

Updated:

5/23/16 @ 10:45am

Added communications for Item 6 - Text Amendment, Parking & Access Standards

5/17/16 @ 5:00pm

LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION CITY HALL, 6 EAST 6TH STREET, CITY COMMISSION MEETING ROOM AGENDA FOR PUBLIC & NON-PUBLIC HEARING ITEMS MAY 23 & 25, 2016 6:30PM - 10:30PM

GENERAL BUSINESS:

Recognize Bruce Liese and Jim Denney for their years of service on Planning Commission.

PLANNING COMMISSION MINUTES

Receive and amend or approve the minutes from the Planning Commission meeting of April 25, 2016.

COMMITTEE REPORTS

Receive reports from any committees that met over the past month.

COMMUNICATIONS

- a) Receive written communications from the public.
- b) Receive written communications from staff, Planning Commissioners, or other commissioners.
- c) Receive written action of any waiver requests/determinations made by the City Engineer.
- d) Disclosure of ex parte communications.
- e) Declaration of abstentions from specific agenda items by commissioners.

AGENDA ITEMS MAY BE TAKEN OUT OF ORDER AT THE COMMISSION'S DISCRETION

REGULAR AGENDA (MAY 23, 2016) MEETING NON-PUBLIC HEARING ITEMS:

ITEM NO. 1 FINAL DEVELOPMENT PLAN FOR SIXTH & MONTEREY WAY PCD MORGAN ADDITION (MKM)

Morgan Addition: **FDP-16-00107**: Consider a Final Development Plan for Phase 1 of Sixth & Monterey Way PCD Morgan Addition, a one-lot development of a *Construction Sales and Service* use and an addition to the existing *Detached Dwelling*. The site, located at 800 Monterey Way, contains approximately 0.6 acres. Submitted by Allen Belot Architect, for Robert J. and Beverly G. Morgan, property owners of record.

ITEM NO. 2 FINAL DEVELOPMENT PLAN FOR BAUER FARM; 4661 BAUER FARM (SLD)

Bauer Farm: **FDP-16-00104**: Consider a Final Development Plan for Zaxby's, a Fast Order Food with Drive-In, located at 4661 Bauer Farm Dr. Submitted by Carter Engineering Consultants, Inc., for JULI, LLC, contract purchaser, Free State Holdings Inc, property owner of record.

PUBLIC HEARING ITEMS:

ITEM NO. 3 FINAL DEVELOPMENT PLAN FOR WESTRIDGE WASH, LUBE, & AUTO SALES; 3530 W 6TH ST (KES)

Westridge Wash, Lube, & Auto Sales: **FDP-16-00103**: Consider a Final Development Plan for Westridge Wash, Lube, & Auto Sales, located at 3530 W 6th St. Submitted by Grob Engineering Services, LLC, for Westridge Lawrence LLC, property owner of record.

ITEM NO. 4 CONDITIONAL USE PERMIT; CLEAN RUBBLE FILL; 1736 E 1550 RD (MKM)

Clean Rubble Fill: **CUP-16-00105**: Consider a Conditional Use Permit for clean rubble landfill, on approximately 40 acres located at 1736 E 1550 Rd. Submitted by Grob Engineering Services, LLC, for Nunemaker-Ross, Inc., property owner of record.

ITEM NO. 5 TEXT AMENDMENT TO LAND DEVELOPMENT CODE; VALET PARKING (SLD)

TA-16-00128: Consider a Text Amendment to the City of Lawrence Land Development Code, Chapter 20, Article 17 and related sections of Article 9, for revisions related to the use and design standards for Valet Parking. *Initiated by the City Commission on 3/28/16.*

ITEM NO. 6 TEXT AMENDMENT TO LAND DEVELOPMENT CODE; PARKING & ACCESS STANDARDS (SMS)

TA-13-00235: Continue discussion related to proposed Text Amendments to the City of Lawrence Land Development Code, Article 9 and related sections of Chapter 20, for comprehensive revisions to parking and access standards. Discussion will focus on Sections 20-908 & 20-915 related to Location, Driveways and Access, including parking configurations for duplex dwellings. *Action on this item will not occur until after the commission completes their discussion on several of the elements of the code language and a final draft is available for their review.*

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

MISC NO. 1 QUORUM EVENT

A possible quorum of the Planning Commission may convene after the meeting to socialize.

ADJOURN

CALENDAR

April 2016						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
					1	2
3	4	5	6	7	8	9
10	11	12	13	14	15	16
17	18	19	20	21	22	23
24	25	26	27	28	29	30

May 2016						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
1	2	3	4	5	6	7
8	9	10	11	12	13	14
15	16	17	18	19	20	21
22	23	24	25	26	27	28
29	30	31				

June 2016						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30		

PCCM Meeting:

(Generally 2nd Wednesday of each month, 7:30am-9:00am)

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2016 LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION MID-MONTH & REGULAR MEETING DATES

Mid-Month Meetings, Wednesdays 7:30 – 9:00 AM ** alternate day/time	Mid-Month Topics	Planning Commission Meetings 6:30 PM, Mon & Wed		
Jan 13	Article 9 text amendments - Parking	Jan 25	Jan 27	
Feb 18 ** Thursday 6:00 PM meeting	Joint meeting with HRC – Oread Design Guidelines	Feb 22	Feb 24	
Mar 9 ** Wednesday 5:30 PM meeting	Joint meeting with Sustainability Advisory Board [Meeting Room C – Lawrence Public Library – 707 Vermont Street]	Mar 21	Mar 23	
Apr 13	Retail Market Study	Apr 25	Apr 27	
May 11	APA Conference recap & Nonconformities 101	May 23	May 25	
Jun 8	TBD	Jun 20	Jun 22	
Jul 13	TBD	Jul 25	Jul 27	
Aug 10	TBD	Aug 22	Aug 24	
Sep 14	TBD	Sep 26	Sep 28	
Oct 12	TBD	Oct 24	Oct 26	
Nov 2	TBD	Nov 14	Nov 16	
Nov 30	TBD	Dec 12	Dec 14	

Suggested topics for future meetings:

How City/County Depts interact on planning issues
Stormwater Stds Update – Stream Setbacks
Overview of different Advisory Groups – potential overlap on planning issues
Joint meeting with other Cities' Planning Commissions
Joint meeting with other Cities and Townships – UGA potential revisions
New County Zoning Codes
Tour City/County Facilities
Water Resources

Communication Towers – Stealth Design, # of co-locations, notice area WiFi Connectivity & Infrastructure Planning Oread Overlay Districts & Design Guidelines Comprehensive Plan – Goals & Policies Affordable Housing Retail Market Impacts Case Studies

Meeting Locations

The Planning Commission meetings are held in the City Commission meeting room on the 1^{st} floor of City Hall, 6^{th} & Massachusetts Streets, unless otherwise noticed.

Planning & Development Services | Lawrence-Douglas County Planning Division | 785-832-3150 | www.lawrenceks.org/pds

	2016 PLA	NNING	СОММ	ISSION A	TTENDA	NCE						
	Jan 25 2016	Feb 22 2016	Mar 21 2016	April 25 2016	May 23 2016							
Britton	Yes	Yes	Yes	Yes								
Butler	Yes	Yes	Yes	Yes								
Carpenter	Yes	Yes	Yes	Yes								
Culver	No	Yes	Yes	Yes								
Denney	Yes	Yes										
Kelly	Yes	Yes	Yes	Yes								
Liese	Yes	Yes	No	Yes								
Sands	Yes	Yes	Yes	Yes								
Struckhoff	Yes	Yes	Yes	Yes								
von Achen	Yes	Yes	Yes	Yes								
	20	<u> 16 MID-I</u>	<u>MONTH</u>	ATTEN	DANCE							
	Jan 13	Feb 18	Mar 9	April 13	May 11	June 8	July 13	Aug 10	Sept 14	Oct 12	Nov 2	Nov 30
	2016	2016	2016	2016	2016	2016	2016	2016	2016	2016	2016	2016
Britton	Yes	Yes	Yes	Yes	Yes							
Butler	No	No	Yes	No	No							
Carpenter	Yes	Yes	Yes	Yes	Yes							
		163	165	res	res							
Culver	Yes	Yes	Yes	Yes	Yes							
Culver Denney												
	Yes	Yes										
Denney	Yes Yes	Yes Yes	Yes	Yes	Yes							
Denney Kelly	Yes Yes Yes	Yes Yes Yes	Yes	Yes	Yes							
Denney Kelly Liese	Yes Yes Yes No	Yes Yes Yes No	Yes Yes No	Yes Yes No	Yes No Yes							



PLANNING COMMISSION MEETING April 25, 2016 Meeting Minutes

April 25, 2016 - 6:30 p.m.

Commissioners present: Britton, Butler, Carpenter, Culver, Kelly, Liese, Sands, Struckhoff, von Achen

Staff present: McCullough, Stogsdill, Day, Ewert, Larkin, M. Miller, Pepper, Simmons

PLANNING COMMISSION MINUTES

Receive and amend or approve the minutes from the Planning Commission meeting of March 21, 2016.

Motioned by Commissioner Struckhoff, seconded by Commissioner Kelly, to approve the March 21, 2016 Planning Commission minutes.

Motion carried 7-0-1, with Commissioner Liese abstaining. Commissioner von Achen was not present for the vote.

COMMITTEE REPORTS

No reports from any committees that met over the past month.

EX PARTE / ABSTENTIONS / DEFERRAL REQUEST

- Receive written communications from staff, Planning Commissioners, or other commissioners.
- No ex parte.
- No Abstentions.

ITEM NO. 1 CONDITIONAL USE PERMIT; STONY POINT HALL; 1514 N 600 RD (MKM)

Stony Point: **CUP-16-00035**: Consider the renewal of a Conditional Use Permit for Stony Point Hall, a reception and banquet hall, on approximately 13.98 acres, located at 1514 N 600 Rd, Baldwin City. Submitted by Russell and Lucretia Carlson, property owners of record. *(Joint meeting with Baldwin City Planning Commission)*

STAFF PRESENTATION

Ms. Mary Miller presented the item.

APPLICANT PRESENTATION

Mr. and Ms. Russell and Lucretia Carlson were present for questioning.

PUBLIC HEARING

Mr. Hank Booth, Baldwin City Chamber of Commerce, supported the Conditional Use Permit.

ACTION TAKEN

Motioned by Commissioner Liese, seconded by Commissioner Butler, to approve the Conditional Use Permit, CUP-16-00035, for Stony Point Hall, a *Recreation Facility* use, and forwarding it to the Board of County Commissioners with a recommendation for approval based upon the findings of fact in the body of the staff report subject to the following condition:

The Conditional Use shall be administratively reviewed every 5 years

Commissioner Britton inquired about the removal of the 10 year expiration time frame.

Ms. Miller said the time limit was a good tool for a use that may not be compatible with an area or if the use has impacts that need to be tracked. She said the previous Board of County Commissioners used it pretty much on every Conditional Use Permit. She said the current Board of County Commissioners was a little more selective and used it when they felt there may be an issue down the road.

ITEM NO. 2 PRELIMINARY PLAT FOR WALNUT ADDITION; 775 WALNUT ST (BJP)

Walnut Addition: **PP-16-00057**: Consider a Preliminary Plat for Walnut Addition, a 7 lot residential subdivision containing 2.018 acres, located at 775 Walnut St. Submitted by Grob Engineering Services LLC, for Lawrence Habitat for Humanity, property owner of record.

STAFF PRESENTATION

Ms. Becky Pepper presented the item.

APPLICANT PRESENTATION

Mr. Dean Grob, Grob Engineering Services, was present for questioning.

PUBLIC HEARING

No public comment.

ACTION TAKEN

Motioned by Commissioner Liese, seconded by Commissioner Carpenter, to approve the Preliminary Plat, PP-16-00057, of Walnut Addition, located at 775 Walnut St.



ITEM NO. 3 PRELIMINARY PLAT FOR FREESTATE DENTAL ADDITION; 4111 W 6TH ST (BJP)

Freestate Dental: **PP-16-00073**: Consider a Preliminary Plat for Freestate Dental Addition, a one lot residential-office subdivision containing 0.850 acres, located at 4111 W 6th St. Submitted by Landplan Engineering, PA, for Freestate Dental Building LLC, property owner of record.

STAFF PRESENTATION

Ms. Becky Pepper presented the item.

APPLICANT PRESENTATION

Mr. Brian Sturm, Landplan Engineering, was present for questioning.

PUBLIC HEARING on Variance

Mr. Doug Garber property owner directly to the east. expressed concern about stormwater runoff.

APPLICANT CLOSING COMMENTS

Mr. Sturm said there was an approved drainage study that staff looked at and it spoke to how the drainage would be handled. He said water would generally be collected and conveyed to the north greenspace and out to the storm sewer system.

COMMISSION DISCUSSION

Commissioner Kelly inquired about the Board of Zoning Appeals variance and if it was related to the right-of-way variance they were looking at tonight.

Mr. McCullough said no, they were two separate issues. He stated the Board of Zoning Appeals variance had been withdrawn.

ACTION TAKEN on Variance

Motioned by Commissioner Liese, seconded by Commissioner Culver, to approve the variance requested from Section 20-810(e)(5)(i) subject to the following condition:

1. The plat shall be revised to note that a variance from the right-of-way width required in Section 20-810(e)(5) of the Subdivision Regulations was granted by the Planning Commission to allow the right-of-way on the subject property side of the center line to remain at 50 ft and list the date of approval

Unanimously approved 8-0. Commissioner von Achen was not present for the vote.

ACTION TAKEN on Preliminary Plat

Motioned by Commissioner Liese, seconded by Commissioner Culver, to approve the Preliminary Plat, PP-16-00073, for Freestate Dental Addition, subject to the following condition:

- 1. The plat shall be revised to include the following note:
 - a. Revise the plat to show the 50' Parking and Building Setback along W 6th Street.

ITEM NO. 4 CONDITIONAL USE PERMIT; PINE FAMILY TREE NURSERY; 1782 E 1500 RD (SLD)

Pine Family Tree Nursery: **CUP-16-00070**: Consider the renewal of a 30.5 acre Conditional Use Permit for Pine Family Tree Nursery, Landscape Center, *Retail Nursery*, located at 1782 E 1500 Rd. Submitted by Landplan Engineering PA, for Pine Family Investments LC, and Sue A Pine, Trustee property owners of record.

STAFF PRESENTATION

Ms. Sandra Day presented the item.

APPLICANT PRESENTATION

Mr. CL Maurer, Landplan Engineering, was present for questioning.

PUBLIC HEARING

No public comment.

ACTION TAKEN

Motioned by Commissioner Liese, seconded by Commissioner Kelly, to approve the Conditional Use Permit, CUP-16-00070, for a *Retail Nursery* and forwarding it to the County Commission with a recommendation for approval subject to the following condition(s):

1) Provision of a revised site plan to include the following notes:

"Changes to the site to add additional covered storage may be amended by a future site plan approved administratively. Changes to add retail sales on this site should be amended by a revised site plan approved by the County Commission with notice to surrounding property owners."

ITEM NO. 5A A & VC TO I-3; 77.5 ACRES; N 1300 RD & E 1750 RD (MKM)

King's Recycle Center: **Z-16-00067**: Consider a request to rezone approximately 77.5 acres from County A (Agricultural) District and VC (Valley Channel) District to County I-3 (Heavy Industrial) District, located at N 1300 and E 1750 Rds. Submitted by Landplan Engineering PA, for Dan King, Denise King, Kent King & Patricia King, property owners of record.

ITEM NO. 5B PRELIMINARY PLAT FOR KING'S RECYCLE CENTER; N 1300 RD & E 1750 RD (MKM)

King's Recycle Center: **PP-16-00068**: Consider a Preliminary Plat for Four King's Subdivision, an industrial subdivision containing approximately 77.5 acres, located at N 1300 Rd & E 1750 Rd. Submitted by Landplan Engineering PA, for Dan King, Denise King, Kent King & Patricia King, property owners of record.

ITEM NO. 5C CONDITIONAL USE PERMIT; KING'S RECYCLE CENTER; N 1300 RD & E 1750 RD (MKM)

King's Recycle Center: **CUP-16-00069**: Consider a Conditional Use Permit for King's Recycling Center, located at N 1300 Rd & E 1750 Rd. The CUP proposes the operation of a borrow pit and recycling of construction materials, as well as continued residential and agricultural uses on the property. Submitted by Landplan Engineering PA, for Dan King, Denise King, Kent King & Patricia King, property owners of record.

STAFF PRESENTATION

Ms. Mary Miller presented items 5A-5C at the same time.

APPLICANT PRESENTATION

Mr. CL Mauer, Landplan Engineering, said the King Family had been in the area for 30 years. He said the area had already been disturbed. He said it contained clay and that there were very few places in Lawrence to get borrow pit material. He said the highway was 30' higher and created a natural berm. He said it created a visual berm for power lines along the South Lawrence Trafficway (SLT). He said ducks were visible on the ponds during the highway construction and not bothered by noise. He talked about the decibel levels on the trafficway and took several readings on his phone while sitting on the SLT near the turnpike entrance. He stated noise would be there whether this project was approved or not. He showed pictures and video on the overhead. The video showed what a driver would visually see driving east and west on the highway.

Mr. Kent King, King Construction, said they wanted to keep the area as rural as possible. He said lean clay was important to the construction trade and that it was a benefit to be able to recycle construction materials. He said it was a resource instead of going to the landfill. He said the crushing machine had a magnet to pull out the steel and would be no louder than the South Lawrence Trafficway (SLT). He said the area had been changed by the SLT. He said Mr. Maurer talked to the neighbors and that they were not against this but did ask for a berm, which would be included.

PUBLIC HEARING

Ms. Karen Heeb, South Siders Neighborhood Association, said she was not against the King family but felt the location for rock crushing recycling was not appropriate. She agreed with the information in the staff report. She did not feel that the use fit the Horizon 2020 plan. She said RD Johnson had a recycling plant less than a mile from this location. She expressed concern about increased traffic and noise.

Mr. Joe Comparato, Jambars Futbol Club, said he had no objections to the business but expressed concern about rezoning to industrial for future uses.

APPLICANT CLOSING COMMENTS

Mr. Maurer said regarding the zoning of the area, everything south was zoned VC (Valley Channel) so no other industrial use could be out there.

Mr. Brian King responded to Ms. Heeb's comment about their recycling center being so close to another one by stating that most towns have recycling centers located near each other. He said Topeka had three recycling centers next to each other.

Ms. Taylor Norris responded to the comments about increased traffic. She said their primary working hours would be 7:00am-4:00pm Monday-Friday so the soccer traffic would not be at the same time.

COMMISSION DICUSSION

Commissioner Pennie von Achen arrived at the meeting at 7:57pm.

Commissioner Sands asked if the area plan extended into this section and what the recommendation was.

Ms. Miller said the area plan did extend to this area but had no recommendations. She said she assumed that was because the South Lawrence Trafficway was proposed to be there.

Commissioner Kelly said the city had already zoned a large portion of the area north of this property for industrial. He asked the applicant if he was making the most of the property for the borrow pit. He wondered if the applicant had interest in moving to the area zoned along K-10 highway.

Mr. King said no.

Commissioner Britton asked if the recycling use had a time limit.

Ms. Miller said it would be an allowed use.

Commissioner Britton wondered about building in a time frame for review or expiration.

Mr. McCullough said the applicable process would be the site plan process through the County Commission if it is rezoned to industrial. He said any use would have to comply with the zoning district and any improvements would go through the site plan process.

Commissioner Sands asked about plans for sector plans along the South Lawrence Trafficway.

Mr. McCullough said there were plans for most of the South Lawrence Trafficway. He said it was not anticipated for this property to urbanize.

Commissioner Sands inquired about the area north of 1300 Rd that looked to be temporary in nature for the purpose of supporting the construction of the highway extension.

Ms. Miller said it was owned by KDOT. She said that site had a former Conditional Use Permit for a recycling facility.

Commissioner Kelly said he was struggling with the concept of gateway. He stated the Comprehensive Plan did not provide much guidance to determine a gateway. He said the argument was challenging.

Commissioner Britton said the plan just identifies that it is a gateway. He said it was hard to draw the line of where it starts and stops. He said the soccer fields were a bonus to the aesthetics of a gateway. He said Mr. Maurer's video of what people would see from the South Lawrence Trafficway was enlightening.

Commissioner Kelly said part of their job was planning. He said this may not be taking the best use of planning. He said this industrial use may be okay but other uses would be permitted.

Commissioner Carpenter expressed concern about limitations of water. He said potential uses could pollute ground water. He said this area was near floodplain and in an isolated area.

Commissioner Butler asked where the other recycling center near this location was at.

Mr. Maurer pointed on the map displayed on the overhead. He said it was a mile north of the site they were talking about.

Commissioner Butler asked how long the King's had owned the property.

Mr. Maurer said three years.

Commissioner Carpenter said the property was not in the city and did not have stormwater. He said it would be reliant on possible future rural water and septic system. He expressed concern about a septic system with the other potential uses of industrial.

Mr. Maurer said there were detention ponds in the area. He said the Rural Water District had already purchased an easement from the King's at the corner of 31st Street and 1750 Road. He said the King's would have minimal water usage. He said the crusher and water trucks would use pond water. He said he would prefer to only have a Conditional Use Permit but it required rezoning to I-3.

Commissioner Britton asked why the rezoning was necessary versus a Conditional Use Permit.

Ms. Miller said the use was most similar to concrete plants. She said it was not just like quarrying because it was bringing material to the site.

Commissioner Liese reiterated the staff report findings.

Complete audio & video from this meeting can be found online: http://www.lawrenceks.org/boards/planning-commission/agendas

ACTION TAKEN on Item 5A

Motioned by Commissioner Liese, seconded by Commissioner Carpenter, to deny the rezoning request, Z-16-00067, for approximately 77.5 acres County A (Agricultural) District to County I-3 (Heavy Industrial) District and forwarding the request to the Board of County Commissioners with a recommendation for denial based on the findings of fact found in the body of the staff report.

Commissioner Britton felt denial was the appropriate call due to potential industrial uses in the future. He said he heard good arguments to have this operation but that it was for County Commission to determine. He said he was inclined to follow the staff recommendation of denial.

Commissioner Culver said it was a unique piece of land and that rezoning it to heavy industrial was concerning for the long term.

Commissioner Sands felt they had a responsibility to consider what this would look in 10 years.

Commissioner Carpenter said he was not opposed to the King business. He wished there was some other way to continue the use without rezoning this location.

Commissioner Struckhoff said he would support the motion for denial mainly due to the non-contiguous I-3 rezoning request and the possibilities that extend there. He appreciated the recycled materials that the King's produced. He wanted to support the business, but not the zoning at this location.

Motion carried 7-1-1, with Commissioner Butler voting against the motion. Commissioner von Achen abstained due to her late arrival at the meeting.

