residential neighborhood. The commercial uses should be scaled to serve the adjacent neighborhoods, providing business opportunities for convenience that would be primarily accessed on foot.

These guidelines are intended to be used only if there is commercial use on the ground floor. If the first floor is not commercial then the pertinent residential district guidelines would be applicable.

**Figure 8: District 6 (Commercial)**

Staff Finding: The Oread Neighborhood was principally developed between the 1870s and the 1950s, with renovation and new development throughout the history of the neighborhood. Contextually in the larger surrounding area, the Oread Neighborhood is established and consistent with other neighborhoods and areas adjacent to the proposed overlay areas.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

The *Oread Neighborhood Plan* recommended the implementation of five overlay districts. During the course of drafting and revising the *Oread Neighborhood Design Guidelines*, it was recognized that a sixth mapped district for commercial uses should also be included. From the beginning the commercial component of the guidelines has been present; however, it was not specifically identified on the map. As the revision process began, the Subcommittee felt that mapping this district to the existing commercial areas was important to show the existing conditions where this chapter would be applicable.

The envisioned overlay districts were to, “provide a greater latitude to certain areas (generally most closely adjacent to KU) to develop more densely by allowing increased building heights, etc., give proper guidance to infill development. The district(s) could address such issues as the mass, scale and bulk of the development as well as impervious and pervious coverage, establish standards to regulate bulk and mass of structures, maintain open space on individually platted lots, and regulate parking.” All of these items were direct Goals, Policies and Implementation Strategies that are contained within of the adopted neighborhood plan, which is incorporated in to Chapter 14: Specific Plans of *Horizon 2020*.

Staff Finding: The proposed urban conservation overlay district rezonings conforms to proposed land use recommendations in the *Oread Neighborhood Plan* and *Horizon 2020*.

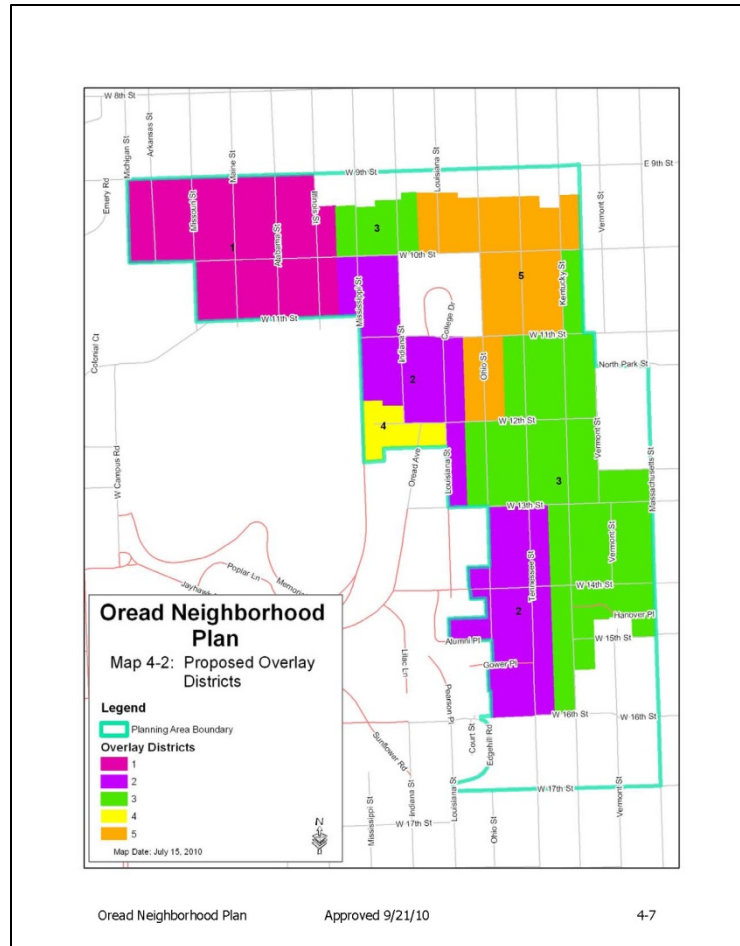


Figure 9: Proposed Overlay Districts from the Adopted Oread Neighborhood Plan

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

The rezoning requests for these areas will not change the underlying base zoning district. The proposed rezonings would apply an Urban Conservation Overlay District to implement the guidelines as prescribed in both the *Oread Neighborhood Plan* and the *Oread Neighborhood Design Guidelines*.