ACTION TAKEN on Item 5B

Motioned by Commissioner Kelly, seconded by Commissioner Liese, to deny the Preliminary Plat, PP-16-00068, for Four King's Subdivision, located at N 1300 Rd & E 1750 Rd, based on the findings of fact found in the body of the staff report.

Commissioner Kelly withdrew his motion for denial, seconded by Commissioner Liese to withdraw the motion for denial.

Motioned by Commissioner Liese, seconded by Commissioner Britton, to defer the Preliminary Plat, PP-16-00068, for Four King's Subdivision, located at N 1300 Rd & E 1750 Rd, until the Board of County Commissioners acts on the rezoning.

Motion carried 8-0-1, with Commissioner von Achen abstaining due to her late arrival at the meeting.

ACTION TAKEN on Item 5C

Motioned by Commissioner Kelly, seconded by Commissioner Liese, to approve the Conditional Use Permit, CUP-16-00069, for King's Recycling Center, located at N 1300 Rd & E 1750 Rd, subject to the conditions in the staff report.

Commissioner Culver asked if condition 3 would still apply to this Conditional Use Permit since it was related to the recycling facility.

Commissioner Kelly amended the motion, seconded by Commissioner Liese, to approve the Conditional Use Permit, CUP-16-00069, for King's Recycling Center, located at N 1300 Rd & E 1750 Rd, subject to the conditions in the staff report, except for conditions 3 and 6 which were related to the recycling facility.

- 1. The Conditional Use Permit shall be administratively reviewed every 5 years. The Conditional Use Approval for the borrow pit use shall expire 10 years from the date of the Board of County Commissioner's approval.
- Commercial truck traffic to the facility is limited to the following roads:
 E 1750 Road/Noria Road; N 1400 Road (Old K-10 Highway); E 1700 Road; K-10 Highway east or west.
- 3. Only clean construction rubble shall be stockpiled or processed on the site.
- 4. Well water usage is limited to the office and employees until the property is serviced by a Rural Water District.
- 5. The borrow pit shall be excavated no deeper than Elev. 800 Ft. (NAVD).
- 6. Provision of a revised CUP plan with the following changes to the landscaping, per Planning approval:
 - Provide a mix of evergreen and deciduous trees for screening landscaping along N 1300 Road.
 - *b.* Revise the landscaping shown along E 1750 Road to include evergreen trees west of the street trees that are shown and to extend the landscaping to the north property line.
 - c. Label the species of the trees along E 1750 Road to match those listed on the Master Street Tree Plan, when approved.
 - d. List Conditions No. 1-5 on the plan.

Motion carried 8-0-1, with Commissioner von Achen abstaining due to her late arrival at the meeting.

ITEM NO. 7 IG TO CS; 3.82 ACRES; 1235 N 3RD ST (KES)

Z-16-00066: Consider a request to rezone approximately 3.82 acres from IG (General Industrial) District to CS (Strip Commercial) District, located at 1235 N 3rd St. Submitted by Allen Belot Architect, for Don E. Westheffer Trustee and Wanda L. Westheffer Trustee, property owners of record.

STAFF PRESENTATION

Ms. Katherine Simmons presented the item.

APPLICANT PRESENTATION

Mr. Allen Belot, Allen Belot Architect, said the property had been cleaned up significantly in the past few years. He said the property owner wanted to take advantage of the Event Center use.

PUBLIC HEARING

No public comment.

ACTION TAKEN

Motioned by Commissioner Liese, seconded by Commissioner von Achen, to approve the rezoning request, Z-16-00066, from IG (General Industrial) District to CS (Commercial Strip) District, located at 1235 N 3rd Street, and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report.

Unanimously approved 9-0.



ITEM NO. 6 TEXT AMENDMENT TO ZONING REGULATIONS & SUBDIVISION REGULATIONS; ACCESSORY DWELLING UNITS (MKM)

TA-15-00461: Consider Text Amendments to the *Zoning Regulations for the Unincorporated Territory of Douglas County, Kansas* and the *Subdivision Regulations for Lawrence and the Unincorporated Areas of Douglas County, KS* to add Accessory Dwelling Units as a permitted use and to establish standards for the use. *Initiated by County Commission on 9/2/15*.

STAFF PRESENTATION

Ms. Mary Miller presented the item.

PUBLIC HEARING

No public comment.

ACTION TAKEN

Motioned by Commissioner Liese, seconded by Commissioner Britton, to approve the proposed amendment, TA-15-00461, to revise Sections 12-303, 12-306, 12-307, 12-308, and to add Section 12-319.9 in the Zoning Regulations and to revise Sections 11-104 and 11-106 in the Subdivision Regulations [20-804 and 20-806, City Code] and that the Planning Commission forward a recommendation for approval to the Board of County Commissioners.

Commissioner Kelly asked why rental was allowed but not bed and breakfast.

Ms. Miller said it could be permitted as an agritourism use or under certain zoning districts. She said having someone different stay every night was seen as more of a commercial use and not compatible with that zoning district. She said long-term rentals were allowed.

Commissioner von Achen said it seemed like a reasonable step to take and asked why this text amendment had not come before now.

Ms. Miller said there had been public inquiries. She stated it had been discussed and was in the works. She said it did not come to the point of a text amendment until recently.

Commissioner Britton inquired about notice to surrounding neighbors.

Ms. Miller said if there was a home business or Conditional Use Permit on the property there would be mailed notice.

Unanimously approved 9-0.

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

Complete audio & video from this meeting can be found online: http://www.lawrenceks.org/boards/planning-commission/agendas

MISC NO. 1 OREAD DESIGN GUIDELINES

Receive staff memo regarding Oread Design Guidelines.

Motioned by Commissioner Britton, seconded by Commissioner Struckhoff, to initiate a text amendment to correct and clarify Article 13 of the Land Development Code to clearly cite and delineate the intended notice requirements.

Motion carried 9-0.

Mr. McCullough reminded Planning Commission about the May 11, 2016 Mid-Month meeting. He said Commissioner Carpenter would provide a brief summary about the American Planning Association conference that he attended. He said they would also have a discussion about non-conformities and the impact from a lending perspective.

ADJOURN 9:41PM

Planning Commission Key Links



Plans & Documents

- o Horizon 2020
- o Sector/Area Plans
- o <u>Transportation 2040</u>
- o 2012 Retail Market Study

Development Regulations

- o Community Design Manual
- o County Zoning Regulations
- o Land Development Code
- o Subdivision Regulations

Online Mapping

- o City of Lawrence Interactive GIS Map
- o Douglas Co. Map Viewer
- o Submittals to the Planning Office

Planning Commission

- o <u>Bylaws</u>
- o <u>Mid-Months & Special Meetings</u>
- o <u>Minutes</u>
- o <u>Planning Commission Schedule/Deadlines</u>

PLANNING COMMISSION REPORT Non-Public Hearing Item

PC Staff Report 05/23/16

ITEM NO. 1 FINAL DEVELOPMENT PLAN FOR SIXTH & MONTEREY WAY PCD MORGAN ADDITION (MKM)

FDP-16-00107: Consider a Final Development Plan for Phase 1 of Sixth & Monterey Way PCD Morgan Addition, a one-lot development of a *Construction Sales and Service* use and an addition to the existing *Detached Dwelling*. The site, located at 800 Monterey Way, contains approximately 0.6 acres. Submitted by Allen Belot Architect, for Robert J. and Beverly G. Morgan, property owners of record.

STAFF RECOMMENDATION: Planning staff recommends approval of the Final Development Plan based upon the findings of fact presented in the body of the Staff Report subject to the following conditions:

- 1. Provision of an executed Site Plan Performance Agreement prior to the recording of the Final Development Plan.
- 2. The final plat for the subject property, Morgan Addition, shall be recorded with the Register of Deeds, prior to the recordation and release of the Final Development Plan.

Applicant's Reason for Request:

This property has been part of the original PCD since its inception. In the current iteration this current PDP has been approved for 32 multi-family dwelling units only. The owners now wish to continue living in the existing and expanded single family residence located at the far eastern area of the property. They also wish to demolish the existing structurally deteriorating contractors shed located west of the single family residence and construct a combination automobile garage/contractors shop immediately adjacent to the existing house. For the remainder of the property, they want to continue with the previously approved multi-family use.

Design Standards to Consider

- Consistency with approved Preliminary Development Plan, PDP-15-00378.
- Consistency with Article 10 of the 1966 Zoning Ordinance.
- Consistency with standards in the 2006 Development Code.

KEY POINTS:

- Final Development Plans for projects with preliminary approvals prior to the adoption of the 2006 Development Code require Planning Commission approval.
- The most recent approved Preliminary Development Plan for Sixth & Monterey Way PCD [PDP-15-00378] modified the previously approved plan for four apartment buildings on the property, identified as Phase A-2, to reduce the number of apartment buildings to three (on Lot 2) while retaining the existing residence on the east side of the property and relocating (and rebuilding) the construction shop to the south of the residence (on Lot 1).

• The proposed development is in substantial conformance with the approved Preliminary Development Plan, PDP-15-00378. The Preliminary Development Plan did not indicate the project would be phased but, due to the development preferences of the owners of the existing residence, the eastern portion of the project, being platted as Lot 1, Morgan Addition, is included in this Final Development Plan and the western portion, being platted as Lot 2, Morgan Addition will be a future phase.

ASSOCIATED CASES

- PDP-15-00378, Sixth & Monterey Way PCD Preliminary Development Plan for Phase A-2. The development plan provided a site plan of the proposed development, revised the use restrictions placed on Phase A-2 to allow *Detached Dwelling*, and *Construction Sales and Services* uses, and also serves as the Preliminary Plat for the property. The Preliminary Development Plan was recommended for approval by the Planning Commission on September 21, 2015 and approved by the City Commission on October 10, 2015.
- PF-15-00380, Morgan Addition Final Plat. This plat will complete the platting of the Sixth & Monterey Way PCD. The plat was approved by the City Commission on October 20, 2015.

OTHER ACTION REQUIRED

- The final plat shall be recorded at the Register of Deeds prior to the release of the Final Development Plans.
- When conditions have been met, the Final Development Plan will be recorded at the Douglas County Register of Deeds Office and released to Development Services for processing of building permits.
- Submittal of building permit application and construction plans. Construction may commence only after building permits have been issued.

PUBLIC COMMENT

No public comment was received prior to the printing of this staff report.

PLANS AND STUDIES REQUIRED

- *Traffic Study* Study was not required as the proposed development was similar to that shown on previously approved Preliminary Development Plan.
- *Downstream Sanitary Sewer Analysis* Provided and accepted with preliminary development plan.
- *Drainage Study* Provided and accepted with preliminary development plan.

GENERAL INFORMATION

Current Zoning and Land Use: PCD-[6th and Monterey Way] (Planned Commercial

Development) District; Detached Dwelling.

Surrounding Zoning: To the west: PCD-[6th and Monterey Way] (Planned

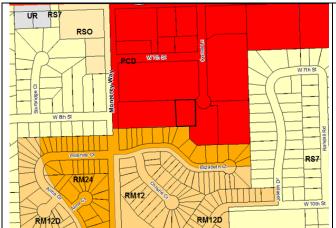
Commercial Development) District; Undeveloped, this property was included in the approved Preliminary Development Plan for *Multi-Dwelling Residential* uses and

will be developed as a future phase.

To the north: PCD-[6th and Monterey Way] (Planned Commercial Development) District; *Multi-Dwelling Structures*.

To the east: PCD-[6th and Monterey Way] (Planned Commercial Development) District; *Construction Sales and Services.*

To the south: RM24 (Multi-Dwelling Residential) District; *Duplexes.* (Figure 1)



With S. Caper C. Cape

Figure 1a Zoning in the area. Subject property is outlined.

Figure 1b. Land use in the area.

STAFF SUMMARY

A Preliminary Development Plan, PDP-15-00378, revised the permitted uses in Phase A-2 of the 6th and Monterey Way Planned Commercial Development to include the proposed uses: *Detached Dwelling* and *Construction Sales and Services*. The Preliminary Development Plan retained the *Multi-Dwelling Structure* use which had been approved for the parcel with previous Preliminary Development Plans and revised the development on the east side of the parcel, which is being platted as Lot 1, Morgan Addition to retain the existing residence. Proposed changes to this site with this phase of the project include an addition to the *Detached Dwelling*, and construction of a contractor shop, *Construction Sales and Services*, to the south of the house.

The Preliminary Development Plan shows the parcel being divided into two lots. The property is being platted in phases and the subject of this Final Development Plan is being platted as Lot 1, Morgan Addition. The remainder of the property is still planned for apartment development and a Final Plat and a Final Development Plan will need to be submitted and approved prior to that phase of the project.

The Planning Commission approved a variance from the setback requirements to allow the single-story contractor shop to be located within 21 ft of the south property line and for the existing residence to maintain the existing 12 ft 4 in. setback that is currently provided from the east property line. This variance was approved subject to the condition that a screening fence be maintained along the south and east property lines. The plan shows a 6 ft cedar fencing in this area. These variances are noted on the plan.

PARKING SUMMARY

The *Detached Dwelling* use requires 2 parking spaces and these are provided in a two car garage. Additional parking could also occur on the access drive. The *Construction Sales and Service* use requires parking at a rate of 1 space for every 500 sq ft of floor area and 1 space for every acre of exterior storage or assembly. The plan does not include any exterior storage or assembly area for this use. The table below summarizes the parking requirements and the amount of parking that is provided. Parking is being provided for this project in compliance with the Parking Regulations in the 2006 Development Code.

Use	Requirement	Required	Provided
Detached Dwelling	2 per dwelling	2 spaces required	
Construction Sales & Services	1 space per each 500 sq ft of floor area PLUS 1 space per acre of outdoor storage or assembly	1200 sq ft building / 3 spaces No outdoor storage or assembly area	5 spaces
TOTAL		5 spaces	
Bicycle	1 per 10 auto spaces	1 space	1 space
ADA	Detached Dwelling: not required Construction Sales & Services: none required as business does not include customer activity on site	0	0

SITE SUMMARY

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Lot 1 Morgan Addition	Existing	Proposed	Change
Lot Area	25,900 sq ft	25,900 sq ft	
Building Coverage	2,011 sq ft	3,605 sq ft	+1,594 sq ft
Pavement Coverage	2,380 sq ft	7,470 sq ft	+5,090 sq ft
Imporvious Coverage	4,391 sq ft	11,075 sq ft	. 4 401 ca ft
Impervious Coverage	(16.95%)	(42.76%)	+6,684 sq ft
Pervious Coverage	21,509 sq ft	14,825 sq ft	- 6,684 sq ft

STAFF REVIEW

The property addressed as 800 Monterey Drive includes both phases of this development and is currently developed with a contractor's shop and a residence. These uses were not included in the list of permitted uses for the subject area, identified as Area A-2 on the plan, with the conditions of the previous Preliminary Development Plans for the Development. The original plan was to replace them with apartments.

As the property owner's plans changed, the Preliminary Development Plan was revised to add these uses to the A-2 Area and to show the Contractor's Shop use (classified as *Construction Sales and Services* in the 2006 Development Code) being relocated to the east side of the property with the residence. This portion of the property is being platted as Lot 1, Morgan Addition. The west portion of the property was shown as Lot 2, Morgan Addition with the

Preliminary Development Plan and was designed for apartment development. A Final Plat and a Final Development Plan will need to be submitted and approved prior to development of Lot 2.

Several Development Plans have been approved for portions of the development. The highlighted area in Figure 2 is the overall planned development. Phase 1 of Area A-2 is marked with a star. The remainder of the parcel to the west is Phase 2.

Common Open Space

Per Section 20-701(j) of the Development Code, a minimum of 20% of the total site area must be provided as Common Open Space. This is defined as being open space that is available for the use and enjoyment of all the tenants of the development. The plan identifies the Common Open Space for this property (Figure 3).

Approximately 22% of the lot is set aside as Common Open Space; however, as the property is developed with a Detached Dwelling the Common Open Space on this property would be more suitable for use by the residents of the dwelling. The apartment development on Lot 2 will also provide 20% of Common Open Space for the residents.

Access and Circulation

The property will have two access points on Morgan Lane, a private street which accesses Monterey Way to the west. One access will serve the residence and the other will serve the shop. This will separate the different types of traffic on the site. The extension of Morgan Lane and the sidewalk to Comet Lane will occur with the Multi-Dwelling development of Lot 2.

Pedestrian Connectivity

A walkway connects the residence to the garage and also to the access drive. The amount of pedestrian connectivity is similar to other detached dwellings in the city. Pedestrian traffic is not expected with the shop; therefore, no walkways are provided. A sidewalk linking to Comet Lane will be installed with the extension of Morgan Lane in Phase 2 to provide linkage between the apartments and Comet Lane.

Liahtina

No exterior lighting is being proposed with this plan.

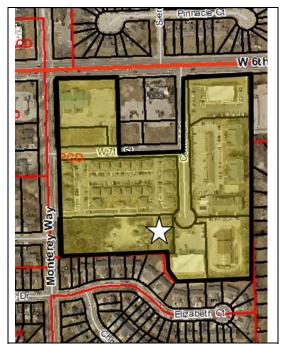


Figure 2. 6th and Monterey Way PCD area, highlighted, and the subject property marked with a star.

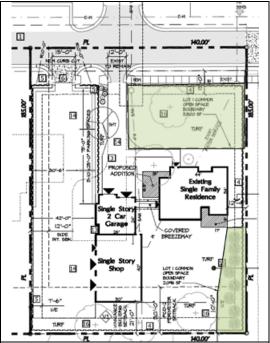


Figure 3. Common Open Space exhibit showing the common open space boundary, shaded in green.

Landscaping

Street trees are not specifically required as the property is adjacent to a private street, but landscaping should be provided to create an attractive streetscape along Morgan Lane. Trees are located along Morgan Lane and a six foot wood fence currently surrounds the residence on the sides, rear, and along Morgan Lane. New fencing will be installed along the shop's drive and access to screen it from the street.

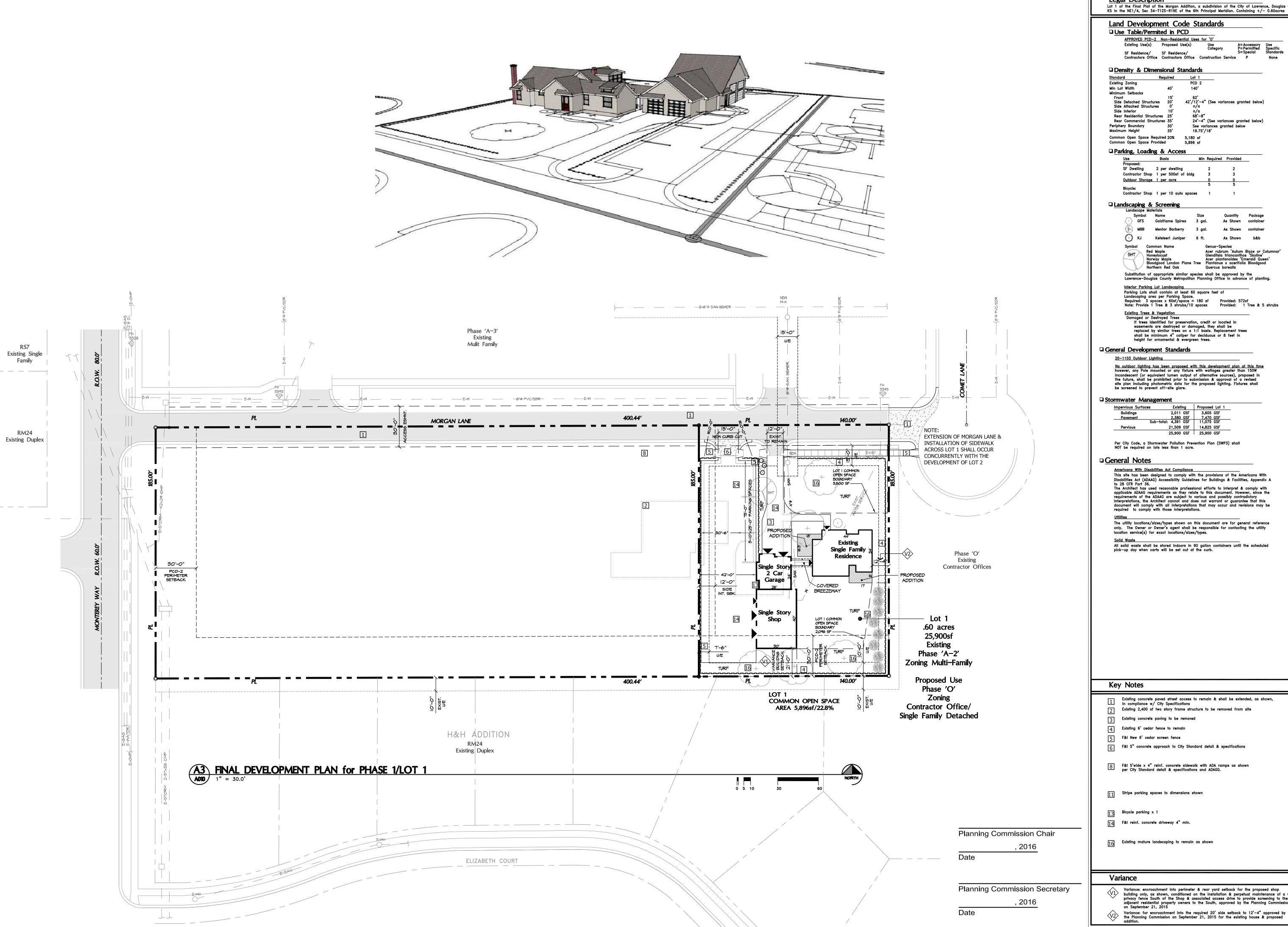
BUILDING ELEVATIONS

The commercial use, the *Contractor Sales and Service* use, will be located behind the garage and will be built with a peaked roof and will have a residential character. The house and the shop appear as a unified development. The structures will have a maximum height of 19.75 ft.



Conclusion

The development proposed with this Final Development Plan conforms with the approved Preliminary Development Plan and is compliant with the provisions of the Development Code.



Lot 1 of the Final Plat of the Morgan Addition, a subdivision of the City of Lawrence, Douglas Co KS in the NE1/4, Sec 34-T12S-R19E of the 6th Principal Meridian. Containing +/- 0.60acres Land Development Code Standards □ Use Table/Permited in PCD APPROVED PCD-2 Non-Residential Uses for '0' Existing Use(s) Proposed Use(s) □ Density & Dimensional Standards Minimum Setbacks Side Interior 10' Rear Residential Structures 25' Rear Commercial Structures 35' ☐ Parking, Loading & Access Basis SF Dwelling 2 per dwelling Contractor Shop 1 per 500sf of bldg Outdoor Storage 1 per acre Contractor Shop 1 per 10 auto spaces ☐ Landscaping & Screening Red Maple
Honeylocust
Norway Maple
Bloodgood London Plane Tree
Northern Red Oak

Acer rubrum 'Autum Blaze or Columnar'
Glenditsia triancanthos 'Skyline'
Acer plantanoides 'Emerald Queen'
Plantanus x acerifolia Bloodgood
Quercus borealls Substitution of appropriate similar species shall be approved by the Lawrence—Douglas County Metropolitan Planning Office in advance of planting. Interior Parking Lot Landscaping Parking Lots shall contain at least 60 square feet of Landscaping area per Parking Space.