Staff Finding: The properties are suitable for the Urban Conservation Overlay District zoning, and are consistent with land use recommendations relating to changes to *Horizon 2020* and the *Oread Neighborhood Plan*.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

The area was principally developed between the 1870s and the 1950s, with renovation and new development throughout the history of the neighborhood. The Guidelines identify the architectural Period of Significance for this area to be between 1877 and 1945, with a stipulation that for Districts 2 through 5, buildings before 1929 be specifically used to determine qualities and context when necessary for consideration. Contextually in the larger surrounding area, the Oread Neighborhood is established and consistent with other neighborhoods and areas adjacent to the proposed overlay area.

Staff Finding: Use of the properties within the proposed overlay districts has been consistent since the initial neighborhood development timeframe.

7. EXTENT TO WHICH REMOVAL OF RESTRICTIONS WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Approval of this request would enact the general guidelines for all districts as prescribed in Chapter 4, and area-specific guidance supplemented by Chapter 5 in relation to parking, the building envelope, and other density and dimensional standards. The *Oread Neighborhood Design Guidelines* provides more precise and location-specific standards than the broad city-wide scope provided by the Land Development Code. These items have been reviewed and considered against each district's specific architecture, site typology, history, and concurrent existing conditions. Chapter 4 of the Guidelines is the general set for Districts 1 through 5; while the more tailored District-specific considerations are located in Chapter 5. Chapter 6 guidelines would apply to the mapped commercial properties, and where commercial storefronts are located on the ground floors. The application of the guidelines would be limited to their applicable areas as defined within the Guidelines.

Staff Finding: Impacts on nearby properties would be minimal. The intent of the *Oread Neighborhood Design Guidelines* is to provide more precise guidance to these specific properties than what is currently contained within the Land Development Code.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Evaluation of these criteria includes weighing the benefits to the public versus the benefits for the owners of the subject properties. Benefits are measured based on the anticipated impacts of the rezoning request on the public health, safety, and welfare.

If the rezoning requests were denied, the properties would retain their various existing base zoning designations. If the rezonings were approved, the change would apply the *Oread Neighborhood Design Guidelines* to the properties through the designation of the –UC (Urban Conservation Overlay) District. This overlay would then apply both the architectural/historic design guidelines, and also site and building envelope modifications to the Land Development Code as prescribed within the *Oread Neighborhood Design Guidelines*.

Approval of the requests would facilitate more compatible development within the designated neighborhood, and also provide for stronger development in a unique sense of place for the Oread Neighborhood.

Staff Finding: Benefits to the community include providing a more tailored set of land use controls to address and implement the site, historic, and architectural considerations that were identified in the adopted *Oread Neighborhood Plan* and the public input gathered during the various public meetings held regarding this proposal.

PROFESSIONAL STAFF RECOMMENDATION

The focus of this report is in regards to the six rezoning cases, one for each district defined within the guidelines, which comprise the implementation step of the *Oread Neighborhood Design Guidelines*. While the six cases are unique in their geographies, they are identical in their overall intent and goals. The six rezoning cases each implement one of the districts within the guidelines; they are linked together in the consideration and drafting of the *Oread Neighborhood Design Guidelines*. Along with the concurrent Text Amendment, these items would integrate the *Oread Neighborhood Design Guidelines* into the Land Development Code.

Staff recommends approval of the rezoning requests for approximately 190.8 acres to apply – UC (Urban Conservation Overlay) District to implement the *Oread Neighborhood Design Guidelines*, and forwarding these items to the City Commission with a recommendation for approval based on the findings of fact in the body of this staff report subject.