Required: 3 spaces x 60sf/space = 180 sf Provided: 572sf Note: Provide 1 Tree & 3 shrubs/10 spaces Provided: 1 Tree Existing Trees & Vegetation

Damaged or Destroyed Trees easements are destroyed or damaged, they shall be replaced by similar trees on a 1:1 basis. Replacement trees shall be minimum 4" caliper for deciduous or 8 feet in height for ornamental & evergreen trees. ☐ General Development Standards 20-1103 Outdoor Lighting No outdoor lighting has been proposed with this development plan at this time however, any Pole mounted or any fixture with wattages greater than 150W incandescent (or equivalent lumen output of alternative sources), proposed in the future, shall be prohibited prior to submission & approval of a revised site plan including photometric data for the proposed lighting. Fixtures shall be screened to prevent off-site glare.
 Existing
 Proposed Lot 1

 2,011 GSF
 3,605 GSF
 Impervious Surfaces <u>Pavement</u> 21,509 GSF 14,825 GSF 25,900 GSF 25,900 GSF Per City Code, a Stormwater Pollution Prevention Plan (SWP3) shall NOT be required on lots less than 1 acre. □ General Notes Americans With Disabilities Act Compliance This site has been designed to comply with the provisions of the Americans With to 28 CFR Part 36. The Architect has used reasonable professional efforts to interpret & comply with applicable ADAAG requirements as they relate to this document. However, since the requirements of the ADAAG are subject to various and possibly contradictory interpretations, the Architect cannot and does not warrant or guarantee that this document will comply with all interpretations that may occur and revisions may be required to comply with those interpretations. The utility locations/sizes/types shown on this document are for general reference only. The Owner or Owner's agent shall be responsible for contacting the utility All solid waste shall be stored indoors in 90 gallon containers until the scheduled pick—up day when carts will be set out at the curb. Key Notes Existing concrete paved street access to remain & shall be extended, as shown, in compliance w/ City Specifications Existing 2,400 sf two story frame structure to be removed from site Existing concrete paving to be removed Existing 6' cedar fence to remain 8 F&l 5'wide x 4" reinf. concrete sidewalk with ADA ramps as shown per City Standard detail & specifications and ADAGG. Stripe parking spaces to dimensions shown 14 F&I reinf. concrete driveway 4" min. Existing mature landscaping to remain as shown

Variance Variance: encroachment into perimeter & rear yard setback for the proposed shop building only, as shown, conditioned on the installation & perpetual maintenance of a 6' privacy fence South of the Shop & associated access drive to provide screening to the adjacent residential property owners to the South, approved by the Planning Commission on September 21, 2015

Allen

24'-4" (See variances granted below)

See variances granted below

Min Required Provided

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| | PROJECT # 1511 DATE: 20JULY15 DRAWN BY: CHECKED BY: REVISIONS: 24AUG15

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PHASE 1/LOT 1 **FINAL** DEVELOPMENT **PLAN**

Sheet

of Sheets

PLANNING COMMISSION REPORT Non-Public Hearing Item

PC Staff Report 05/23/2016

ITEM NO. 2 FINAL DEVELOPMENT PLAN FOR BAUER FARM ZAXBY'S; 4661 BAUER FARM DR (SLD)

FDP-16-00104: Consider a Final Development Plan for Zaxby's, a Fast Order Food with Drive-In, located at 4661 Bauer Farm Dr. Submitted by Carter Engineering Consultants, Inc., for JULI, LLC, contract purchaser, Free State Holdings Inc, property owner of record.

STAFF RECOMMENDATION: Planning staff recommends approval of the Final Development Plan based upon the findings of fact presented in the body of the Staff Report, subject to the following conditions:

- 1. Execution of a site plan performance agreement.
- 2. Submission of a revised Final Development Plan to show the following notes and changes:
 - a. Revise sanitary sewer service line location to meet City design standards per the approval of the City Utility Engineer.
 - b. Show additional pedestrian ramps for sidewalk connections between building and Bauer Farm Drive.
 - c. Show the deed book and page of separately recorded document for shared access and cross access for this property.
- 3. Filing of the Minor subdivision with the Register of Deeds Office.

Attachment:

A. Proposed Final Development Plan.

Design Standards to Consider

- Consistency with approved Preliminary Development Plan, PDP-14-00055.
- Consistency with Article 10 of the 1966 Zoning Code and the standards contained in the 2006 Land Development Code.

KEY POINTS:

- Final Development Plans for projects with preliminary approvals prior to the adoption of the 2006 Land Development Code require Planning Commission approval.
- A revised Preliminary Development Plan for Bauer Farms [PDP-15-00529] was approved by the Planning Commission on December 14, 2015 and by the City Commission on January 6, 2016.

ASSOCIATED CASES

- PDP-15-00529; Bauer Farm Preliminary Development Plan.
- PF -15-00094; Bauer Farm 7th Plat
- MS-16-00126: Minor Subdivision

OTHER ACTION REQUIRED

Applicant shall apply for and obtain Building Permits prior to development.

PUBLIC COMMENT

No public comment was received prior to the printing of this staff report.

PLANS AND STUDIES REQUIRED

- Traffic Study a Revised Traffic Impact Study (TIS) was provided and accepted with the Preliminary Development Plan. An updated letter was received on March 16, 2016 with this application. Summary was accepted by staff.
- Downstream Sanitary Sewer Analysis The downstream sanitary sewer analysis and cover letter dated January 20, 2010 from Landplan Engineering was previously accepted for the Bauer Farm Development Plan to satisfy the criteria required for the DSSA as outlined in Administrative Policy 76. The proposed project meets the land use description used in the DSSA for this location.
- *Drainage Study* The drainage study dated 2-20-2014 meets the specified requirements and is approved.
 - o City of Lawrence Right of Way Permit will be needed to tie the proposed storm sewer line into the south side of the existing junction box on Bauer Farm Drive.
- Retail Market Study Not applicable to application.

GENERAL INFORMATION

Current Zoning and Land Use: PCD [Bauer Farm] (Planned Commercial Development)

District; developing mixed use project.

Surrounding Zoning and land use: To the north, east and west:

PCD [Bauer Farm] (Planned Commercial Development) District. A *Theater* is located on the north side of Bauer Farm Drive. A *Fast Order Food, Drive-In* use building located to the west known as Burger King. Undeveloped

lot is located to the east

To the south:

PCD [Westgate] (Planned Commercial Development) located on the south side of W 6th Street; existing multi-

tenant retail commercial building.

STAFF SUMMARY

This Final Development Plan proposes the development of a 3,662 SF single story *Fast Order Food, Drive-In* on approximately 39,940 sq ft parcel [Proposed Lot 1, Bauer Farm Eighth Plat] located on the south side of Bauer Farm Drive. This proposed use is consistent with the approved Preliminary Development Plan.

STAFF REVIEW

The Bauer Farm Development was proposed and planned as a New Urbanism type of development which would utilize several Traditional Neighborhood Design concepts. The waivers which were approved by the Planning Commission were necessary to accommodate these TND concepts such as smaller lots, narrower rights-of-way, and reduced peripheral setbacks. Several Development Plans have been approved for both the commercial and residential portions of the development. The Planning Commission approved revised waivers with their consideration of the 2015 revised Preliminary Development Plan, PDP-15-00529, based on the type of development being proposed. The proposed use is located on the south

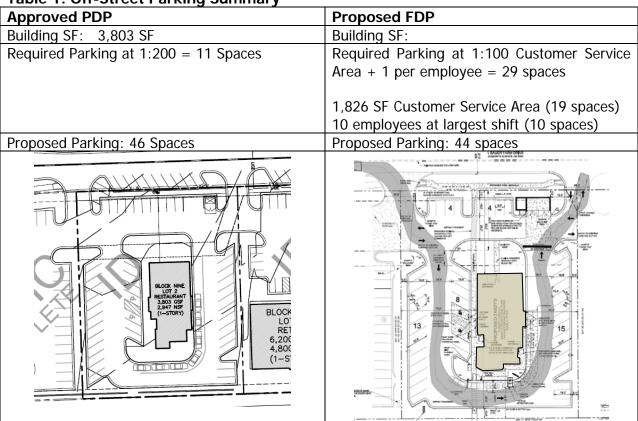
side of Bauer Farm Drive. Uses on the south side were intended for more auto-oriented access with pedestrian sidewalks connecting uses within the development.

A development item related to this application is a pending Minor Subdivision. The Minor Subdivision has been submitted and is being processed. The subdivision review shall be completed and the document recorded prior to the issuance of a building permit for this property.

Off-Street Parking

In the pre-2006 Zoning Code off-street parking is calculated at a different ratio and an alternative methodology was specifically applicable to Planned Developments. Per the 1966 Code, parking was based on net square feet of building space rather than gross square feet in the current Code. Additionally, Planned Development off-street parking was calculated at a rate of 1 space per 200 net square feet regardless of use. The following table summarizes the off-street parking shown on the approved Preliminary Development Plan and the proposed Final Development Plan.

Table 1: Off-Street Parking Summary



This plan provides a parking summary based on the current parking design requirements. The proposed development will result in excess spaces but two fewer than the approved Preliminary Development Plan.

The approved Preliminary Development Plan shows parking along Bauer Farm Drive. This parking is not shown on this proposed Final Development Plan. The on street parking will be provided by the developer of Bauer Farm at a future date with the development of the adjacent

Item No. 2 - 4

lot to the east. The proposed development exceeds the required off-street parking for this use and is consistent with the approved Preliminary Development Plan.

REVIEW CRITERIA

The Planning Commission is responsible for the final decision-making of a Final Development Plan of the pre-2006 Code. Applications submitted for Final Development Plan approval must be in substantial compliance with the approved Preliminary Development Plan. This section of the report provides a summary of the proposed development's "substantial compliance the approved Preliminary Development Plan" based on the 1966 Zoning Code for which it was originally approved and for the current Land Development Code standards.

Section 20-1013 (b) of the 1966 Zoning Code states that a plan submitted for final approval shall be in substantial compliance with the plan previously given preliminary approval and that modifications may not:

- 1. Increase gross residential density or intensity by more than 5% or involve a reduction in the area set aside for common open space.
- 2. Increase the total floor area proposed for non-residential or commercial uses by more than 10%.
- 3. Increase the total ground area covered by a building by more than 5% or involves a substantial change in the height of buildings.

The proposed Final Development Plan:

- 1. Does not include any residential use or include a reduction in common open space within the phase or development.
- 2. The proposed building is slightly smaller than the building shown on the Preliminary Development Plan.
 - a. The approved plan included a 3,803 SF building.
 - b. The proposed plan includes a 3,652 SF building.
- 3. The proposed final development plan does not increase the ground covered by a building. The proposed building is smaller than the building shown on the approved Preliminary Development Plan. The building height is not altered by this request.

The proposed development meets the requirements of being in substantial compliance with the plan previously given preliminary approval. The approved Preliminary Development Plan included a *Fast Order Food, Drive-In* use.

This application complies with the 1966 Zoning Code for consideration of a Final Development Plan. A new public hearing is not required. The 1966 Code requires the Planning Commission to approve a Final Development Plan as the determining authority.

Drive thru Uses in Bauer Farm Planned Development

A significant feature of the Traditional Neighborhood Design concept is the limited number of vehicle oriented uses, specifically drive-thru uses. The development plan, including all the areas zoned PCD, PRD and POD, was modified in 2008 to restrict drive-thru uses to the following mix:

- Three restaurant uses (3)
- One Bank (1)
- One other non-restaurant such as a pharmacy (1)

The plan included automotive service uses that did not count toward these "drive-thru" restrictions.

The CVS, 4841 Bauer Farm Drive, and Taco Bell, 4721 Bauer Farm Drive, buildings were the first in the area to be developed with a drive-thru. To date, two other restaurant uses have been developed with a drive-thru: 4701 Bauer Farm Drive, Starbucks and 4671 Bauer Farm Drive, Burger King.

The Preliminary Development Plan was modified in 2015 to increase the total number of drivethru uses, excluding automotive service uses, within the Bauer Farm Planned Development (inclusive).

- Four restaurant uses (4)
- Two bank uses (2)
- One other non-restaurant such as a pharmacy (1)

Development of this site will result in the final allowable drive-thru restaurant use for the Bauer Farm Planned Development. One remaining non-restaurant drive-thru use (bank) may be developed on the northwest corner of Folks Road and W. 6th Street (4431 Bauer Farm Drive).

Common Open Space

A minimum of 20% of the area in the Planned Development is required to be placed into Common Open Space. The majority of common open space for the Bauer Farm Planned Development is located within the residential portions of the project. Open space for individual commercial lots is located along the perimeter of the lots and along the street frontages. The proposed development includes 22% of the total lot as open space.

Access and Circulation

Vehicular access to this site is provided from Bauer Farm Drive and is consistent with that approved on the Preliminary Development Plan. Access to the site is shared with the adjacent properties to the east and west. Access driveways are centered on the shared property lines.

Pedestrian Connectivity

A pedestrian walkway connects the building with the sidewalks on Bauer Farm Drive. The walkway provides access to the building entrance.

Lighting

A photometric plan including a point by point illumination array has been provided. The lighting levels and fixture type are compliant with the standards in Section 20-1103(d) (3).

Landscaping

Peripheral boundary buffer yards are not required between PUDs in the same development. Parking lots are also required to be screened. The original Preliminary Development Plan included a screening wall located along the south side of the commercial properties along W. 6th Street. This screening wall was not constructed along the Burger King site or where the buildings west of Champion Lane abut W. 6th Street.



Figure 1: Location of screening walls along W. 6th Street



Figure 2: Screening walls west of Champion Lane



Figure 3: Landscape screening east of Champion Lane East of Champion Lane.

Use of landscaping to screen the parking lot is an acceptable solution and will provide a unifying appearance along W. 6th Street east of Champion Lane.

The plan notes that all mechanical equipment will be building mounted and will be screened per City Code.

Solid Waste is accommodated in a screened storage area located along Bauer Farm Drive as shown on the plan. The location and design have been approved by the City Solid Waste Division.

Commercial Design Guidelines

Planned Developments provide the developer greater flexibility to achieve their desired design and development. They also allow for greater control over the aesthetics of a site. Proposed

building elevations include variation in material, building planes, a varied roof line and a strongly defined main entry. The proposed building meets the intent of the Design Guidelines.

Infrastructure Improvements

Much of the needed infrastructure exists for this site. The applicant will be responsible for connecting service lines between the building and the water and sanitary sewer main lines. Sidewalk is located along Bauer Farm Drive and W. 6th Street. This project will relocate the sidewalk along Bauer Farm Drive in anticipation of construction of the on-street parking along the south side of Bauer Farm Drive. The City Traffic Engineer noted that "Pedestrian pathway to Bauer Farm Drive needs ramps where appropriate." The plan should be revised to show the ramps as noted.

PANTID DAYED VILLOW LINE

PANTID DAYED VILLO

Figure 4: Location for additional pedestrian ramps

The on-street parking as noted will be a future site improvement made by the development group with the development of the adjacent lot to the east.

The City Stormwater Engineer has noted that a right-of-way permit will be needed to connect to the City Storm Sewer. This requirement will be managed through the building permit process.

Conclusion

The development proposed with this Final Development Plan is consistent with the intent of the development that was approved with the Preliminary Development Plan and is compliant with the provisions of the Development Code.

EARTHWORK SPECIFICATIONS:

- 1. CLEARING AND GRUBBING SHALL CONSIST OF CLEARING THE SURFACE OF THE GROUND OF THE DESIGNATED AREAS OF ALL TREES, LOGS, SNAGS, BRUSH, UNDERGROWTH, HEAVY GROWTH OF GRASS, WEEDS, FENCE STRUCTURES,
- 2. BURNING ON SITE IS NOT PERMITTED WITHOUT WRITTEN APPROVAL OF THE LOCAL GOVERNING AUTHORITIES HAVING JURISDICTION
- EXISTING TREES AND AREA OUTSIDE OF GRADING LIMITS LINE TREES AND VEGETATION TO BE SAVED SHALL BE PROTECTED FROM DAMAGE BY BE SAVED SHALL BE DESIGNATED BY THE OWNER. NO TREES ARE TO BE REMOVED FROM THE AREA OUTSIDE THE LIMITS OF GRADING OR FROM SPECIFICALLY DESIGNATED AREAS WITHIN THE CONSTRUCTION AREAS. IF, IN THE EACH DAMAGED TREE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR ALL
- WITHIN THE AREA UPON WHICH FILL IS TO BE PLACED SHALL BE STRIPPED OR BE OTHERWISE REMOVED BEFORE THE FILL OPERATION IS STARTED. IN NO CASE SHALL UNSUITABLE MATERIAL REMAIN IN OR UNDER THE FILL AREA. SLOPED GROUND SURFACE STEEPER THAN ONE VERTICAL TO FOUR HORIZONTAL, ON ASTM D-698). MOISTURE CONTENT SHALL BE WITHIN 3 PERCENT OF THE OPTIMUM MOISTURE CONTENT. PROOF ROLL THE AREAS TO BE FILLED OR ON WHICH STRUCTURES ARE TO BE PLACED. A LOADED DUMP TRUCK OR OTHER RUBBER TIRED EQUIPMENT SHALL BE USED PROOF ROLLING. OVERLAPPING PASSES OF A VEHICLE SHOULD BE MADE ACROSS THE SITE IN ONE DIRECTION AND THEN PERPENDICULAR TO THE ORIGINAL DIRECTION OF ROLLING.
- 3.2. ANY YIELDING, PUMPING OR SOFT AREAS SHOULD BE CUT OUT AND REPLACED WITH FILL COMPACTED AS DESCRIBED HEREIN.
- 3.3. THE PROPOSED FILL SHOULD BE LIMITED TO SOILS CLASSIFIED IN ACCORDANCE WITH ASTM D-2487 AS GM, GC, SW, SM, SC, ML AND CL, SOIL CLASSIFIED AS PT. OH OL. CH AND MH ARE NOT SATISFACTORY AS COMPACTED FILL
- 4. FILLS AND EMBANKMENTS SHALL BE CONSTRUCTED AT THE LOCATIONS AND TO THE LINES AND GRADES INDICATED ON CONSTRUCTION PLANS. THE SLOPE SHALL NOT EXCEED 2 FOOT HORIZONTAL TO 1 FOOT VERTICAL. (3 FOOT HORIZONTAL TO 1 FOOT VERTICAL IN THE PUBLIC RIGHT OF WAY) THE COMPLETED FILL SHALL DIMENSIONS GREATER THAN 6 INCHES (4 INCHES IN TRENCHES FOR UTILITIES). MAXIMUM DRY DENSITY (STANDARD PROCTOR). EACH LIFT SHALL BE ROLLED TIRED DUMP TRUCK, SCRAPER OR LOADER. IF THE SOIL IS TOO DRY, A WATER TRUCK WITH SPREADER BAR OR SPRAY HOSE SHALL BE USED TO BRING THE SOIL TO THE PROPER MOISTURE RANGE. THE WATER SHALL BE THOROUGHLY AND PROPERLY MIXED WITH THE SOIL PRIOR TO COMPACTION.
- 3.5. STORM DRAIN PIPES SHALL BE PLACED ON FIRM BOTTOM AND HAND TAMPED TO SAFE UP THE PIPE. A CUSHION OF SOIL SHALL BE TAMPED ABOVE THE CROWN O THE PIPE IN ACCORDANCE WITH THE PIPE MANUFACTURER'S RECOMMENDATIONS SO THAT THE HEAVIER COMPACTION EQUIPMENT CAN THEN BE USED TO BRING THE SOIL TO A DENSITY AS DESCRIBED ABOVE FOR FILL AREAS.
- 3.6. IF SOILS INVESTIGATION REPORT IS PROVIDED, THEN FOLLOW THE RECOMMENDATIONS OF THE REPORT IF THEY EXCEED THE RECOMMENDATIONS

OF THESE SPECIFICATIONS. TOPSOIL

- 1.1. UNLESS OTHERWISE SPECIFIED, AREAS DESIGNATED FOR GRADING OPERATIONS THAT CONTAIN A BLANKET OF TOPSOIL SHALL BE STRIPPED AND PLACED IN CONVENIENT STOCKPILES FOR LATER USE AS A TOPSOIL BLANKET ON THE NEW GRADED AREAS SPECIFIED HEREIN, OR AS DESIGNATED. TOPSOIL SHALL BE STRIPPED FROM ALL AREAS DESIGNATED TO RECEIVE FILL. THE STRIPPING OF MATERIAL FOR TOPSOIL SHALL BE CAREFULLY DETERMINED AND ONLY THE QUANTITY REQUIRED SHALL BE STOCKPILED. MATERIAL STOCKPILED SHALL BE STORED IN A SATISFACTORY MANNER TO AFFORD PROPER DRAINAGE. WHEN GRADING OPERATIONS PERMIT, INSTEAD OF STOCKPILING, THE TOPSOIL SHALL BE HAULED AND SPREAD DIRECTLY ON THE AREAS DESIGNATED TO RECEIVE
- **ROCK EXCAVATION**
- 5.1. IF ROCK IS ENCOUNTERED, CLEAR AWAY EARTH TO EXPOSE MATERIAL. NOTIFY OWNER AND RECEIVE WRITTEN INSTRUCTIONS PRIOR TO EXCAVATION. REMOVE ROCK TO A DEPTH OF 6 INCHES BELOW AND 8 INCHES ON EACH SIDE OF PIPES IN TRENCHES. A MEASUREMENT OF EXTENT OF ROCK TO BE REMOVED SHALL BE MADE. ROCK EXCAVATION SHALL BE PAID FOR IN ACCORDANCE WITH AGREEMENT WITH THE OWNER.