**Report to
Lawrence Douglas County Planning Commission
and
Lawrence City Commission
Urban Conservation Overlay Districts
Oread Neighborhood**

July 20, 2016

A. SUMMARY

The Lawrence Historic Resources Commission (HRC), at their meeting on June 16, 2016, reviewed the Urban Conservation Overlay Districts and the development/design standards associated with the districts proposed for the Oread Neighborhood. The HRC voted unanimously to recommend the overlay districts and design guidelines as were presented to the HRC at that public hearing.

(http://lawrenceks.org/assets/pds/planning/documents/Oread_Neighborhood_Design_Guidelines_2016_June.pdf)

The HRC respectfully submits this report for review and consideration as required by Chapter 20-308(d)(3) of the Land Development Code.

B. HRC RECOMMENDATION

The Lawrence Historic Resources Commission recommends approval of the proposed UC District zoning for the Oread Neighborhood Urban Conservation Overlay Districts and the associated development/design standards identified as the Oread Neighborhood Design Guidelines (June 6, 2016 edition).

C. CHAPTER 20 REPORT REQUIREMENTS (20-308(d)(3))

(1) *Explanation of how the area meets or does not meet the selection criteria contained in Section 20-308(b);*

(A) *the general pattern of development, including Streets, Lots and Buildings, shall have been established at least 25 years prior to the Effective Date;*

This portion of the Oread Neighborhood was platted before 1884 with the only exceptions new plats for modern development. The plats created the street and lot patterns that are the dominant patterns of the existing area. Buildings in the area date from the late 1860s to present day modern infill structures.

(B) *the area shall possess built environmental characteristics that create an identifiable setting, character and association;*

Portions of the area have been designated as National Register of Historic Places districts. While some areas have qualities that are unique to that district, the entire area shares an identifiable setting, association, and character that are described in the proposed development/design standards, the Oread Neighborhood Design Guidelines.

(C) the designated area shall be a contiguous area of at least five (5) acres in size.

The total area incorporated by the proposed overlay is 190.8 acres.

(2) In the case of an area found to meet the criteria in Section 20-308(b):

(A) description of the general pattern of development, including Streets, Lots and Buildings in the area; and

The HRC reviewed the information included in the proposed development/design standards, the Oread Neighborhood Design Guidelines, on pages 27-51 and concurs with this description.

(B) Development/Design Standards to guide development within the District;

Oread Neighborhood Design Guidelines, June 6, 2016 edition.

(3) a map showing the recommended boundaries of the UC District;

A map showing the recommended boundaries of the UC District is located on page 11 in the proposed development/design standards, the Oread Neighborhood Design Guidelines.

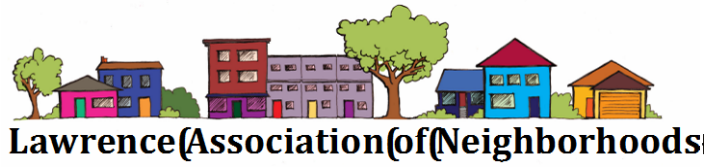
(4) a record of the proceedings before the Historic Resources Commission;

The record of the public hearing before the Historic Resources Commission is located here

<http://lawrenceks.org/boards/historic-resources-commission/agendas-and-minutes>

ATTACHMENTS

1. Oread Neighborhood Design Guidelines
http://lawrenceks.org/assets/pds/planning/documents/Oread_Neighborhood_Design_Guidelines_2016_June.pdf
2. Historic Resources Commission Action Summary June 16, 2016
<http://lawrenceks.org/boards/historic-resources-commission/agendas-and-minutes>



Dear Planning Commission,

6-13-16

The Lawrence Association of Neighborhoods (LAN) continues to enthusiastically support the proposed Design Guidelines and Overlay District for the Oread Neighborhood. This document provides important tools that will help preserve the historic integrity of Oread and enhance the livability of the area for both renters and homeowners. This is an exceptionally well-crafted document.

These guidelines reflect the intention of the **Oread Neighborhood Plan** that specifies areas of varying density as well as addressing the unique characteristics of old town-site plans, which differs from newer city neighborhoods. Sensitive and thoughtful planning tools are important to address these differences and respect the historic and unique nature of the Oread Neighborhood.