- THE DRAWINGS AND SPECIFICATIONS ARE INTENDED TO COVER A COMPLETE PROJECT, READY TO USE, AND ALL ITEMS NECESSARY FOR A COMPLETE AND WORKABLE JOB SHALL BE FURNISHED AND INSTALLED. ANY DISCREPANCY SHALL BE IMMEDIATELY REPORTED TO THE OWNER OR HIS REPRESENTATIVE.
- NOTIFY THE INSPECTOR OF THE LOCAL GOVERNING AUTHORITY 24 HOURS BEFORE
- ALL WORK SHALL COMPLY WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL CODES. ALL NECESSARY LICENSES AND PERMITS SHALL BE OBTAINED BY THE CONTRACTOR, AT HIS EXPENSE, UNLESS ALREADY OBTAINED BY THE OWNER. THE CONTRACTOR SHALL COORDINATE LOCATION AND INSTALLATION OF ALL UNDERGROUND UTILITIES AND APPURTENANCES TO MINIMIZE DISTURBING
- THE EXISTING UTILITIES SHOWN ARE FOR THE CONTRACTOR'S CONVENIENCE ONLY THERE MAY BE OTHER UTILITIES NOT SHOWN ON THESE DRAWINGS. THE UTILITIES
- DEVIATIONS FROM THESE PLANS AND SPECIFICATIONS WITHOUT PRIOR CONSENT OF THE ENGINEER AND THE MUNICIPALITY MAY CAUSE FOR THE WORK TO BE UNACCEPTABLE.
- ALL MATERIALS SHALL BE NEW UNLESS USED OR SALVAGED MATERIALS ARE AUTHORIZED BY THE OWNER.
- THE CONTRACTOR SHALL FURNISH AND MAINTAIN ALL NECESSARY BARRICADES AROUND THE WORK AND SHALL PROVIDE PROTECTION AGAINST WATER DAMAGE
- ALL WORK SHALL BE PERFORMED IN A FINISHED AND WORKMANLIKE MANNER TO THE ENTIRE SATISFACTION OF THE OWNER, AND IN ACCORDANCE WITH THE BEST RECOGNIZED TRADE PRACTICES.
- THE CONTRACTOR SHALL PROVIDE SHEETING AND SHORING FOR ALL TRENCH
- CONSTRUCTION IN ACCORDANCE WITH OSHA GUIDELINES.
- PIPES (STORM AND SANITARY SEWER) SHALL BE LAID ON SMOOTH, CONTINUOUS GRADES WITH NO VISIBLE BENDS AT THE JOINTS.
- BEDDING REQUIREMENTS SPECIFIED HEREIN ARE TO BE CONSIDERED AS MINIMUM REQUIRED FOR RELATIVELY DRY STABLE EARTH CONDITIONS. ADDITIONAL BEDDING SHALL BE REQUIRED FOR ROCK TRENCHES TO PROVIDE SUCH ADDITIONAL
- BEDDING AS REQUIRED TO PROPERLY CONSTRUCT WORK. ALL STORM DRAINAGE INLET STRUCTURES SHALL HAVE METAL RING AND COVER FOR ACCESS
- ALL ANGLES SHOWN ARE 90 DEGREES UNLESS SHOWN OTHERWISE ALL GRADES SHOWN ARE FINISHED GRADES. CONTRACTOR SHALL VERIFY
- DIMENSIONS, GRADES, AND EXISTING ELEVATIONS PRIOR TO CONSTRUCTION
- CONCRETE CURBS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS SHOWN ON PLANS. MATERIALS, EQUIPMENT, METHODS OF CONSTRUCTION AND WORKMANSHIP SHALL CONFORM TO STATE D.O.T. STANDARD SPECIFICATIONS.
- ALL CONCRETE SHALL HAVE 3000 PSI COMPRESSIVE STRENGTH AFTER 28 DAYS, WITH A MAXIMUM SLUMP OF FOUR (4) INCHES. UNLESS SPECIFIED OTHERWISE.
- ALL EXPOSED CONCRETE SHALL HAVE A FINE HAIR BROOMED FINISH. 0. PARKING AND DRIVEWAY BASE COURSE AND ASPHALTIC CONCRETE SURFACE AND PRIME MATERIALS, EQUIPMENT, METHODS FOR CONSTRUCTION AND
- WORKMANSHIP SHALL CONFORM TO STATE D.O.T. STANDARD SPECIFICATIONS CONTRACTOR TO FIELD VERIFY ALL STORM, SANITARY, WATER AND OTHER UTILITIES LOCATIONS AND INVERTS PRIOR TO INSTALLATION OF ANY UTILITIES. NOTIFY ENGINEER PRIOR TO PROCEEDING WITH ANY WORK IF DISCREPANCIES
- 22. CONTRACTOR SHALL USE CONCRETE THRUST BLOCKS FOR INSTALLATION OF
- 23. ALL DIMENSIONS ARE MEASURED TO THE BACK OF CURB UNLESS OTHERWISE

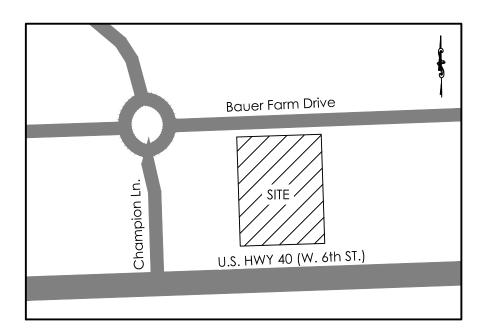
SITE DEVELOPMENT PLANS



FOR

JULI, LLC BAUER FARM PCD 4661 BAUER FARM DRIVE CITY OF LAWERENCE DOUGLAS COUNTY, KANSAS

FEBRUARY 5, 2016



LOCATION MAP SCALE: 1" = 200'

PROJECT NOTES:

OWNER / DEVELOPER: P.O. Box 1439 Watkinsville, GA 30677 Contact: Ben Williams

Tel.: (706) 714-1197

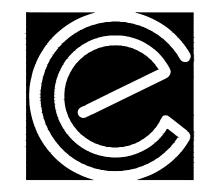
Carter Engineering Consultants, Inc. 3651 Mars Hill Road Suite 2000 Watkinsville, Georgia 30677 Contact: Mark Campbell, P.E. Tel.: (770) 725-1200 e-mail: mark@carterengineering.net

SHEET INDEX

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	EXISTING CONDITIONS & DEMO PLAN
3	SITE PLAN
4	UTILITY PLAN
5	GRADING & DRAINAGE PLAN
6	EROSION CONTROL PLAN
7	STORM PIPE PROFILES
8	STANDARD DETAILS
9	ADA ACCESS PLAN
10	ADA DETAILS
11	LANDSCAPE PLAN
12	OUTDOOR LIGHTING PLAN
A105	FRONT & REAR EXTERIOR ELEVATIONS
A106	DRIVE THRU & RIGHT EXTERIOR ELEVATIONS

REVISION BLOCK

REV. NO.	DATE	DESCRIPTION
Α	02/05/16	INITIAL SUBMITTAL
В	02/29/16	REVISED PER CITY COMMENTS
С	03/16/16	ADDRESS COMMENTS
D	04/29/16	REVISE BOUNDARY PER SURVEY



Carter Engineering Consultants, Inc. 3651 Mars Hill Road Watkinsville, GA 30677 P: 770.725.1200 F: 706.559.7435



24-HOUR CONTACT: Ben Williams (706) 714-1197

SITE DEVELOPMENT PLANS



NOTE: FINAL DEVELOPMENT PLANS MUST BE RECORDED WITH THE DOUGLAS COUNTY REGISTER OF DEEDS OFFICE.

JULI, LLC ZONED: PCD TOTAL SITE AREA: 0.92 ACRES DISTURBED AREA: 0.95 ACRES

SCOTT McCULLOUGH **DIRECTOR, PLANNING &**

DATE **DEVELOPMENT SERVICES**

APPROVED AS A FINAL DEVELOPMENT PLAN UNDER

THE REGULATIONS OF THE CITY OF LAWRENCE & THE

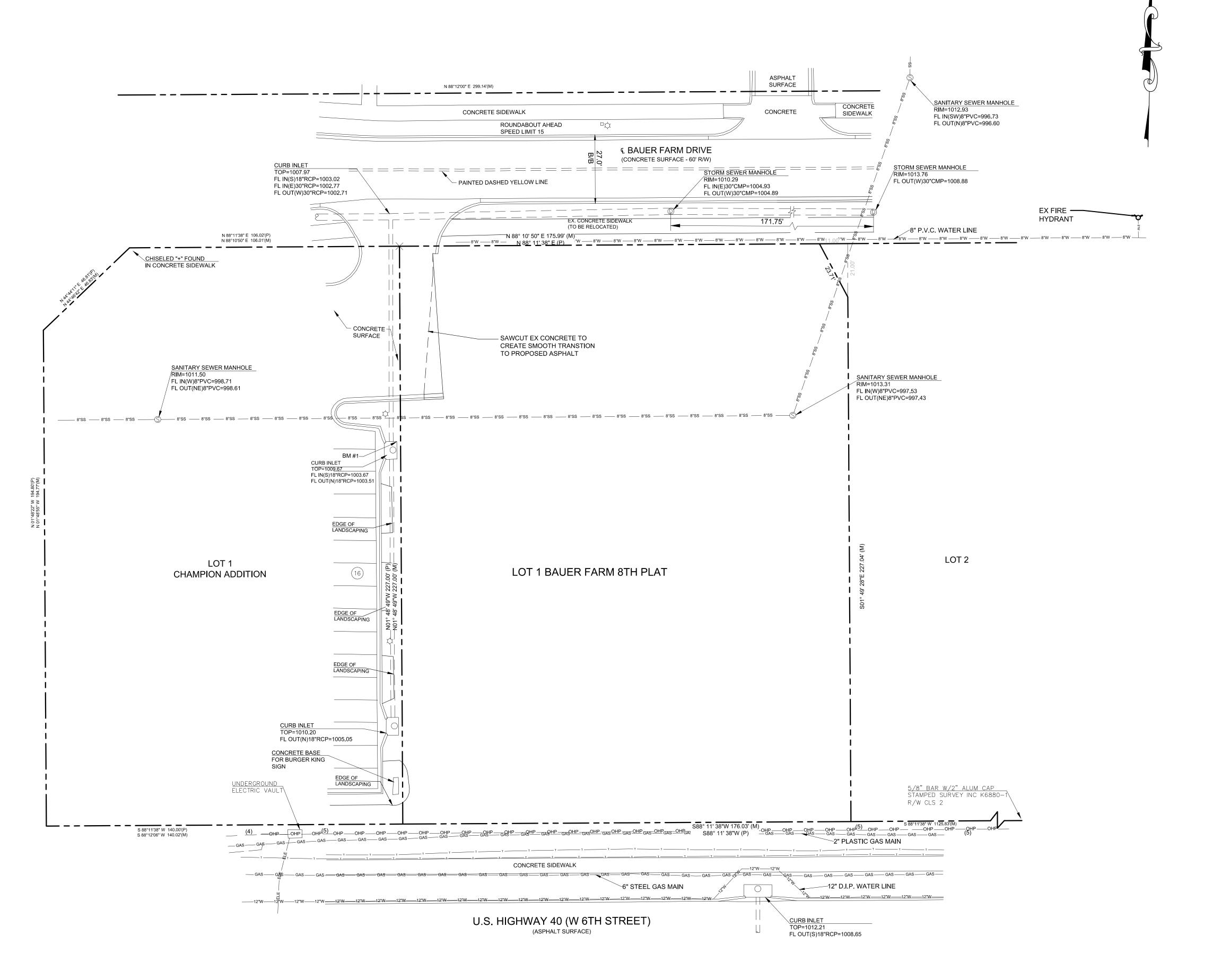
UNINCORPORATED AREAS OF DOUGLAS COUNTY

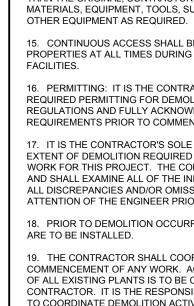
APPROVED AS A FINAL DEVELOPMENT PLAN UNDER THE REGULATIONS OF THE CITY OF LAWRENCE & THE UNINCORPORATED AREAS OF DOUGLAS COUNTY

PLANNING COMMISSIONER

CHAIR

DATE



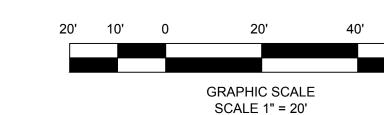


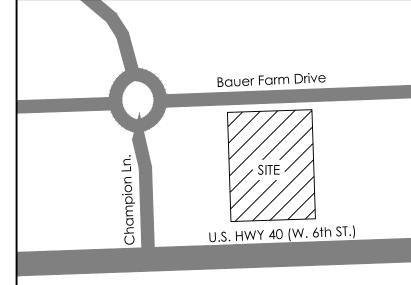
17. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO DETERMINE THE ATTENTION OF THE ENGINEER PRIOR TO BID SUBMITTAL.

18. PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.

19. THE CONTRACTOR SHALL COORDINATE WITH OWNER PRIOR TO COMMENCEMENT OF ANY WORK. ACTUAL REMOVAL AND/OR RELOCATION OF ALL EXISTING PLANTS IS TO BE CONDUCTED BY THE LANDSCAPE CONTRACTOR. IT IS THE RESPONSIBILITY OF THE SITEWORK CONTRACTOR TO COORDINATE DEMOLITION ACTIVITIES WITH THE LANDSCAPE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING AND PRESERVING TREES AS INDICATED BY THE OWNER. 20. CONTRACTOR SHALL LIMIT ALL DEMOLITION ACTIVITY TO THAT AREA

DELINEATED IN THE DRAWING, ALL OTHER EXIST. UTILITIES INCLUDING: STORM DRAINAGE, GAS, ELECTRIC, TELEPHONE, AND WATER & SEWER





LOCATION MAP SCALE: 1" = 200'

DEMOLITION NOTES:

AS SHOWN ON PLANS.

1. THE LOCATIONS OF ALL EXSTING FACILITIES SHOWN ON THIS PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ON SITE LOCATIONS OF EXISTING UTILITIES.

2. THE CONTRACTOR SHALL FURNISH ALL MATERIALS, LABOR, SUPERVISION AND EQUIPMENT REQUIRED FOR THE ORDERLY DEMOLITION AND REMOVAL OF EXISTING STRUCTURES, PAVEMENT AND UTILITIES AS SHOWN ON THE DRAWINGS AND DESCRIBED HEREIN.

3. THE CONTRACTOR IS REQUIRED TO FAMILIARIZE HIMSELF WITH THE STRUCTURES TO BE DEMOLISHED. A BRIEF DESCRIPTION OF THE STRUCTURES IS INCLUDED FOR THE CONTRACTOR'S CONVENIENCE ONLY.

4. THE FOLLOWING LIST OF STRUCTURES REQUIRING DEMOLITION IS INCLUDED FOR THE CONTRACTORS CONVENIENCE ONLY. THE DRAWINGS INDICATE THE SCOPE OF DEMOLITION WHERE DEMOLITION IS REQUIRED.

A. DEMOLITION AND REMOVAL OF EXISTING ONE-STORY BRICK AND GLASS

PAVING, AND CURBING TO DEMOLITION LINE SHOWN. C. REMOVAL OF EXISTING ON SITE ABOVE-GROUND AND UNDERGROUND UTILITIES, INCLUDING REMOVAL AND/OR PLUGGING OF EXISTING UTILITIES

B. DEMOLITION AND REMOVAL OF EXISTING ON SITE ASPHALT, CONCRETE

5. PRIOR TO REMOVAL OF ANY UNDERGROUND SEWAGE TANK AND COMPONENTS FROM SERVICE, CONTRACTOR MUST COMPLETELY DRAIN THE SYSTEMS TO AN APPROVED SANITATION TANK FOR DISPOSAL AT AN APPROVED LOCATION AND IN ACCORDANCE WITH LOCAL & STATE REQUIREMENTS.

6. ALL ON SITE UNDERGROUND STRUCTURES AND PIPING MUST BE COMPLETELY REMOVED AND OVER-EXCAVATED BY A MINIMUM OF 12" BENEATH THE STRUCTURES. CONTRACTOR SHALL USE APPROVED FILLING MATERIAL FOR FILLING THESE AREAS. FILL SHALL BE PLACED IN LOOSE LIFTS NOT EXCEEDING 8" IN THICKNESS AND COMPACTED TO AT LEAST 98% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM D-)698.

7. ALL EXISTING STRUCTURES, PAVEMENTS, SLABS, FOUNDATIONS, STEPS AND OTHER EXISTING FEATURES INDICATED ON THE DRAWINGS TO BE REMOVED SHALL BE DEMOLISHED AND REMOVED BY THE CONTRACTOR. REMOVE NO STRUCTURE SUBSTANTIALLY AS A WHOLE. DEMOLISH COMPLETELY ON THE PREMISES.

8. ALL EXISTING SEWERS, PIPING, UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES. GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES AND CAP ALL LINES BEFORE PROCEEDING WITH THE WORK.

9. ELECTRICAL, TELEPHONE, CABLE AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY PRIOR TO COMMENCEMENT OF CONSTRUCTION. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS A NECESSITY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE.

10. PROVIDE ADEQUATE PROTECTION FOR PERSONS AND PROPERTY AT ALL TIMES. EXECUTE THE WORK IN A MANNER TO AVOID HAZARDS TO PERSONS AND PROPERTY AND PREVENT INTERFERENCE WITH THE USE OF AND ACCESS TO ADJACENT BUILDINGS. STREETS AND SIDEWALKS SHALL NOT BE BLOCKED BY DEBRIS AND EQUIPMENT.

11. AIR HAMMERS OR OTHER DEVICES WILL BE PERMITTED ON EXTERIOR

12. CONTRACTOR MUST STOP OPERATION AND NOTIFY THE OWNER FOR PROPER DIRECTION IF ANY ENVIRONMENTAL OR HEALTH RELATED CONTAMINATE IS ENCOUNTERED DURING THE DEMOLTION/EXCAVATION PROCESS.

A. REMOVE AND LEGALLY DISPOSE OF ALL OTHER RUBBISH, RUBBLE, AND DEBRIS. COMPLY WITH ALL APPLICABLE LAWS AND REGULATIONS GOVERNING DISPOSAL OF WASTE AND DEBRIS. 14. PAVEMENT REMOVAL

A. WHERE EXISTING PAVEMENT IS TO BE REMOVED, SAW-CUT THE SURFACING LEAVING A UNIFORM AND STRAIGHT EDGE WITH A MINIMUM OF DISTURBANCE TO THE REMAINING ADJACENT SURFACING. IF CONSTRUCTION RESULTS IN RAVELING OF THE SAW-CUT SURFACE, RECUT BACK FROM THE RAVELED EDGE PRIOR TO RESTORATION.

B. WHERE EXISTING PAVEMENT, CURB, CURB AND GUTTER, SIDEWALK, DRIVEWAY OR VALLEY GUTTER IS REMOVED FOR THE PURPOSE OF CONSTRUCTING OR REMOVING BOX CULVERTS, PIPE, INLETS, MANHOLES, APPURTENANCES, FACILITIES OR STRUCTURES, SAID PAVEMENT, ETC., SHALL BE REPLACED AND RESTORED IN EQUAL OR BETTER CONDITION THAN THE ORIGINAL, CONTRACTOR SHALL PROVIDE ALL NECESSARY LABOR, MATERIALS, EQUIPMENT, TOOLS, SUPPLIES, AND

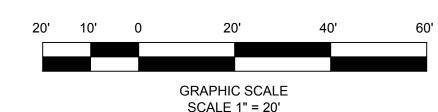
15. CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING

16. PERMITTING: IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN ANY REQUIRED PERMITTING FOR DEMOLITION FROM RESPONSIBLE REGULATIONS AND FULLY ACKNOWLEDGE AND COMPLY WITH ALL REQUIREMENTS PRIOR TO COMMENCING DEMOLITION WORK.

EXTENT OF DEMOLITION REQUIRED IN ORDER TO PERFORM THE CONTRACT WORK FOR THIS PROJECT. THE CONTRACTOR SHALL CONDUCT SITE VISITS AND SHALL EXAMINE ALL OF THE INFORMATION WITHIN THESE DOCUMENTS: ALL DISCREPANCIES AND/OR OMISSIONS SHALL BE BROUGHT TO THE

SHALL BE PRESERVED & PROTECTED. 21. A SEPARATE DEMOLITION PERMIT IS REQUIRED FOR THE DEMOLITION

OF THE ACTUAL BUILDING.



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5/3/16



3651 Mars Hill Road, Suite 2000

CONSULTANTS

Watkinsville, GA 30677 P: 770.725.1200 F: 770.725.1204 www.carterengineering.net

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PROJECT NAME:

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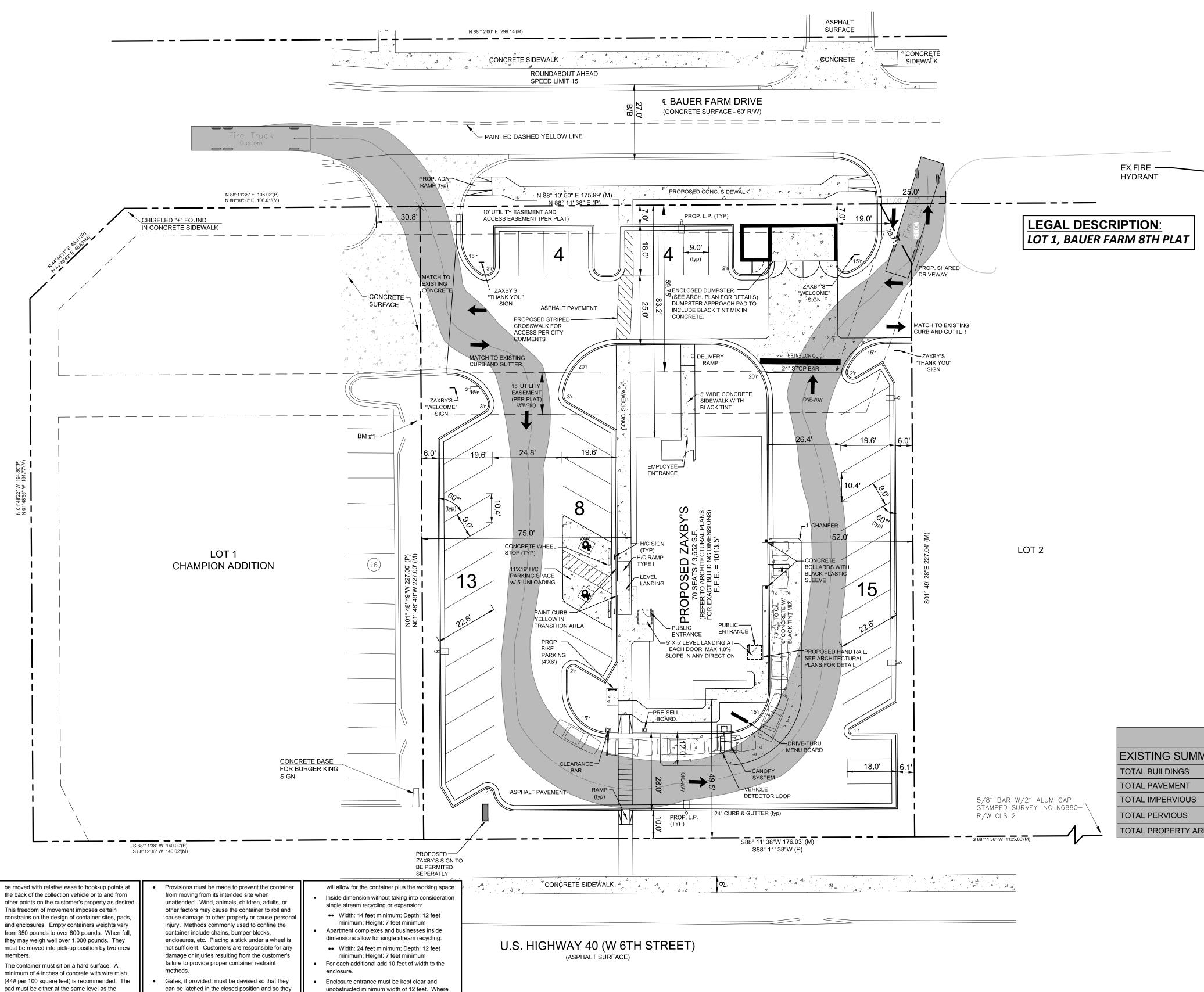
SHEET TITLE: **EXISTING CONDITIONS & DEMO PLAN**

SHEET NUMBER:

PROJECT NUMBER:

15135ZAX

02/05/16



OUMPSTER ENCLOSURE NOTES:

container weight.

clearance to 21 feet.

Refuse containers are serviced by large, heavy

location of the container. The customer bears full

driveway strength. The City is not responsible for

Approach lanes to the container sites must provide

Removal of snow and ice to permit safe approach

to the container is the customer's responsibility.

This is a particular if the approach is not level. It is

the customer's responsibility to remove snow and

ice from within enclosure and from approaches to

Enclosures are designed for trash and recycling

locations for grease bins and other items or make

accommodate other items such as grease bins so

the necessary adjustments to the enclosure to

the solid waste service will not be impeded and

Front load trucks are not highly maneuverable.

very limited visibility directly behind the truck.

These limitations should be considered when

They are operated by one-person crew and have

selecting sites for containers to prevent property

damage and injury. Front load containers are not

equipped with wheels; therefor, these containers

cannot be moved except by use of the refuse

collection truck or other heavy equipment. Rear

loaders containers used by the City are equipped

with four, non-locking, swivel casters so they can

that equipment and personnel is not at risk.

dumpsters ONLY. Plan for other appropriate

collection vehicle or a shallow grade ramp from

must be provided. Container pads must be level,

or nearly so, to facilitate movement by the crew.

The collection truck must be on a level or nearly

level plane when connecting to the container due

Collection crews will not be able to lift containers

to and from curb height, over bumper blocks, or

gravel. Container pads or enclosure floors must

The approach to the container site should provide

a concrete pad 6" thick with wire mesh (44# per

enclosure entrance out to a distance of 8 feet so

The area over the container and for 20 feet in from

of the container must be clear of overhead

obstructions to a height of 21 feet.

that the truck wheels are adequately supported

100 square feet) from the container front or

pull them through soft dirt, mud, snow, ice, or

have a hard surface.

during the dump process.

to the fixed height of the attachment point.

the surface of the pad to the lot or street level

can be secured in the fully open position while the

enclosure entrance. Gates may be required if the

"bumper posts" per container along the inside of

the back wall to prevent the container from being

pushed into the wall. The front loading collection

system imposes sever visibility limitations on the

driver while he is connecting and disconnecting

the container and while maneuvering the truck

with the container on the pick-up forks. Operating

in hours of darkness or adverse weather further

aggravates this limitation. The "bumper posts"

should be 4" diameter heavy steel posts set in

of each post should be 24" minimum.

concrete at least 4" from the back wall. The height

Containers which will not be in an enclosure but

will be placed near a building wall or fence must

be provided "bumper posts" as described above.

A clear working area of 12" is needed on all sides

of the container. Using the following dimensions

collection vehicle is entering and leaving the

All enclosures must have a minimum of two

enclosure is visible from the street.

gates are required on enclosures, a minimum

Enclosure walls must be protected from impact

• All enclosures must have a minimum of two

• All enclosures must be protected from impact

the inside of the enclosure; OR

bumper posts per container along the inside

of the back wall to prevent the container from

with the container by the provision of either:

••• Properly placed bumper blocks around

••• A striker board, or reinforcement boards

around the inside of the enclosure, 2" x

8" minimum, centered 49.5" above the

with the container (see diagram)

being pushed into the wall.

- SOUTH LINE OF SEC 28

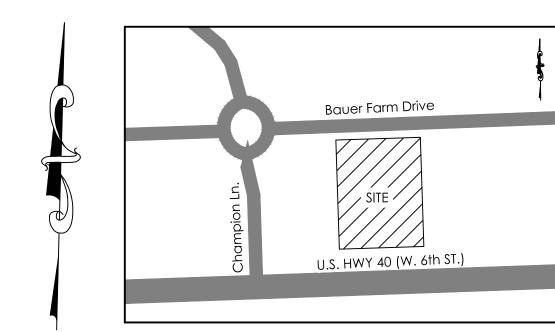
width of 12 feet openings between gate posts are

trucks. These trucks must be driven to the

responsibility for the privately owned lot and

damage to pavement resulting from truck or

12 feet of unobstructed width and overhead



LOCATION MAP SCALE: 1" = 200'

PROJECT NOTES: OWNER / DEVELOPER: JULI, LLC P.O. Box 1439 Watkinsville, GA 30677 Contact: Ben Williams Tel.: (706) 714-1197 ENGINEER: Carter Engineering Consultants, Inc. 3651 Mars Hill Road, Suite 2000 Watkinsville, Georgia 30677 Contact: Mark Campbell, P.E. Tel. (770) 725-1200 mark@carterengineering.net Property located at 4661 Bauer Farm Drive, Lawerence, KS. Current Zoning: PCD

Setbacks: Front 15' Rear 35' Proposed use is a Fast Order Food, Drive-in

Proposed building height is 20'

Boundary and topographical information obtained from field run survey by KAW Valley Engineering dated: January 8, 2016 Phone: (865) 689-6169

Project tract contains 0.92 acres, disturbed area = 0.95 acres

Contour interval is 1 foot (NAVD 88)

The property shown hereon does not lie within a 100 year flood plain according to Flood Insurance Rate Map # 20045CO156E dated: September 2, 2015.

The underground utilities shown hereon have been located from field information and existing drawings. The surveyor nor engineer warrants that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor nor engineer warrants that the underground utilities shown are in the exact location indicated. The surveyor nor engineer has physically located all the underground utilities.

It is the responsibility of the contractor to field locate all utilities prior to commencing work and notify engineer if a discrepancy is found.

The contractor shall verify the invert elevations of all existing storm and sanitary sewer structures prior to commencement of storm and sanitary sewer construction.

UNDERGROUND UTILTIES DISCLAIMER

Information regarding the reputed presence, size, character and location of existing underground utilities and structures related to underground utilities is shown heron. There is no certainty of the accuracy of this information and it shall be considered in that light by those using this drawing. The location and arrangement of underground utilities and structures related to underground utilities shown hereon may be inaccurate and utilities and structures related to underground utilities not shown may be encountered. The owner, his employees, his consultants and his contractors shall hereby distinctly understand that the surveyor is not responsible for the correctness or sufficiency of this information regarding the underground utilities and structures related to underground utilities shown hereon.

PROPERTY SURFACE SUMMARY						
EXISTING SUMMARY		SUMMARY AFTER PROJECT COMPLETION				
TOTAL BUILDINGS	0 S.F.	TOTAL BUILDINGS	3,652 S.F.			
TOTAL PAVEMENT	907 S.F.	TOTAL PAVEMENT	27,508 S.F.			
TOTAL IMPERVIOUS	907 S.F.	TOTAL IMPERVIOUS	31,160 S.F.			
TOTAL PERVIOUS	38,918 S.F.	TOTAL PERVIOUS	8,665 S.F.			
TOTAL PROPERTY AREA	39,825 S.F.	TOTAL PROPERTY AREA	39,825 S.F.			

PARKING DATA:

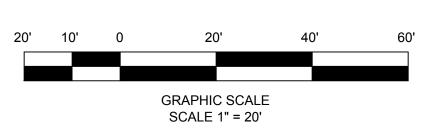
- PARKING SPACES REQUIRED: 1 PER 100 S.F. OF C.S. AREA PLUS 1 PER EMPLOYEE -1826 S.F. / 100 = 18.26 = 19 SPACES - 10 EMPLOYEES BASED ON LARGEST SHIFT TOTAL REQUIRED = 29 SPACES

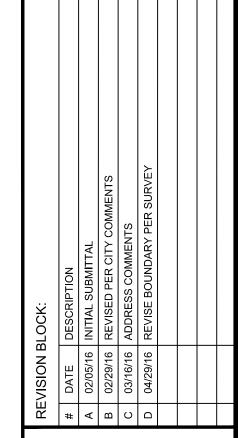
44 SPACES PROPOSED (42 regular spaces provided) (2 handicap spaces provided)

PAVEMENT DISCLOSURE: THE CITY WILL NO BE HELD LIABLE FOR DAMAGE TO PAVEMENT FROM TRUCKS

NOTE:
NO OUTDOOR DINING AREA PROPOSED AS PART OF THIS PROJECT.

Know what's below Call before you dig dial 811







5/3/16

CONSULTANTS 3651 Mars Hill Road, Suite 2000

ENGINEERING

Watkinsville, GA 30677 P: 770.725.1200 F: 770.725.1204 www.carterengineering.net

PLAN: OPMENT

SITE



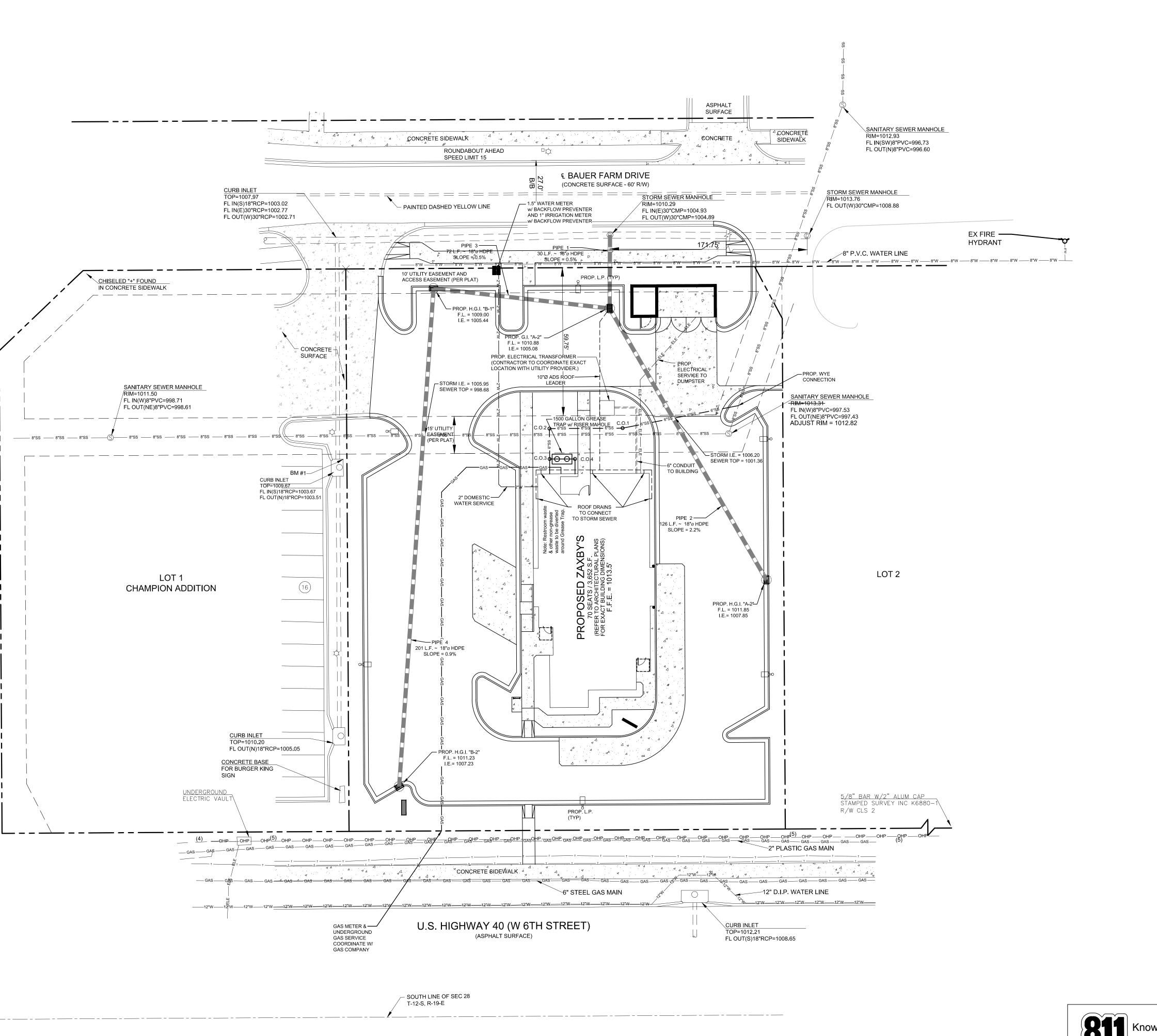
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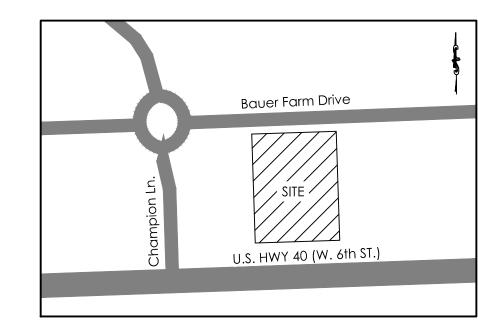
SITE PLAN

SHEET NUMBER:

PROJECT NUMBER: 15135ZAX

02/05/16





LOCATION MAP SCALE: 1" = 200'

PROJECT NOTES: OWNER / DEVELOPER: JULI, LLC P.O. Box 1439 Watkinsville, GA 30677 Contact: Ben Williams Tel.: (706) 714-1197

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Contour interval is 1 foot (NAVD 88)

physically located all the underground utilities.

The property shown hereon does not lie within a 100 year flood plain according to Flood Insurance Rate Map # 20045CO156E dated: September 2, 2015.

The underground utilities shown hereon have been located from field information and existing drawings. The surveyor nor engineer warrants that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor nor engineer warrants that the underground utilities shown are in the exact location indicated. The surveyor nor engineer has

It is the responsibility of the contractor to field locate all utilities prior to commencing work and notify engineer if a discrepancy is found.

The contractor shall verify the invert elevations of all existing storm and sanitary sewer structures prior to commencement of storm and sanitary sewer construction.

UNDERGROUND UTILTIES DISCLAIMER

Information regarding the reputed presence, size, character and location of existing underground utilities and structures related to underground utilities is shown heron. There is no certainty of the accuracy of this information and it shall be considered in that light by those using this drawing. The location and arrangement of underground utilities and structures related to underground utilities shown hereon may be inaccurate and utilities and structures related to underground utilities not shown may be encountered. The owner, his employees, his consultants and his contractors shall hereby distinctly understand that the surveyor is not responsible for the correctness or sufficiency of this information regarding the underground utilities and structures related to underground utilities shown hereon.

SEWER CHART							
TOP INVERT							
PROP. WYE	1014.25'	997.37'					
C.O. 1	1013.50'	1003.57'					
C.O. 2	1013.50'	1007.62'					
C.O. 3	1013.50'	1008.34'					
C.O. 4	1014.50'	1008.44'					
BLDG	1015.50'	1008.50'					

GREASE TRAP SIZING CALCULATION:

(S) X (GS) X (HR/12) X (LF) = CAPACITY IN GALLONS (S) - Number of seats in dining area = 70 Seats

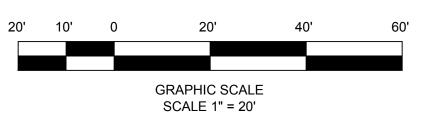
(GS) - Gallons of waste water per seat (Single Service Article Restaurant) = 10 gal. (HR) - Numbers of Hours of Establishment is open = 12 hr. (LF) - Loading Factor (Main Highway) = 1.00

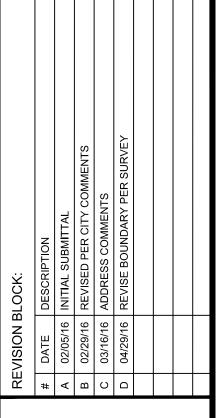
 $(70) \times (10) \times (12/12) \times (1.00) = 700 \text{ GALLONS CAPACITY MINIMUM}$ 1500 GALLON EXTERIOR PRECAST CONCRETE GREASE INTERCEPTOR PROVIDED IN LANDSCAPED AREA AT REAR OF BUILDING.

SEWER SERVICE LINE NOTE 6" SERVICE LINE SHALL MAINTAIN A MINIMUM 1% SLOPE.

METER APPLICATION NOTE: WATER METER APPLICATION TO BE SUBMITTED PRIOR TO RECEIVING THE METER AND INSTILLATION











5/3/16

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PLAN CD VELOPMENT SITE

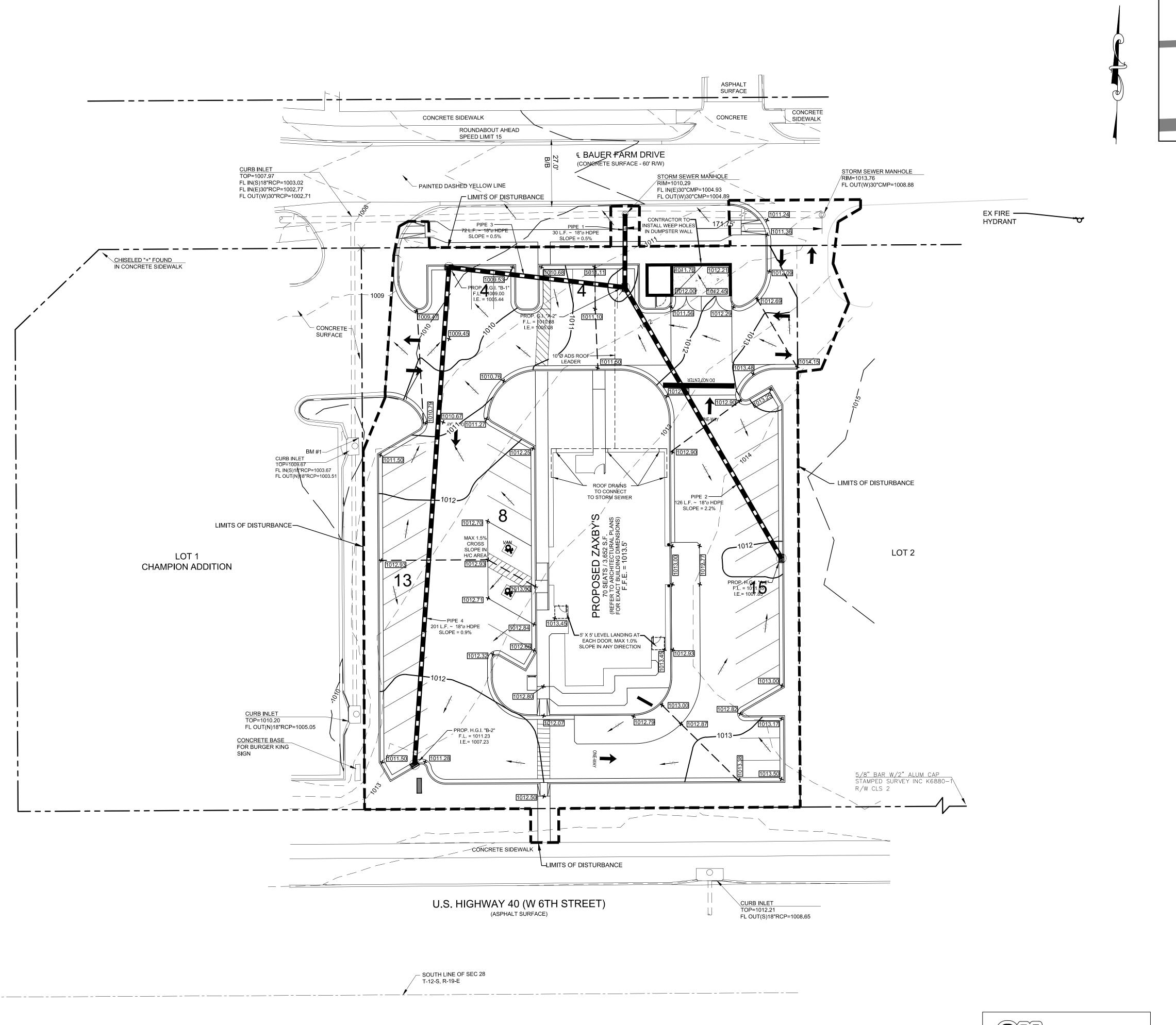
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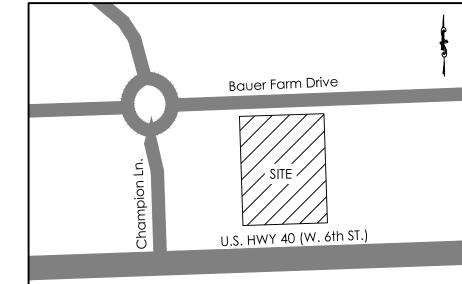
SHEET TITLE:

UTILITY PLAN

SHEET NUMBER:

PROJECT NUMBER: 15135ZAX





LOCATION MAP SCALE: 1" = 200'

STORM NOTE:

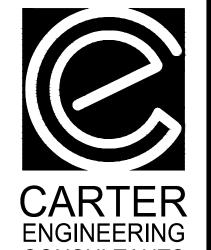
THE SITE DRAINS TO AN EXISTING STORM SYSTEM THAT DRAINS TO A MASTER STORMWATER MANAGMENT FACILTIE FOR THE OVERALL DEVELOPMENT.