Oread zoning was unwisely changed over 40 years ago from low density to the highest density in the city. This has created on-going problems and conflicts among property owners due to the single-family nature of the neighborhood (houses make up 80% of the neighborhood) and high density zoning that does not conform to existing structures or lot sizes.

Density should increase from single family, duplex, triplex, fourplex, and higher multi-dwelling units. No more than 4 unrelated individuals may live in a single dwelling unit (most often a house) in multi-family zoned areas. Rental units require parking standards. **It is essential to provide adequate parking in all rental units in the Oread Neighborhood.** All rentals units should have the same parking standard (one parking space per BR), including duplexes. Duplexes presently provide 50% less parking than other rentals due to the allowance of stacked parking. Stacked parking is not accessible parking. Oread streets should not become duplex parking lots.

The Planning Department has put together an impressive document that will be of great benefit to the Oread Neighborhood and the City. The Design Guidelines and the Overlay District will help promote increased stability and vitality in historic Oread by encouraging responsible development and redevelopment. Ideas of new urbanism will be apparent when both renters and homeowners live in a well-planned environment that considers the design and livability of the area.

Sincerely,
Candice Davis, LAN Chair and Oread Resident

Lynann L. Chance and John R. Haase
1945 East 845 Road
Lecompton, Kansas 66050
(785) 887-6501

RECEIVED

JUN 14 2016

City County Planning Office
Lawrence, Kansas

June 8, 2016

Lawrence – Douglas County Metropolitan Planning Commission
Lawrence Historic Resources Commission
6 East Sixth Street
P.O. Box 708
Lawrence, Kansas 66044

RE: Proposed Rezoning of 303 West Eleventh Street, Lawrence, Kansas

Dear Commission Members:

While we applaud your objective to stabilize the Oread Neighborhood, the impact on our property appears to be unnecessary, arbitrary, and disproportionate with respect to the recommendation for other commercial properties. **We respectfully request the property maintain its current commercial zoning.**

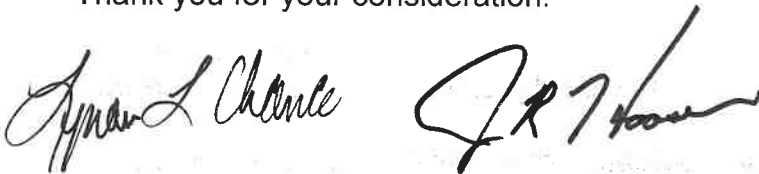
This property is on the fringe of the overlay district. It is adjacent to heavily trafficked streets controlled by two signalized intersections.

Similarly positioned commercial properties are being allowed to maintain their commercial zoning.

In 1987 a concrete block structure was replaced by the existing building that was designed to meld with the neighborhood. This would not have been possible if the proposed zoning was in place.

Should the proposed rezoning occur, we will regard it as a real estate "taking" and proceed accordingly. The property value will be diminished by approximately \$500K. We will seek to recover these damages and petition to have the property revalued and reclassified for property tax purposes. The cost to the community will be substantial; the gain will be of no consequence.

Thank you for your consideration.



Lynann L. Chance and John R. Haase

LAW OFFICES
BARBER EMERSON, L.C.

1211 MASSACHUSETTS STREET
POST OFFICE BOX 667

LAWRENCE, KANSAS 66044

(785) 843-6600

FACSIMILE (785) 843-8405

JOHN A. EMERSON
BYRON E. SPRINGER
RICHARD L. ZINN
CALVIN J. KARLIN
JANE M. ELDREDGE
MARK A. ANDERSEN*
TERRENCE J. CAMPBELL*
MATTHEW S. GOUGH*

LINDA K. GUTIERREZ
CATHERINE C. THEISEN
BETHANY J. ROBERTS
G. CLAYTON RANDLE*
LAUREN L. OBERZAN*

RICHARD A. BARBER
(1911-1998)

Matthew S. Gough
Email: mgough@barberemerson.com

*ADMITTED IN KANSAS AND MISSOURI

March 21, 2016

VIA E-MAIL ONLY

Mr. Scott McCullough, Director
Planning and Development Services
City Hall, 6 E. 6th Street
Lawrence, KS 66044