GRAPHIC SCALE SCALE 1" = 20'

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CONSULTANTS

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PLANS DEVELOPMENT SITE

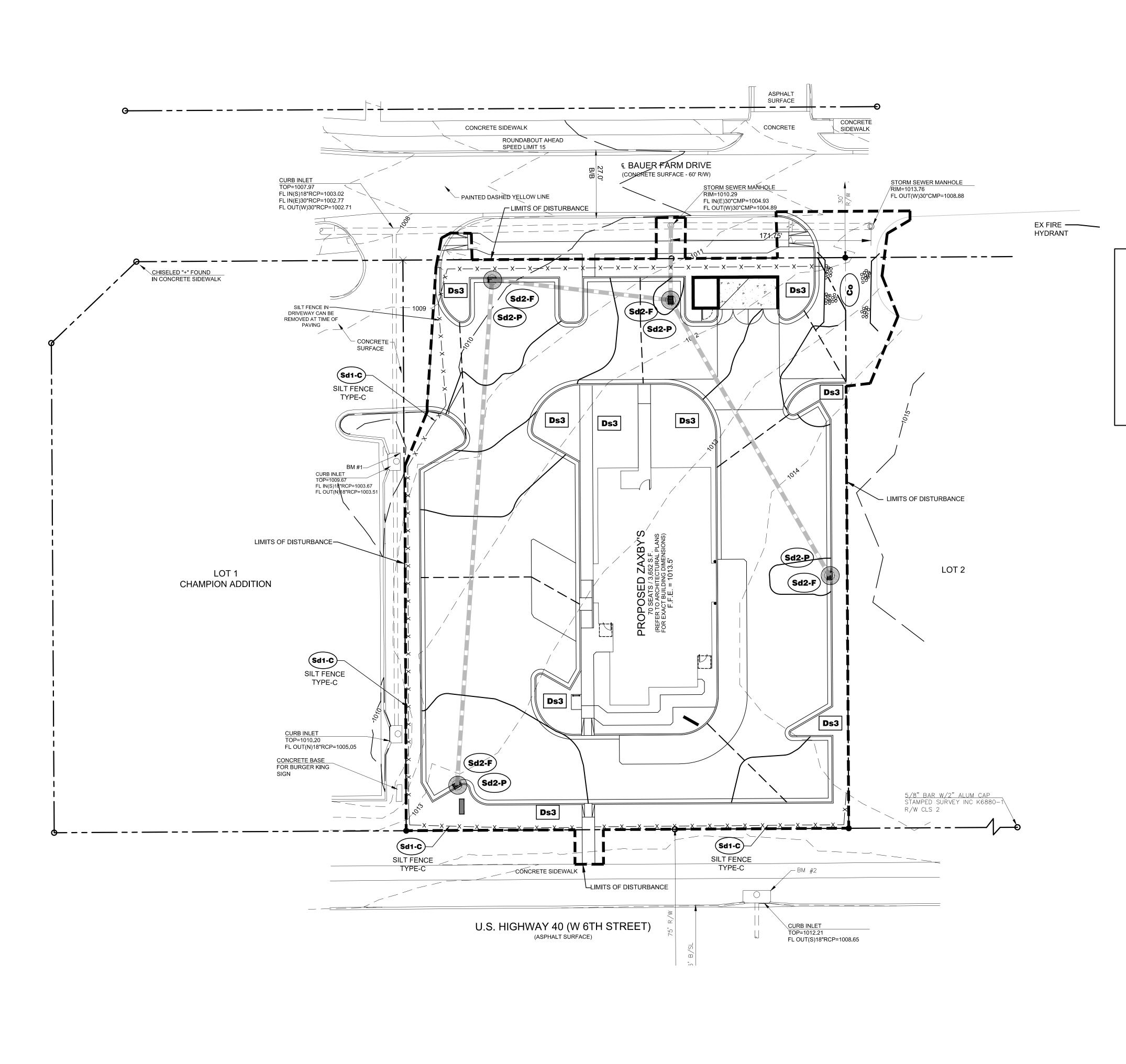
SHEET TITLE:

GRADING AND DRAINAGE PLAN

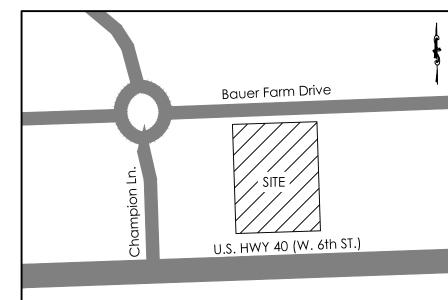
SHEET NUMBER:

PROJECT NUMBER: 15135ZAX









LOCATION MAP SCALE: 1" = 200'

EROSION CONTROL NOTES:

- DEVELOPER/OWNER:JULI, LLC; P.O. BOX 1439, WATKINSVILLE, GA 30677 TEL. (706) 714-1197 2. 24 HOUR CONTACT: BEN WILLIAMS (706) 714-1197
- 3. PROJECT INCLUDES CONSTRUCTION OF A 3,652 S.F. BUILDING WITH ASSOCIATED UTILITIES.
- TOTAL TRACT AREA = 0.92 acres, TOTAL DISTURBED AREA = 0.95 acres ADDITIONAL MEASURES WILL BE ADDED IF DETERMINED TO BE NEEDED BY ON-SITE
- INSPECTION THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO, OR CONCURRENT
- WITH, LAND-DISTURBING ACTIVITIES. 6. EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLANS DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO
- STANDARD AND SPECIFICATIONS: ALL DESIGN WILL CONFORM TO AND ALL WORK WILL WILL BE PERFORMED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE PUBLICATION ENTITLED "MANUAL FOR EROSION AND SEDIMENT CONTROL IN KANSAS"

CONTROL OR TREAT THE SEDIMENT SOURCE. PRACTICES WILL BE CHECKED DAILY.

ACTIVITY SCHEDULE:

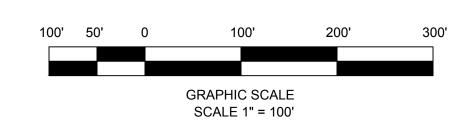
			ИΟ	NTF	IS 2	2016	3
TASK DESCRIPTION:	MAY	JUNE	JULY		SEPTEMBER		
CONSTRUCTION EXIT AND PERIMETER SILT FENCE	X				_	_	
TEMPORARY SEDIMENT STORAGE FACILITIES	X						
CLEARING & GRUBBING	X			Ш			
ROUGH GRADING	X	Х					
TEMPORARY STABILIZATION (GRASSING)		Х	Х				_
CURB AND GUTTER			Х				
GRAVEL SUBBASE FOR ROADS AND PARKING AREAS			Х				
BUILDING CONSTRUCTION		Χ	Х	Х	Х		
FINAL GRADING				Х			
PAVING					Х		
FINAL STABILIZATION (LANDSCAPING)					Х		

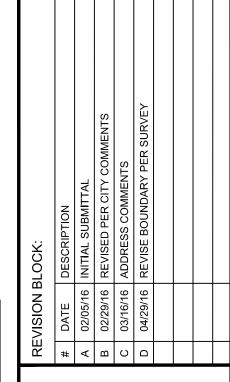
LEGEND	
DS3 DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)	
Co CONSTRUCTION EXIT	<u>** **</u>
Sd1 SEDIMENT BARRIER	—x —x —
Sd2-F INLET SEDIMENT TRAP *	
Sd2p INLET SEDIMENT TRAP *	
Mb MATTING BLANKET	
LIMITS OF DISTURBANCE	

NOTE: * INLET SEDIMENT TRAP Sd2-F SHALL BE USED IN NON PAVED ARES UNTIL PAVING INS COMPLETE. WHILE Sd2-P SHALL BE USED IN AREAS THAT IS PAVED.

DISTURBANCE NOTE: THE PROPOSED DISTURBANCE IS LESS THAN 1.0 ACRE.

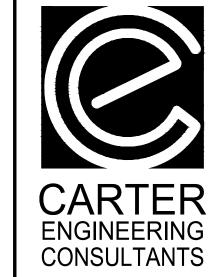








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PLANS PCD DEVELOPMENT

PROJECT NAME: ZAXBYS

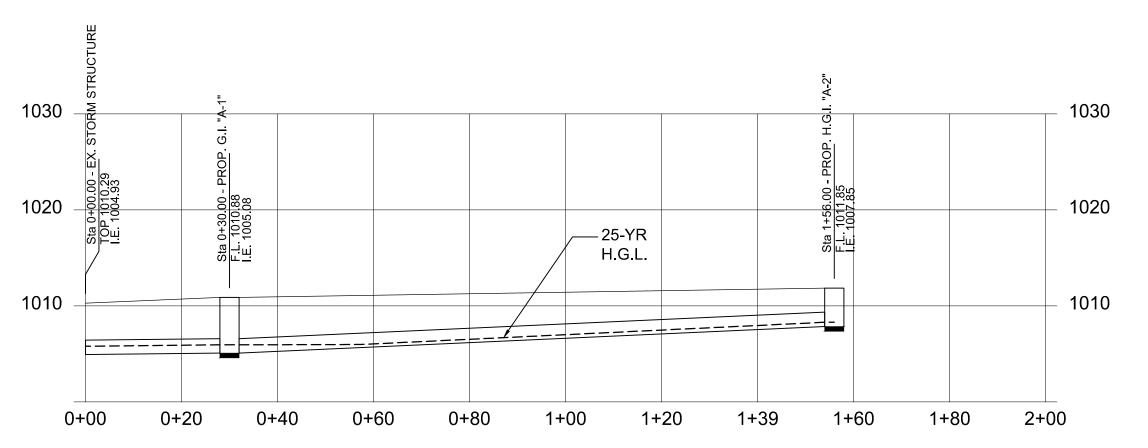
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SITE

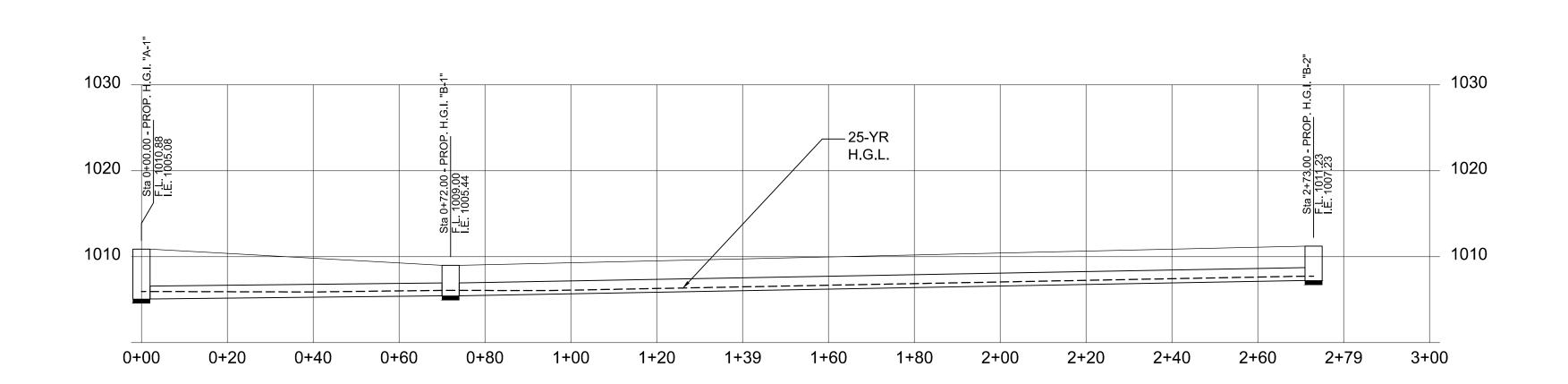
EROSION CONTROL PLAN

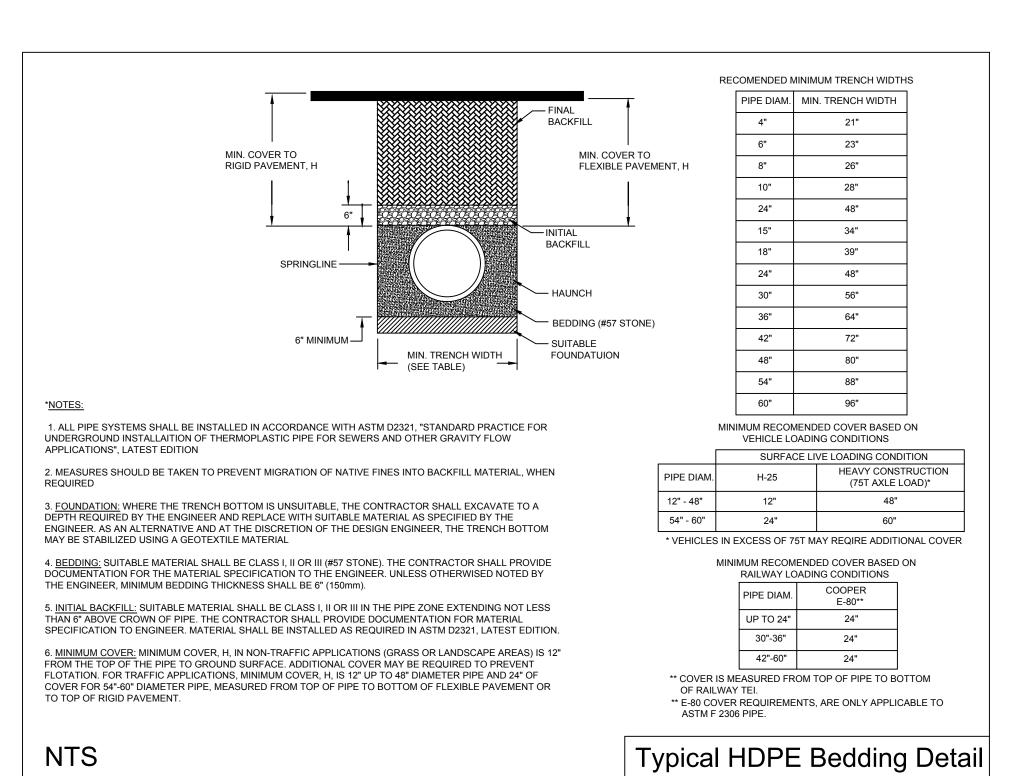
SHEET NUMBER:

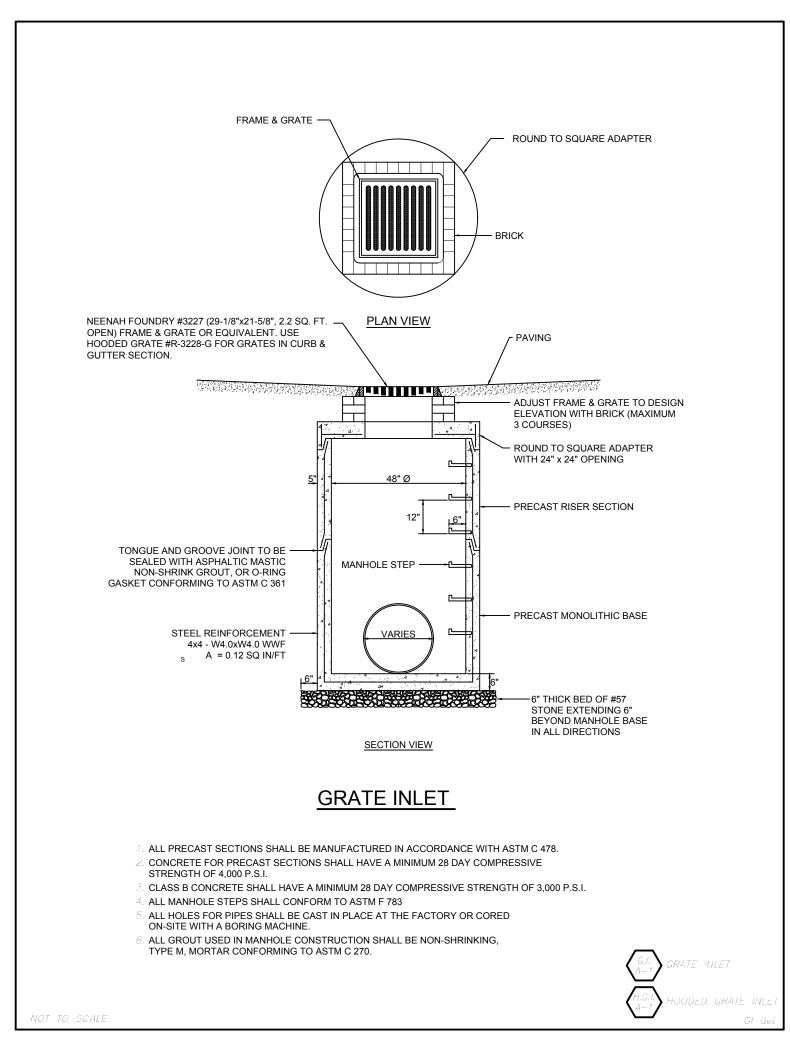
PROJECT NUMBER: 15135ZAX

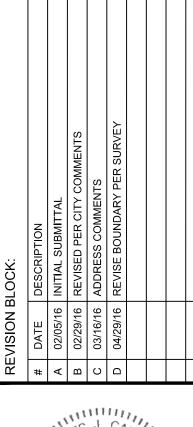


			25	- YR STO	ORM SEW	ER PROF	ILES			
				SCALE: 1	(in) = 20 (ft) H,	1(in) = 10(ft)	V			
Pipe No.	Flow	Size	Mat'l	Length	I.E. Down	I.E. Up	Slope	HGL Down	HGL Up	Velocity
	(cfs)	(in)		(ft)	(ft)	(ft)	(%)	(ft)	(ft)	(ft/s)
1	5.12	18	HDPE	30	1004.93	1005.08	0.5	1005.80	1005.95	4.81
2	1.44	19	HDPE	126	1005.08	1007.85	2.2	1005.95	1008.30	2.29
3	2.84	20	HDPE	72	1005.08	1005.44	0.5	1005.95	1006.08	3.31
4	1.79	21	HDPE	201	1005.44	1007.23	0.9	1006.08	1007.73	2.97











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PLAN CD OPMENT SITE

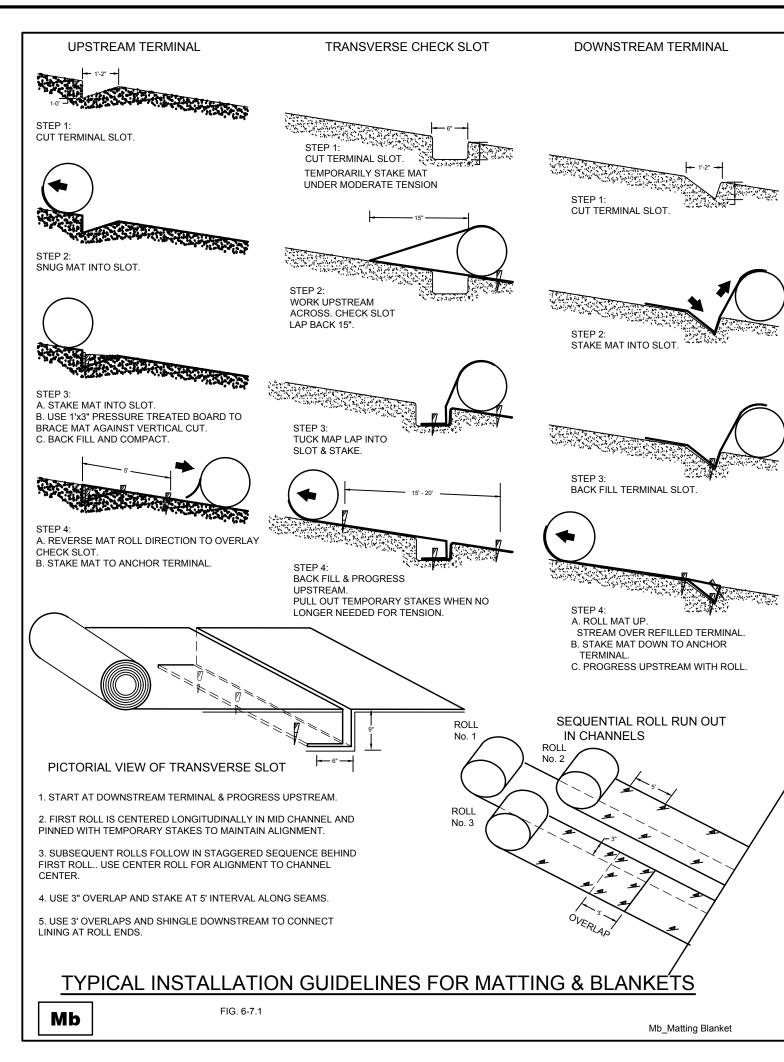
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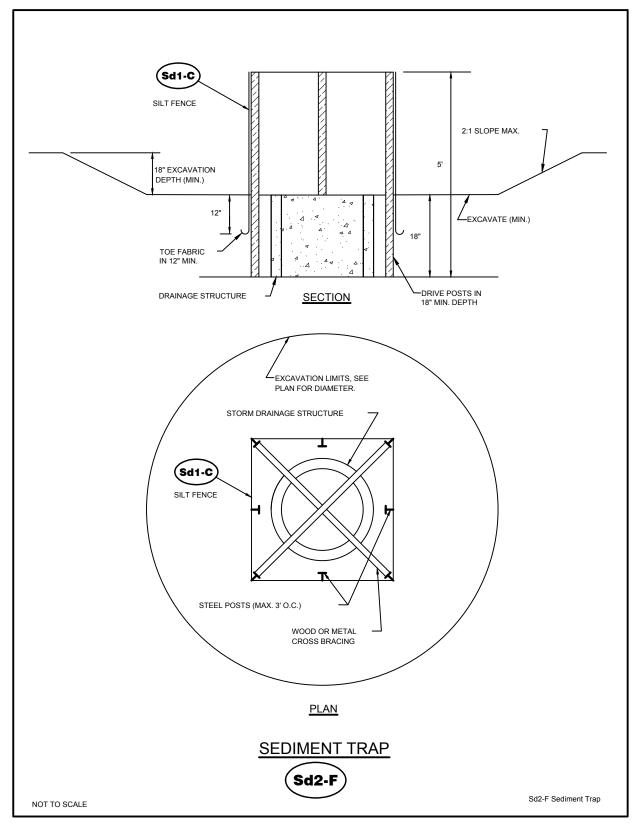
SHEET TITLE:

PIPE PROFILES

SHEET NUMBER:

PROJECT NUMBER: 15135ZAX





Ds1 MULCHING RATES USE DRY STRAW OR DRY HAY OF GOOD QUALITY AND FREE OF WEEDS SEEDS. DRY STRAW OR HAY SHALL BE APPLIED AT THE RATE OF 2.5 TONS PER ACRE. MULCHING SHALL BE USED MULCH WILL BE SPREAD UNIFORMLY WITHIN 24-HOURS AFTER SEEDING. MULCH SHALL BE USED DURING MONTHS THAT GRASSING SOULD NOT BE APPLIED BASED ON THE SCHEDULE BELOW. TEMPORARY AND PERMANENT VEGETATION SHALL BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF CHAPTER 6 OF "THE MANUAL FOR EROSION AND SEDIMENTATION CONTROL Ds2 TEMPORARY GRASSING TEMPORARY GRASSES SHALL CONSIST OF SOWING A QUICK GRASS SUCH AS RYE, BROWN TOP MILLET, OR GRASS SUITABLE TO THE AREA AND SEASON. LIME AND FERTILIZER WILL BE OMITTED. MULCH IS NOT REQUIRED BUT SHOULD BE USED AS DICTATED BY SITE CONDITIONS. TEMPORARY GRASSING IS REQUIRED WHEN DISTURBED AREA IS LEFT EXPOSED MORE THAN 14 DAYS. **SPECIES** RATE PLANTING DATES JAN-APR/AUG-DEC RYE GRASS 40#/ACRE APRIL - JUNE AUGUST-DECEMBER **BROWN MILLET** 10-40#/ACRE 1/2-3 bu./ACRE FEBRUARY-JUNE WEEPING LOVEGRASS 2-4#/ACRE FEBRUARY-APRIL ANNUAL LESPEDEZA 10-40#/ACRE APRIL-AUGUST SEPTEMBER-DECEMBER SUDAN GRASS 1/2-3bu./ACRE

PERMANENT GRASSING Ds3 ERMANENT GRASSING SHALL CONSIST OF GROUND PREPARATION, LIMING, FERTILIZATION, MULCHING AND SEEDING. THE GROUND SHALL BE PREPARED BY PLOWING AND DISKING TO A DEPTH NOT LESS THAN 4". FERTILIZER AND LIME SHALL BE UNIFORMLY MIXED INTO THE GROUND, WITH FERTILIZER AT THE RATE OF 1500#/ACRE AND LIME AT THE RATE OF 2000#/ACRE. THE GROUND SHALL BE FINISHED OFF SMOOTH AND UNIFORM AND BE FREE OF ROCKS, CLODS, ROOTS AND WEEDS. FERTILIZER SHALL BE APPLIED PER THE TABLE BELOW. WEATHER PERMITTING, SEEDING SHALL BE DONE WITHIN 24 HOURS OF FERTILIZER APPLICATION. SEED SHALL BE UNIFORMLY SPREAD AT THE RATES SHOWN BELOW. MULCHING IS REQUIRED AND SHALL BE DONE IMMEDIATELY AFTER SEEDING. MULCH SHALL BE UNIFORMLY APPLIED OVER THE AREA LEAVING APPROXIMATELY 25% OF THE GROUND SURFACE EXPOSED. THE RATE OF APPLICATION SHALL BE DOUBLED ON SLOPES STEEPER THAN 4:1.

GRA	SSING RATES AND S	SCHEDULE
SPECIES	RATE	PLANTING DATES
TALL FESCUE	30-50#/ACRE	MARCH-APRIL/ AUGUST-OCTOBER
SERICEA LESPEDEZA (b) WEEPING LOVE GRASS UNHULLED BERMUDA HULLED BERMUDA BAHIA	60#/ACRE 2-4#/ACRE 6-10#/ACRE 6-10#/ACRE 30-60#/ACRE	MARCH-JUNE MARCH-JUNE JAN-FEB/OCT-DEC MARCH-JUNE APRIL-MARCH

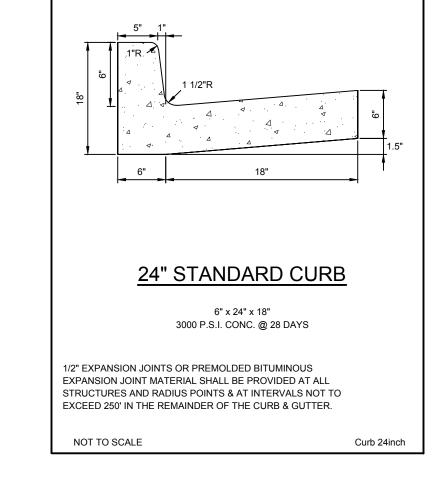
(a) USE A MINIMUM OF 40# SCARIFIED SEED. REMAINDER MAY BE UNSCARIFIED, CLEAN (b) USE EITHER COMMON SERALA, OR INTERSTATE SERICEA LESPEDEZA.

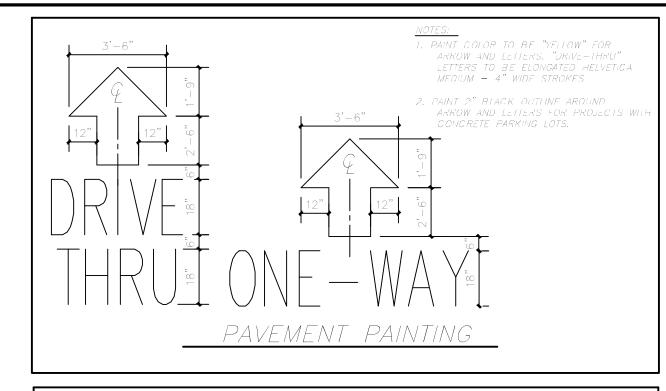
LIMING RATES

Agricultural lime is required at the rate of one to two tons per acre unless soil tests indicate otherwise. Graded areas require lime application. If lime is applied within six months of planting permanent perennial vegetation, additional lime is not required. Agricultural lime shall be within the specifications of the Kansas Department of Agriculture.

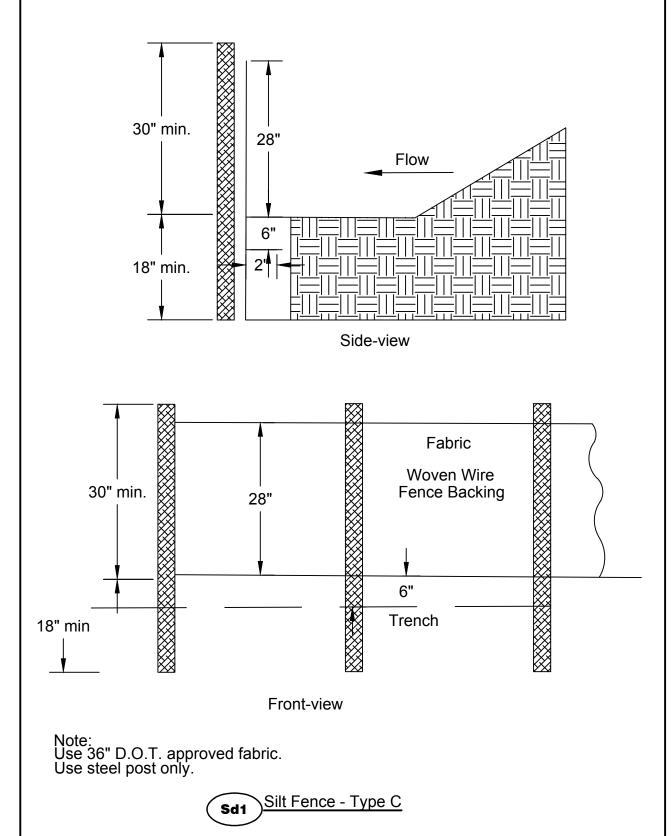
FERTILIZER REQUIREMENTS

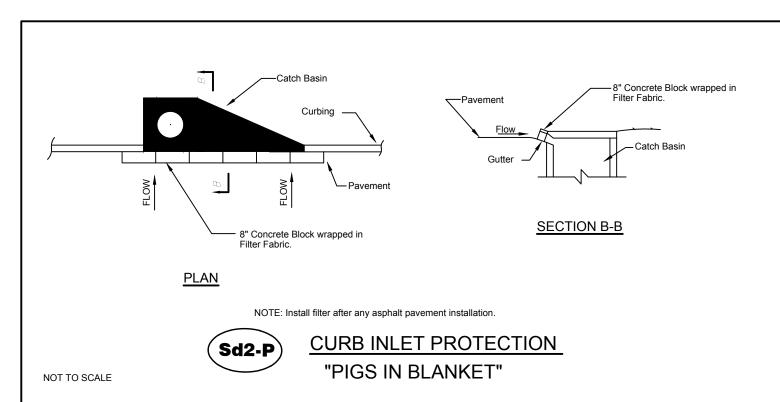
TYPES OF SPECIES	YEAR	ANALYSIS OR EQUIVALENT N-P-K	RATE	N TOP DRESSING RATE
Cool season grasses	First Second Maintenance	6-12-12 6-12-12 10-10-10	1500 lbs./ac. 1000 lbs./ac. 400 lbs./ac.	50-100 lbs./ac/ 1/2/ - 30
Cool season grasses and legumes	First Second Maintenance	6-12-12 0-10-10 0-10-10	1500 lbs./ac. 1000 lbs./ac. 400 lbs./ac.	0-50 lbs./ac. 1/ -
3. Ground Covers	First Second Maintenance	10-10-10 10-10-10 10-10-10	1300 lbs./ac. 3/ 1300 lbx./ac. 3/ 1100 lbx./ac.	-
4. Shrub Lespedeza	First Maintenance	0-10-10 0-10-10	700 lbs./ac. 700 lbs./ac. 4/	-
5. Warm season grasses	First Second Maintenance	6-12-12 6-12-12 10-10-10	1500 lbs./ac. 800 lbs./ac. 400 lbs./ac.	50-100 lbs./ac. 2/ 6/ 50-100 lbs./ac. 2/ 30 lbs./ac.
Warm season grasses and legumes	First Second Maintenance	6-12-12 0-10-10 0-10-10	1500 lbs./ac. 1000 lbs./ac. 400 lbs./ac.	50 lbs./ac. 6/
				Ds1_Ds2_Ds3.dwg



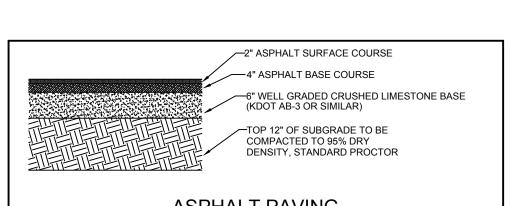


— 6" PORTLAND CEMENT CONCRETE -4" WELL GRADED CRUSHED LIMESTONE BASE (KDOT AB-3, OR SIMILAR) TOP 12" OF SUBGRADE TO BE COMPACTED TO 95% DRY DENSITY, STANDARD PROCTOR **CONCRETE DETAIL**

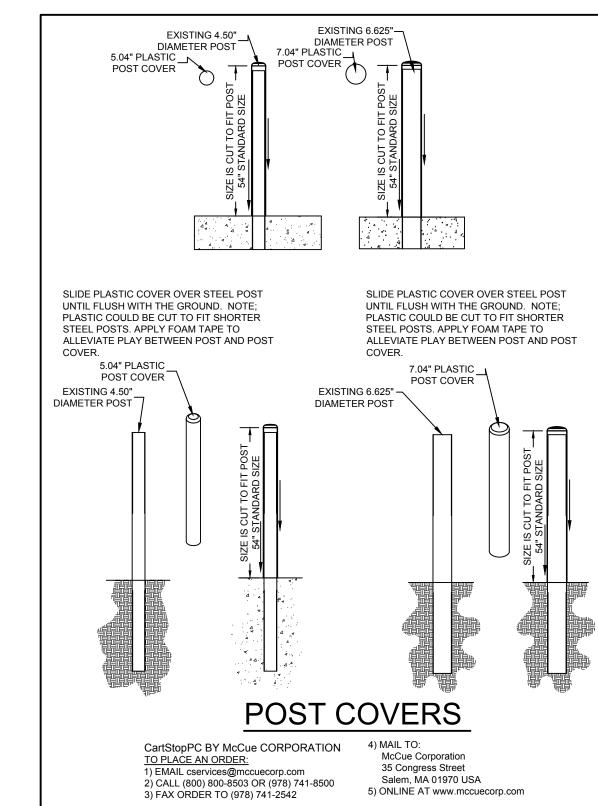


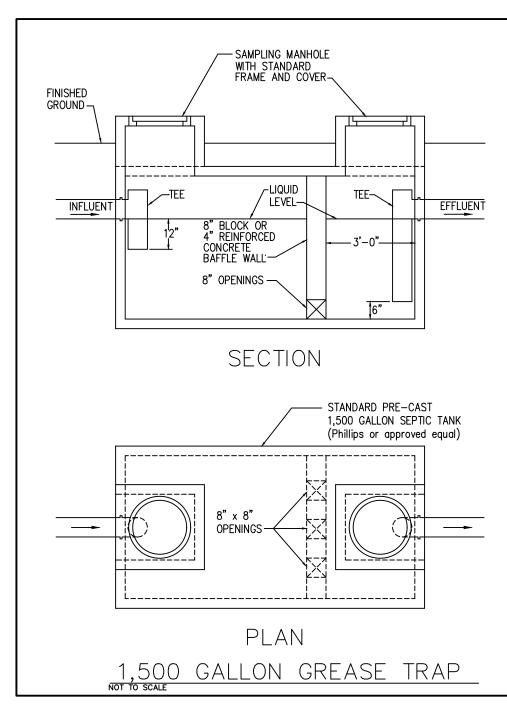


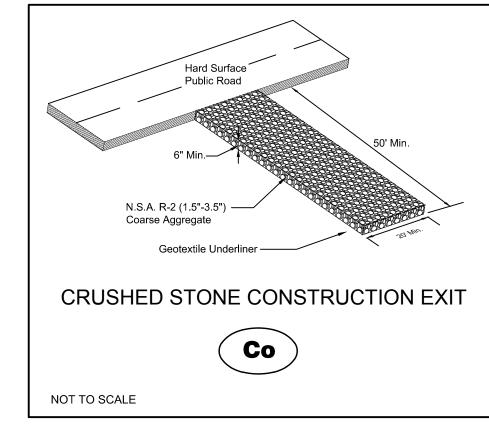
Terracon, dated: February 23, 2016.

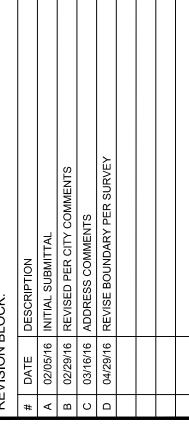


ASPHALT PAVING The intent of this specification is to provide heavy duty asphalt in all applicable areas. The geotechnical report is provided by













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PLANS DEVELOPMENT

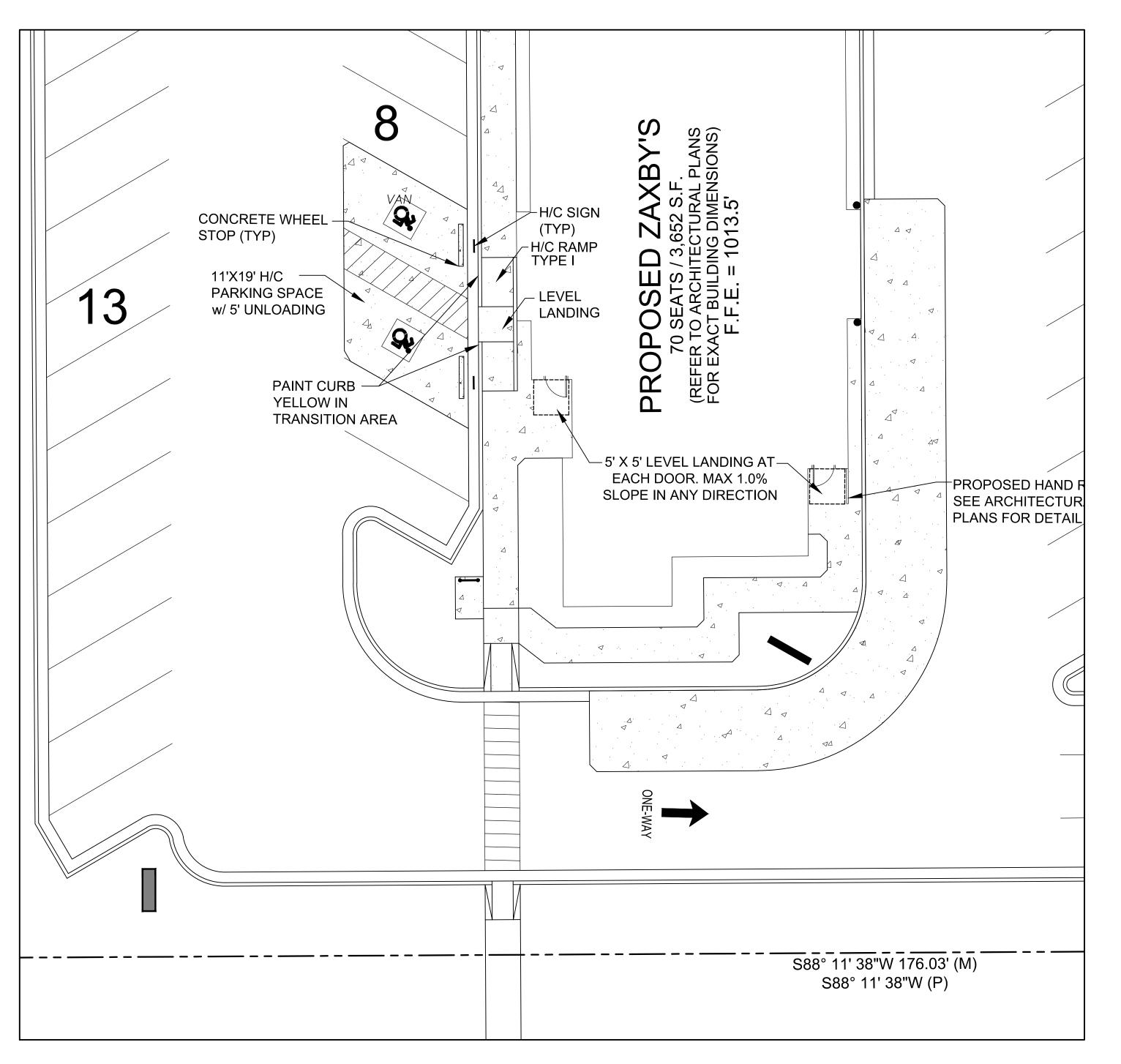
ZAXBYS

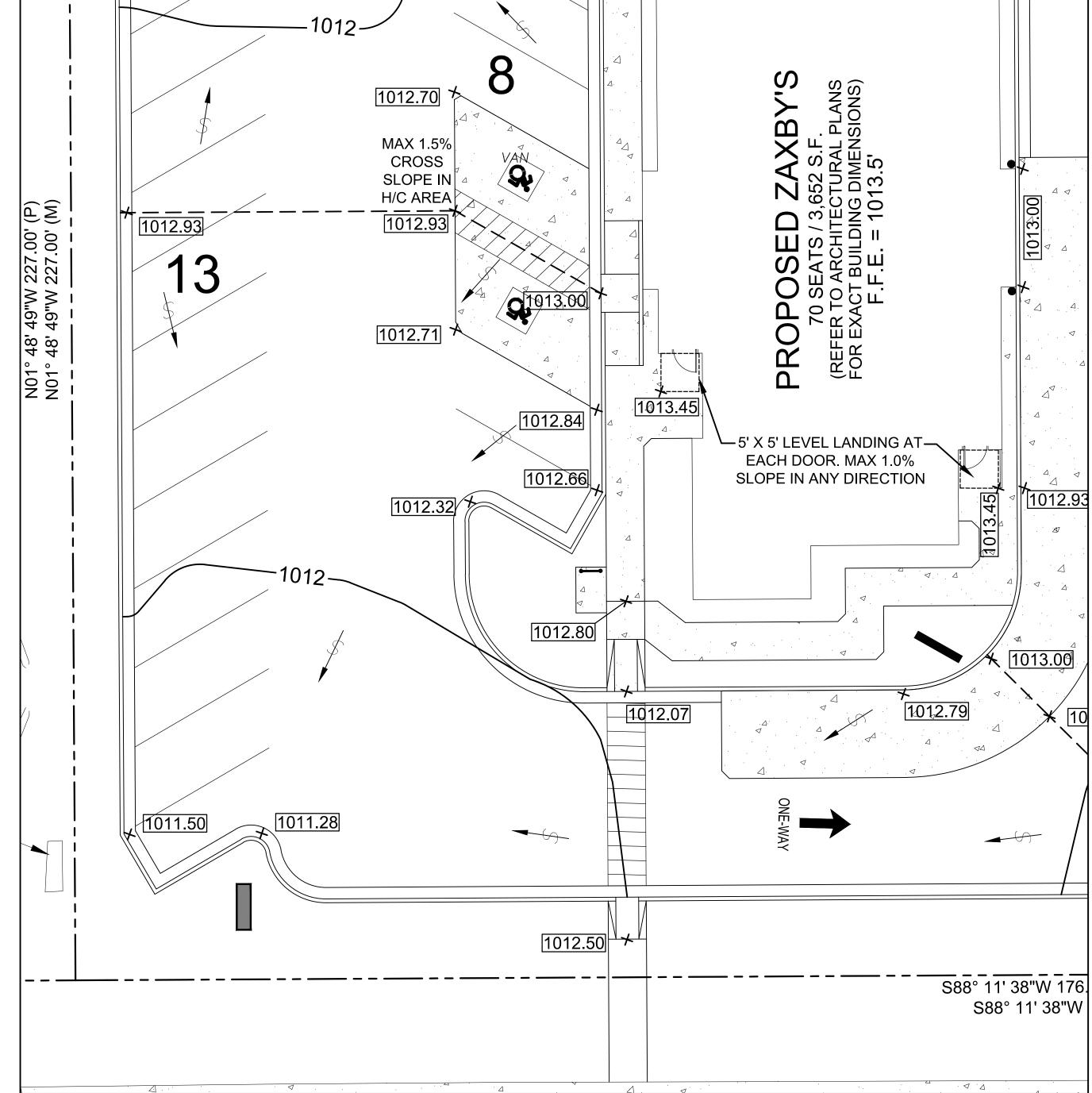
SHEET TITLE: STANDARD **DETAILS**

SITE

SHEET NUMBER:

PROJECT NUMBER: 15135ZAX

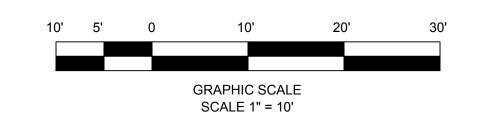


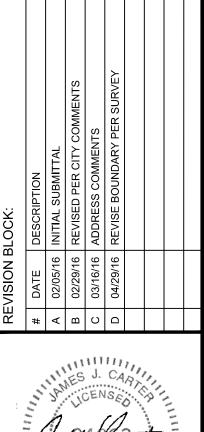


SITE PLAN SCALE: 1" = 10'



ACCESSIBLE PARKING SPACES, ACCESS AISLES AND PASSENGER LOADING ZONES SHALL HAVE SURFACE SLOPES NOT STEEPER THAN 1:67 (1.5%) IN ANY DIRECTION.