Re: *Draft Oread Neighborhood Design Guidelines (the “Guidelines”)*

Dear Scott:

I have been engaged by multiple rental property owners in the Oread Neighborhood. Each of them have complied with the current Land Development Code (the “Code”) to make improvements to their properties. Much has been written in the Oread Neighborhood Plan (the “Plan”) and in the Guidelines about the need to preserve the aesthetic and feel of the neighborhood’s historic roots. However, nothing has been written about the wholesale creation of legal non-conformities these Guidelines appear to cause overnight because of the Guidelines’ limited parking options for Duplexes. If the Guidelines truly exist to “[e]ncourage quality development which protects investment and helps attract other quality developers,” and “[r]educ[e] tensions related to development/redevelopment within the neighborhood” (Guidelines, p. 4) the Guidelines need to expressly accommodate recently and lawfully updated multi-family structures. The applicability of the Guidelines must also be expressly limited to the scope of the “project” being initiated, must integrate with the Code, and must be fair, particularly regarding parking and repairs necessitated by Acts of God.

The Historic Resources Commission (“HRC”) and the Planning Commission should adopt and insert the following clarifications and revisions to the Guidelines:

- 1. The Guidelines shall be limited in applicability to the scope of the “project” being proposed.**

If an applicant files a building permit to replace a rotted or broken window, the Guidelines should only apply to the review and comment on that window, not all the windows. If an owner desires to replace the siding of a property, the Guidelines should apply only to the siding (and work directly related thereto, such as trim board replacement), not the front porch. The Guidelines must apply to the same extent as the work being proposed, and not more. City

Mr. Scott McCullough, Director
Planning and Development Services
March 21, 2016
Page 2

Staff indicates verbally that this is the intent of the Guidelines, but an express reference is necessary to codify that intent.

2. **The Guidelines' parking requirements should only apply when and to the same extent that the parking regulations of the Code are triggered under Section 20-901, *et seq.*; provided, however, in the case of a structure that was site planned under the Code (i.e., after 2006), that structure's existing parking configuration should be *de facto* permitted if there is no proposed increase in occupancy.**

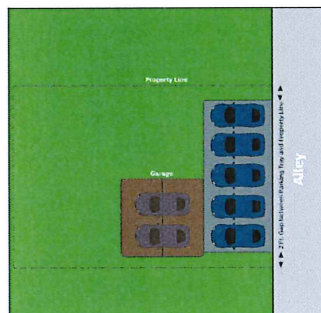
Today, the Code generally permits stacked parking on Duplexes, and owners in the neighborhood have relied upon that configuration to lawfully make significant improvements and obtain occupancy permits greater than what the Guideline's limited parking configurations would allow. *See*, Guidelines, p. 61-62. **Adoption of the Guidelines may create non-conformities on a large scale basis.** There would be no greater disincentive to updating, renovating and repairing properties in the Oread Neighborhood than the prospect of losing available off-street parking and, thus, losing permitted occupancy. Rental properties derive their value from the income they may create, and a reduction in occupancy may significantly reduce fair market value (and the tax base). The trigger for parking compliance under the Guidelines must, *at a minimum*, be identical to those under the Code.

Additional accommodation needs to be expressly made for those structures which have undergone site plan review since the adoption of the Code. If an eight bedroom Duplex has eight site-planned parking spaces, the Guidelines should not punish that property in the future by reducing parking, when a "project" does not increase occupancy. Adopting this accommodation does not increase density or parking in the neighborhood, and helps avoid the undesirable result of non-conforming status for the most recently updated properties. Imagine the potential hardship created by taking parking away from these properties (especially if taken involuntarily because of a fire or tornado) after having undergone the time consuming and expensive task of bringing them into compliance with the Code. Consider also the extreme disincentive to undertake "projects" that reduce parking.

3. Legal non-conforming uses, structures and lots must be afforded the same protections from the implementation of the Guidelines as they are from the implementation of the Code.

Article 15 of the Code acknowledges the existence of legal non-conforming uses, structures and lots, and the limitations and opportunities associated therewith. To the extent they are not protected by paragraph 2, above, owners who find themselves with non-conforming uses, structures, or lots (upon application of the Guidelines) deserve express protections regarding the perpetuation of those legal non-conformities. In this regard, whatever opportunities exist under the Code to renovate, expand, or rebuild those non-conformities should likewise exist under the Guidelines.

4. The parking configuration depicted below should be re-inserted in the Guidelines because it is not “double-stacking.” This design should be an option in appropriate circumstances, determined on a case-by-case basis.



It is unfair to completely eliminate today the possibility of allowing a Duplex more than five parking spaces in the future, especially for those properties that have been redeveloped since the adoption of the Code in 2006. The most recent draft of the Guidelines permit two limited configurations that effectively limit parking to five spaces on any Detached Dwelling or Duplex, and eliminates the only seven-car option.¹ The above configuration, which permits a two car garage and five outside spaces, should be re-inserted because (aesthetically, at least) it's not “double-stacking” if a garage door separates the cars. On a “case-by-case” basis, that configuration may be appropriate in the future.

¹The Guidelines accomplish the wholesale reduction of off-street parking without ever expressly saying that double stacking is prohibited or that no more than five spaces will be approved under the Guidelines. An outcome this significant should be achieved via a base zoning district amendment or by express text amendment to the Code, rather than being buried on pages 61-62 of the Guidelines, where they are unlikely to draw much attention.

5. **When there is no alley behind a Lot, and when a property can accommodate parking on an existing driveway, parking should continue to be permitted in the driveway if a “project” does not increase occupancy.**

It may be practically impossible to provide for parking behind the front facade of a structure. The requirement on page 61 of the Guidelines (Section B4.b.ii) should be revised to “should” instead of “shall” in the case of “projects” that do not increase occupancy. The stricter requirement for rear parking should be limited to “projects” that propose to increase occupancy, and only to the extent of such increased occupancy.

6. **When the “project” commences as a result of a casualty event (e.g., wind, fire, or hail) and when the “project” is financed by insurance proceeds, the implementation of the Guidelines should be reasonably applied to accommodate the availability of such proceeds.**

The Guidelines regularly call for the use of specialty building materials or designs that appear to be more expensive (labor and materials) than what may otherwise be permitted under the Building Code. For example, the Guidelines’ list of six permitted types of roofing material (Guidelines, p. 65) are likely more expensive than the “basic” 3-tab asphalt shingles being used on virtually all new construction (and roof replacements) in Lawrence. Front porch foundations must be supported by 16" x 16" columns and the space between the foundation columns must be infilled with wood siding, lattice, brick or stone. If an owner is unable to secure insurance proceeds to fully comply with the Guidelines, the Staff should have flexibility to accommodate the owner’s budget, at least with respect to properties that are not in a historic district.

7. **The mere replacement of roofing materials should not trigger the Guidelines’ requirements for specialty roofing materials.**

Although the City’s website indicates that the replacement of roofing materials are exempt from the City’s building permit requirements,² the Code section referenced on that web page (*See* Section 5-144) does not appear to contain the same exemption. If, in fact, one needs a building permit to replace roofing materials (thereby triggering the applicability of the Guidelines), the mandatory use of “special” roofing materials in the Oread Neighborhood may create a disincentive to make ordinary and necessary roof repairs and replacements. If no permit is required, the Guidelines would not apply to the roof replacement anyway.

²See: https://www.lawrenceks.org/assets/pds/devservices/bsd/ds_bsd_permit_exemptions.pdf

Mr. Scott McCullough, Director
Planning and Development Services
March 21, 2016
Page 5

The Guidelines, as revised and clarified above, help establish predictability for those who voluntarily and deliberately undertake a "project" in the neighborhood (whether large or small), while avoiding unfair or uncertain outcomes for those who find themselves confronted with the Guidelines involuntarily following an insured casualty loss. Express language providing assurances regarding the applicability of new parking guidelines is essential to maintain investor (and lender) confidence, and does not increase density in the neighborhood.

Some proponents of the Guidelines (as currently drafted) may view them as an opportunity to reduce density by eliminating off-street parking. However, taking the stated goals of the Guidelines and the Plan at face value, we must not adopt Guidelines that achieve that end on a wholesale basis. To do so may stifle investment, reinforce the status quo, and lead to the neighborhood's decline over time. I do not believe the majority of owners in the neighborhood truly understand the impact of the Guidelines' parking configurations. Duplex owners would be more vocal if the effect of the Guidelines' were made more obvious. The revisions discussed above help achieve the goal of making the Guidelines understandable, functional, and fair, and they merit your sincere consideration.

Very truly yours,

BARBER EMERSON, L.C.



Matthew S. Gough

MSG:plh

cc: Lawrence-Douglas County Metropolitan Planning Commission (via e-mail only)
Historic Resources Commission (via e-mail only)

The Lawrence Association of Neighborhood (LAN) supports the Oread Guidelines and Overlay District with the exception of any allowed stacked parking.

The **Design Guidelines and Overlay District** are important tools that can help preserve the historic integrity of the Oread Neighborhood. They should reflect the intention of the **Oread Neighborhood Plan**, which specifies areas of varying density. Single-family units (houses) make up 80% of the neighborhood. Oread zoning was changed over 40 years ago from low density to the highest density in the city. This has created problems and conflicts among property owners due to the single-family nature of the neighborhood and high density zoning that does not conform to existing structures or lot sizes.

Density should increase incrementally from duplex, triplex, fourplex, and higher multi-dwelling units. Problems have arisen because duplex parking codes allow stacked (tandem) parking. The Planning Commission is presently reviewing parking codes. Stacked parking allows an increase in the number of occupants without providing adequate access and regress for vehicles. **Additional logistical problems are created when located off of narrow allies versus wide city streets.**

Current city parking codes for dwelling units require:

Single-family-lowest density zoning (usually houses, family living, can stack cars in driveways).

Duplex –lowest density zoning in multi-family (can stack cars, may not be family-living).

Triplex – one parking space per BR (no stacked parking).

Fourplex- one parking space per BR (no stacked parking).

Larger multi-dwelling units - one parking space per BR plus one space per 10 units for visitors and guests (no stacked parking).

Duplex, triplex, fourplex indicate the number of units on a lot (4 unrelated individuals can live in one unit in multi-zoned areas, 3 unrelated in one unit in single-family zoned areas). The number of BR's is determined by the number of parking spaces that can be provided on each lot. Stacked parking could increase the number of BR's by as much as 50%. When stacking vehicles : Today's Duplex=2 units =4 BRs each side =8BRs Past Duplex=2units=2BRs each side=4BRs

Four BR duplexes that allow stacked parking are attractive to developers as they increase profits. Four BR duplexes “game” duplex regulations. These are not family-units but rather small apartment complexes, appealing to investors, but problems for lower density neighborhoods. Without adequate parking, duplex occupants crowd neighborhood streets when they avoid stacking their vehicles and use on-street parking spaces.

The Design Guidelines and the Overlay District will help promote increased stability and vitality in historic Oread by encouraging responsible development and redevelopment. Stacked parking should not be allowed.

Sincerely, Candice Davis

Oread Resident, Lawrence Association of Neighborhoods Chair

From: Sheila Stogsdill

Sent: Monday, February 29, 2016 10:12 AM

To: Bruce Liese (bruce@kansascitysailing.com); Bryan Culver (bcculver@gmail.com); David Clay Britton (clay.britton@yahoo.com); Eric Struckhoff (eric.c.struckhoff@gmail.com); Jim Carpenter (jecarpenter15@gmail.com); Jim Denney (denney1@sunflower.com); Julia Butler; Patrick Kelly (Pkelly@usd497.org); Pennie von Achen; Rob Sands

Cc: Scott McCullough; Lynne Zollner; Jeff Crick; Denny Ewert; (djbrown806@gmail.com); Diane Stoddard

Subject: FW: duplex tandem parking in core neighborhoods

Commissioners –

Please see the photos and message below from Dennis Brown.

This property is located in the 900 block of Maine in the proposed Urban Conservation Overlay - Oread Design Guidelines District 1 (Low Density) that is on your agenda March 21st.

Sheila M. Stogsdill, *Planning Administrator* - [sstogsdill@lawrenceks.org](mailto:ssogsdill@lawrenceks.org)

Planning & Development Services Department | www.lawrenceks.org/pds

City Hall, 6 E. 6th Street

P.O. Box 708, Lawrence, KS 66044-0708

office (785) 832-3157 | fax (785) 832-3160

From: Dennis Brown [<mailto:djbrown806@gmail.com>]

Sent: Sunday, February 28, 2016 3:51 PM

To: Sheila Stogsdill

Subject: Fwd: duplex tandem parking in core neighborhoods

Sheila, can you or Scott send this to Planning Commissioners?

----- Forwarded message -----

From: **Dennis Brown** <djbrown806@gmail.com>

Date: Sun, Feb 28, 2016 at 1:48 PM

Subject: Fwd: duplex tandem parking in core neighborhoods

To: Lynne Zollner <lzollner@lawrenceks.org>

Lynne, can you send this to HRC Commissioners?

----- Forwarded message -----

From: **Dennis Brown** <djbrown806@gmail.com>

Date: Sun, Feb 28, 2016 at 1:46 PM

Subject: duplex tandem parking in core neighborhoods

To: Diane Stoddard <dstoddard@lawrenceks.org>

Diane, can you send this to City Commissioners? Feel free to post it on the City site.

Commissioners: I am attaching photos of what happens to older housing stock when its area is zoned for duplex up to four bedrooms a side and allows four space wide tandem parking to achieve the eight spaces required. Maximum density and profit is incentivized and preservation of old housing stock is a joke.

At the joint meeting of the HRC and PC a week and a half ago, there was much discussion about approving parking guidelines in the overlay plan that would not allow tandem parking in new duplex applications. This cannot happen fast enough for what is left of the Oread Neighborhood. The pictured property is in the 900 block of Maine.

Dennis J Brown LPA President





The **Design Guidelines and Overlay District** are important tools that can help preserve the historic integrity of the Oread Neighborhood. They should reflect the intention of the **Oread Neighborhood Plan** which specifies areas of varying density. Single dwelling units (houses) make up 80% of the neighborhood. Oread zoning was changed over 40 years ago from low density to the highest density in the city. This has created problems and conflicts among property owners due to the single-family nature of the neighborhood and a high density zoning that has never conformed to existing structures or lot sizes.

Density should increase incrementally from duplex, triplex, fourplex, and higher multi-dwelling units. Problems have arisen because duplex parking codes allow stacked (tandem) parking. The Planning Commission is presently reviewing parking codes. Stacked parking allows an increase in the number of occupants without providing adequate access and regress for vehicles. Additional logistical problems are created when located off of narrow allies versus wide city streets.

Current city parking codes for dwelling units:

Single-family-lowest density zoning (usually houses, family living, can stack cars in driveways).

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Triplex – one parking space per BR (no stacked parking).

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Larger multi-dwelling units - one parking space per BR plus one space per 10 units for visitors and guests (no stacked parking).

Duplex, triplex, fourplex indicate the number of units on a lot (4 unrelated individuals can live in a unit in multi-zoned areas, 3 unrelated in a unit in single-family zoned areas). The number of BR's is determined by the number of parking spaces that can be provided on each lot. Stacked parking could increase the number of BR's by as much as 50%.

Desirable property development that will increase profits is now the 4 BR duplex that allows stacked parking. Four BR duplexes “game” duplex regulations. The two BR family-oriented units of the past have now become small apartment complexes, appealing to investors, but problems for neighborhoods. Duplex occupants crowd neighborhood streets when they avoid stacking their vehicles and instead use on-street parking spaces.

Duplexes have become higher density, non-family, units in what should be lower density zoned areas. Parking is an effective tool to control density. Why should a duplex be allowed a parking advantage over other multi-zoned units? **Stacked parking leads to an increase in density without providing adequate parking, thus incentivizing building large duplexes in lower density zoned areas.**

The Design Guidelines and the Overlay District will help promote increased stability and vitality in historic Oread by encouraging responsible development and redevelopment.

Sincerely, Candice Davis Oread Neighborhood Resident
Chair, Lawrence Association of Neighborhoods