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SITE DEVELOPMENT PLAN for JULI, LLC BAUFR FARM PCD

PROJECT NAME:

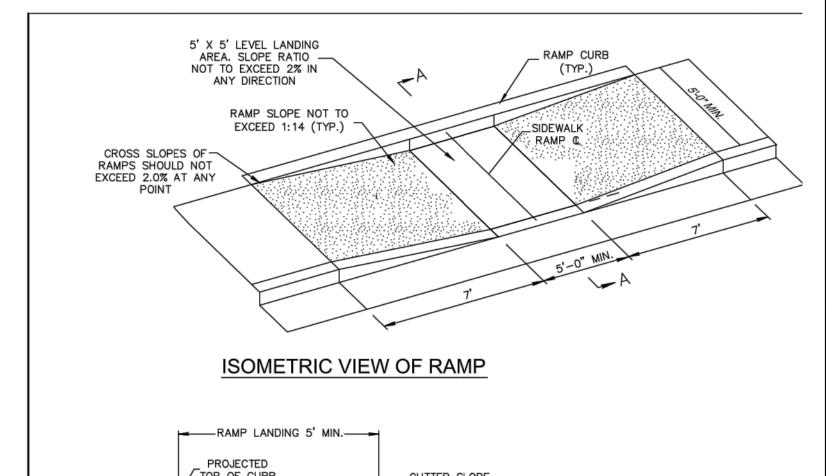
SHEET TITLE:

ADA ACCESS
PLAN

SHEET NUMBER:

9

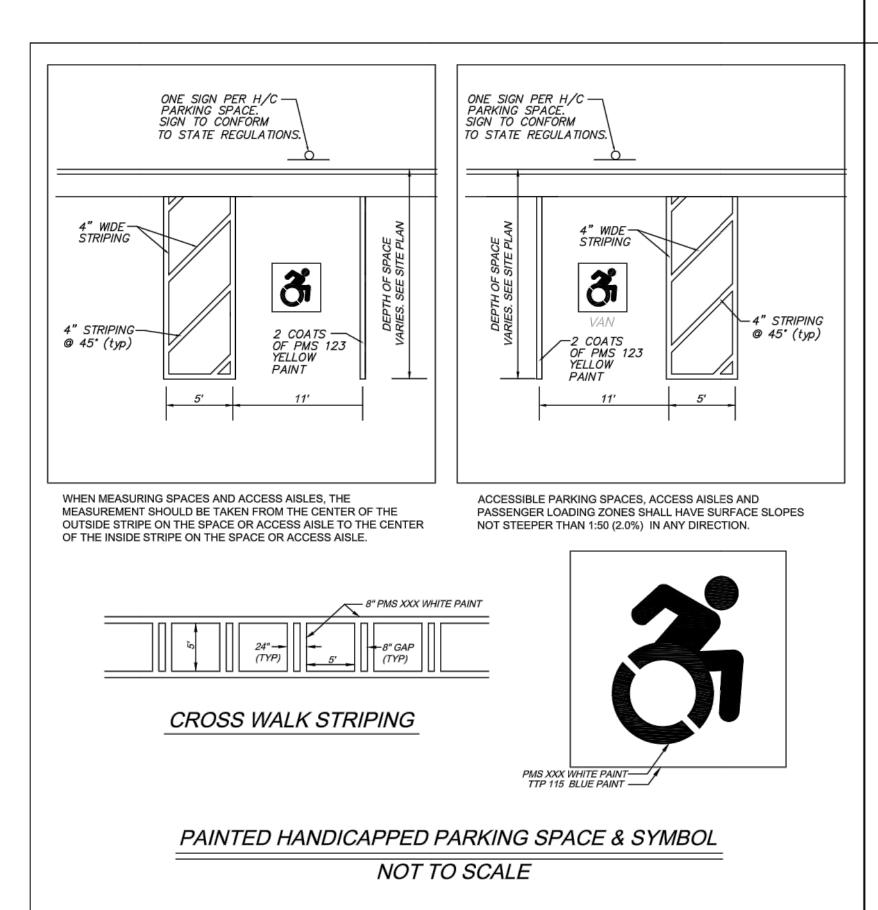
PROJECT NUMBER: 15135ZAX

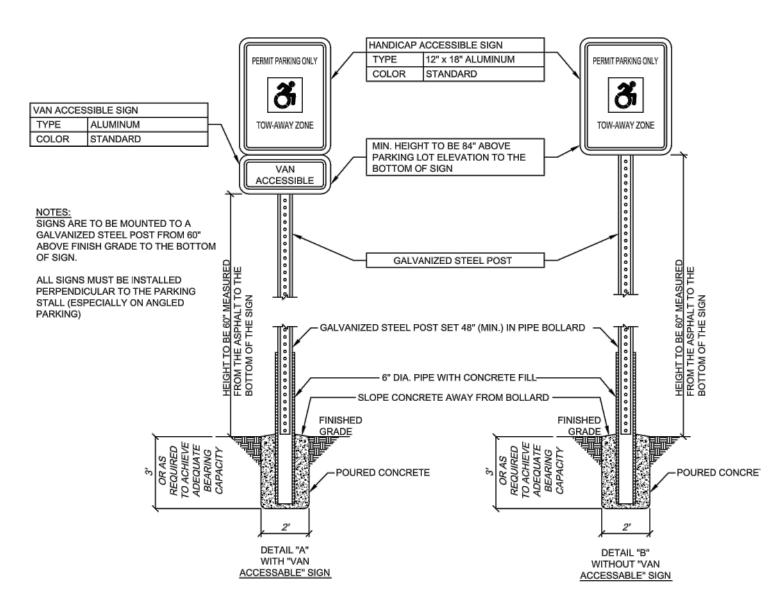


NC

- 1. HANDICAP RAMP CONSTRUCTION SHALL CONFORM TO ALL FEDERAL, STATE AND CITY CODES AND SPECIFICATIONS.
- 2. SURFACES SHALL MEET THE REQUIREMENTS OF 2010 ADA AND THE 2004 ADAAG
- 3. THE SLOPE OF THE GUTTER MUST NOT EXCEED 1:20 ADJACENT TO THE RAMP.

STANDARD CONSTRUCTION DETAIL HANDICAP RAMP TYPE I





"HANDICAP PARKING ONLY" SIGNS
NOT TO SCALE

REVISION BLOCK:

DATE DESCRIPTION

A 02/05/16 INITIAL SUBMITTAL

B 02/29/16 REVISED PER CITY COMMENTS

C 03/16/16 ADDRESS COMMENTS

D 04/29/16 REVISE BOUNDARY PER SURVEY



5/3/16



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DEVELOPMENT PLANS
for
JULI, LLC
BAUER FARM PCD
BAUER FARM PCD

PROJECT NAME:

SHEET TITLE:

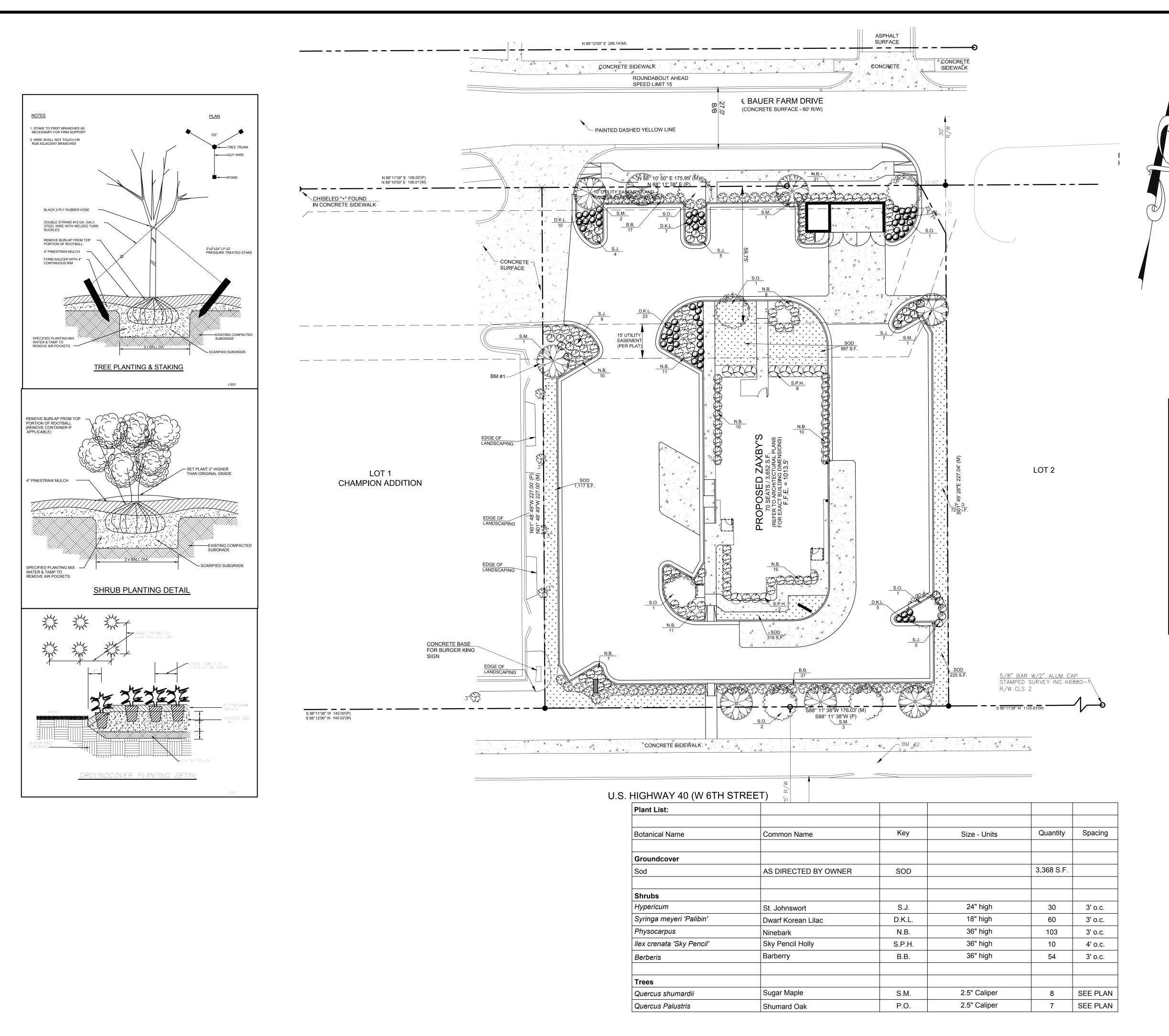
SITE

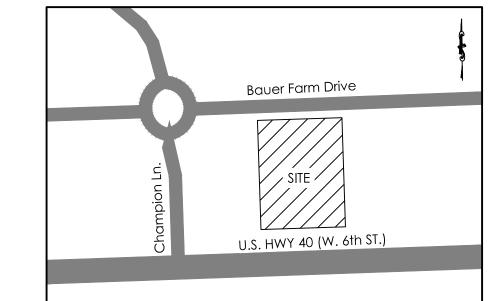
ADA DETAILS

SHEET NUMBER:

10

PROJECT NUMBER: 15135ZAX





LOCATION MAP SCALE: 1" = 200'

- LANDSCAPING SHOWN ON THIS PLAN SHALL BE MAINTAINED IN COMPLIANCE WITH CITY LANDSCAPING ORDINANCE
- TREES SELECTED FOR PLANTING MUST BE FREE FROM INJURY, PESTS, DISEASE, NUTRITIONAL DISORDERS OR ROOT DEFECTS, AND MUST BE OF GOOD VIGOR, SO AS TO ASSURE A REASONABLE EXPECTATION OF SURVIVABILITY.
- ALL TREES TO BE BALLED AND BURLAPPED WHEN SHIPPED TO THE SITE FOR PLANTING. TREES TO BE A MINIMUM OF 10' IN HEIGHT AT TIME OF PLANTING.
- ALL DISTURBED GROUND SHALL HAVE GROUND COVER OF MULCH AND/OR GRASSING.
- SOD SHALL BE SPECIFIED BY OWNER. IRRIGATION SHALL BE PROVIDED.

LANDSCAPE CALCULATIONS:

STREET FRONTAGE (US HWY 40):

1 TREE PER 40 L.F. OF FRONTAGE

1 TREE / 40 L.F. X 176 = 4.4 = 5 TREES

5 TREES PROVIDED (3 S.M. & 2 S.O.)

STREET FRONTAGE (BAUER FARM DRIVE):

1 TREE PER 40 L.F. OF FRONTAGE

1 TREE / 40 L.F. X 165 OF FRONTAGE = 4.125 = 5 TREES 5 TREES PROVIDED (3 S.M. & 2 S.O.)

PARKING LOT REQUIREMENTS:

1 TREE PER 10 PARKING SPACES PROVIDED 44 SPACES PROVIDED = 1 TREE / 10 SPACES X 44 SPACES = 4.4 = 5 TREES REQUIRED

5 TREES PROVIDED (3 S.O., AND 2 S.M.)

3 SHRUB PER 10 PAKRKING SPACES PROVIDED 3 SHRUBS X 44 SPACES / 10 = 13.2 = 14 SHRUBS REQUIRED.

93 SHRUBS PROVIDED (74 N.B. & 19 S.J.)

40 S.F. OF LANDSCAPED AREA PER PARKING SPACE 40 S.F. X 44 SPACES = 1,760 S.F. REQUIRED

2,756 S.F. PROVIDED PARKING LOT FRONTAGE (US HWY 40):

1 TREE PER 25 L.F. OF PARKING ALONG FRONTAGE

110 L.F. X 1 TREE / 25 L.F. = 4.4 = 5 TREES REQUIRED 5 TREES PROVIDED

PARKING LOT FRONTAGE (BAUER FARM DRIVE):

1 TREE PER 25 L.F. OF PARKING ALONG FRONTAGE 81 L.F. X 1 TREE / 25 L.F. = 3.24 = 4 TREES REQUIRED

4 TREES PROVIDED



5/3/16



3651 Mars Hill Road, Suite 2000 Watkinsville, GA 30677 P: 770.725.1200

CONSULTANTS

F: 770.725.1204 www.carterengineering.net

PLANS PCD VELOPMENT

PROJECT NAME:

SITE

SHEET TITLE: LANDSCAPE PLAN

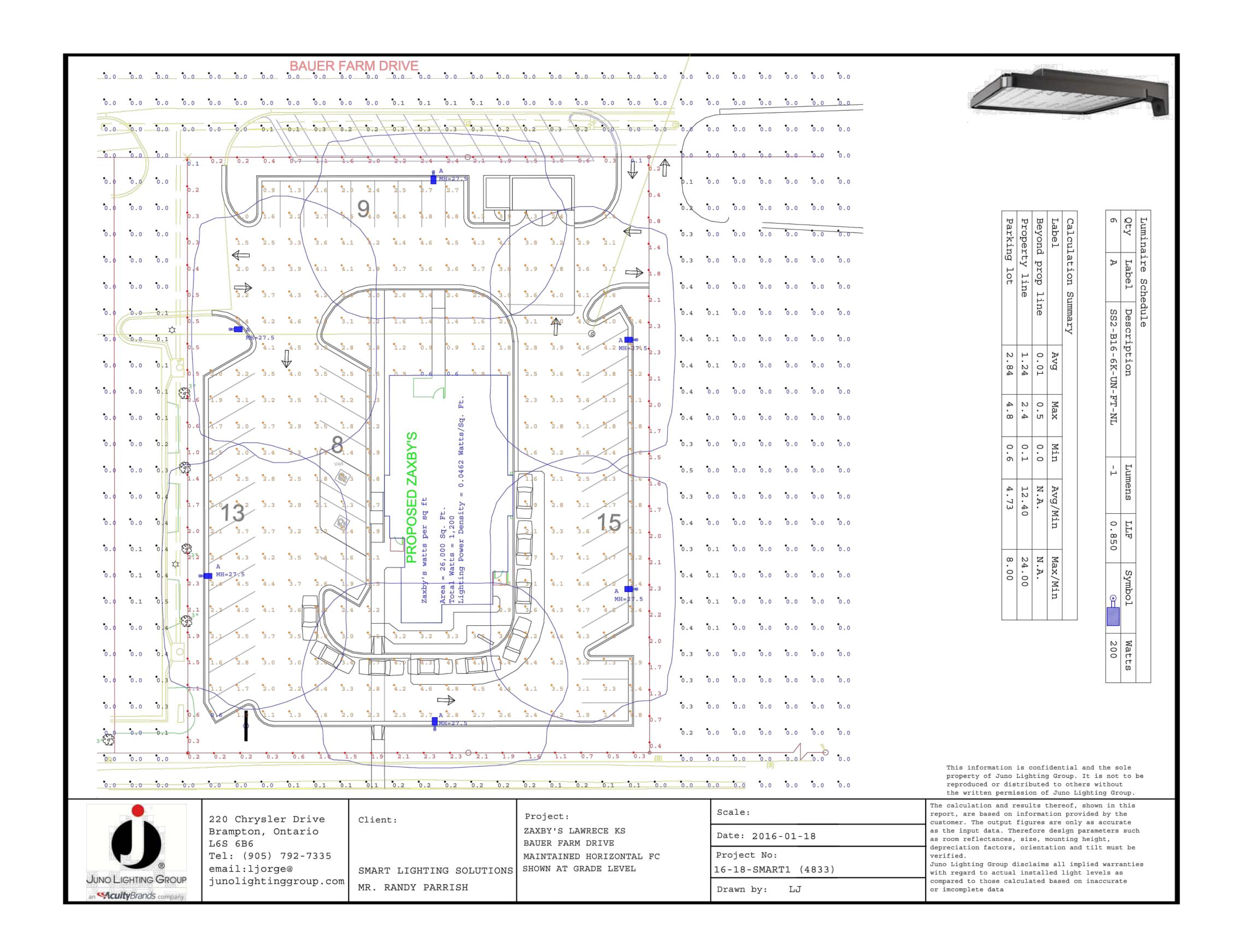
SHEET NUMBER:

PROJECT NUMBER:

15135ZAX

200'

GRAPHIC SCALE SCALE 1" = 100'



REVISION BLOCK:

DATE DESCRIPTION
A 02/05/16 INITIAL SUBMITTAL
B 02/29/16 REVISED PER CITY COMMENTS
C 03/16/16 ADDRESS COMMENTS
D 04/29/16 REVISE BOUNDARY PER SURVEY



CARTER

Carter Engineering Consultants, I 3651 Mars Hill Road, Suite 2000 Watkinsville, GA 30677 P: 770.725.1200 F: 770.725.1204 www.carterengineering.net

CONSULTANTS

DEVELOPMENT PLANS

III E DEVELOPIME for JULI, LL BAUER FARN

PROJECT NAME:

SHEET TITLE:

OUTDOOR LIGHTING PLAN

SHEET NUMBER:

12

PROJECT NUMBER: 15135ZAX

EXTERIOR FINISH SCHEDULE

ALL MATERIALS & INSTALLATION SHALL CONFORM TO APPLICABLE ASTM STANDARDS UNLESS GOVERNING AUTHORITIES HAVE DIFFERENT REQUIREMENTS.

STANDARD BRICK UNIT 3 5/8" x 2" x 7 5/8" RUNNING 1/2 BOND

1 COLOR: BRICK - MOSSWOOD (BORAL BRICK)
MORTAR - BUFF BRICK (BORAL BRICK)

ALTERNATE BRICK:

1 COLOR: BRICK - MOSSTOWN (CHEROKEE BRICK)

MORTAR - ANTIQUE BUFF (CEMEX)

15# ROOFING FELT OVER ALL SUBSTRATES OR SEALED DENS GLASS GOLD SHEATHING GROUT VOID SOLID @ FENCE AND BELOW F.F. PROVIDE VAPOR BARRIER AS INDICATED.

CONTINUOUS, THROUGH WALL FLASHING, EPDM (BY W.R. GRACE) OR EQUAL, AT BOTTOM OF WALLS & ABOVE OPENINGS. CUT FLASHING FLUSH WITH EXT. FACE. PROVIDE CELL VENT TYPE WEEPS, ABOVE FLASHING @ 32" O.C. MAX.

3/8" THICK MORTAR JOINTS. ASTM C-270, TYPE N.

HOHMANN & BARNARD, INC. HORIZ REINF. ON CMU FENCE HOHMANN & BARNARD, INC., DW-10 HS TRIANGULAR MTL. WALL TIES (OR EQUAL) @ 16" O.C. VERT. & AT HORIZ. STUD SPACING, 14 GA. SEE SECTIONS & SPECS. ATTACH TO STUDS WITH (2) 10 - 16 x 1-1/2" SCREWS.

- 2 STANDING SEAM MTL. ROOFING SEE 4
- 3 STO TO HAVE "SWIRL" FINISH AND CAN BE PAINTED. FINISH COLORS, BELOW AND ON SHEET A-103 CAN BE PAINTED. STO BRAND EIFS ON CMU, OR SHEATHING:
 "STO ESSENCE" SPEC. E100
 ICC EVALUATION REPORT NO. ESR-1720
- 3F = FIELD COLOR: SHERWIN WILLIAMS ZAXBY'S CREAM 3A = ACCENT COLOR: SHERWIN WILLIAMS COLOR DARK TAN
- PREFINISHED, METAL COPING OR GRAVEL STOP & ROOFING
 COLOR: COPINGS: MATCH SURROUNDING CORNICE COLOR
 ROOFING @ REAR SHED: BLACK
 MATERIAL SHALL BE 24 GAUGE, HOT DIPPED GALVANIZED STEEL OR

ROOFING @ REAR SHED: BLACK
MATERIAL SHALL BE 24 GAUGE, HOT DIPPED GALVANIZED STEEL OR
0.032" THICK ALUM. FINISH SHALL BE KYNAR 500 FLUOROCARBON COATING.
ACCEPTABLE MANUFACTURES ARE AS FOLLOWS:

A. BERRIDGE MANUF. CO., HOUSTON, TX. B. PAC-CLAD

C. McELROY METAL CO., PEACHTREE CITY, GA.

D. APPROVED EQUALS WILL BE ACCEPTED

PROVIDE ALL RELATED ACCESSORIES: FLASHING, END CLOSURES, GUTTERS, DOWNSPOUTS, ETC. IN MATCHING COLOR. INSTALL ALL MATERIALS PER MANUF.'S RECOMMENDATIONS.

A MANUFACTURER'S WARRANTY AGAINST DISCOLORATION & WEATHER-TIGHTNESS WILL BE REQUIRED.

NOT ALL ROOF PENETRATIONS & ACCESSORIES ARE SHOWN ON THIS DRAWING. IT IS THE RESPONSIBILITY OF THE G.C. TO INSTALL ITEMS INDICATED ELSEWHERE.

5 STOREFRONT: WINDOWS & ENTRANCE DOORS (BLACK ANODIZED)
ACCEPTABLE MANUFACTURER: KAWNEER, REFER TO SPECIFICATIONS
ALL GLAZING SHALL BE IN ACCORDANCE W/ THE FOLLOWING VALUES:

GLAZING - UNCOATED TINTED GLASS							
APPEARANCE	LOCATION	TYPE & THICKNESS	U FACTOR	SHGC	VLT	VLR	
GRAY TINT	EXTERIOR SIDE	1/4" TEMPERED	.47	.45	39.6	7.3	
CLEAR -NO TINT	INTERIOR SIDE	1/4" TEMPERED	.58	.69	79	15	

CONTACT PERSON @ QUALITY GLASS: JOHNNY DOSTER @ 706-548-4481

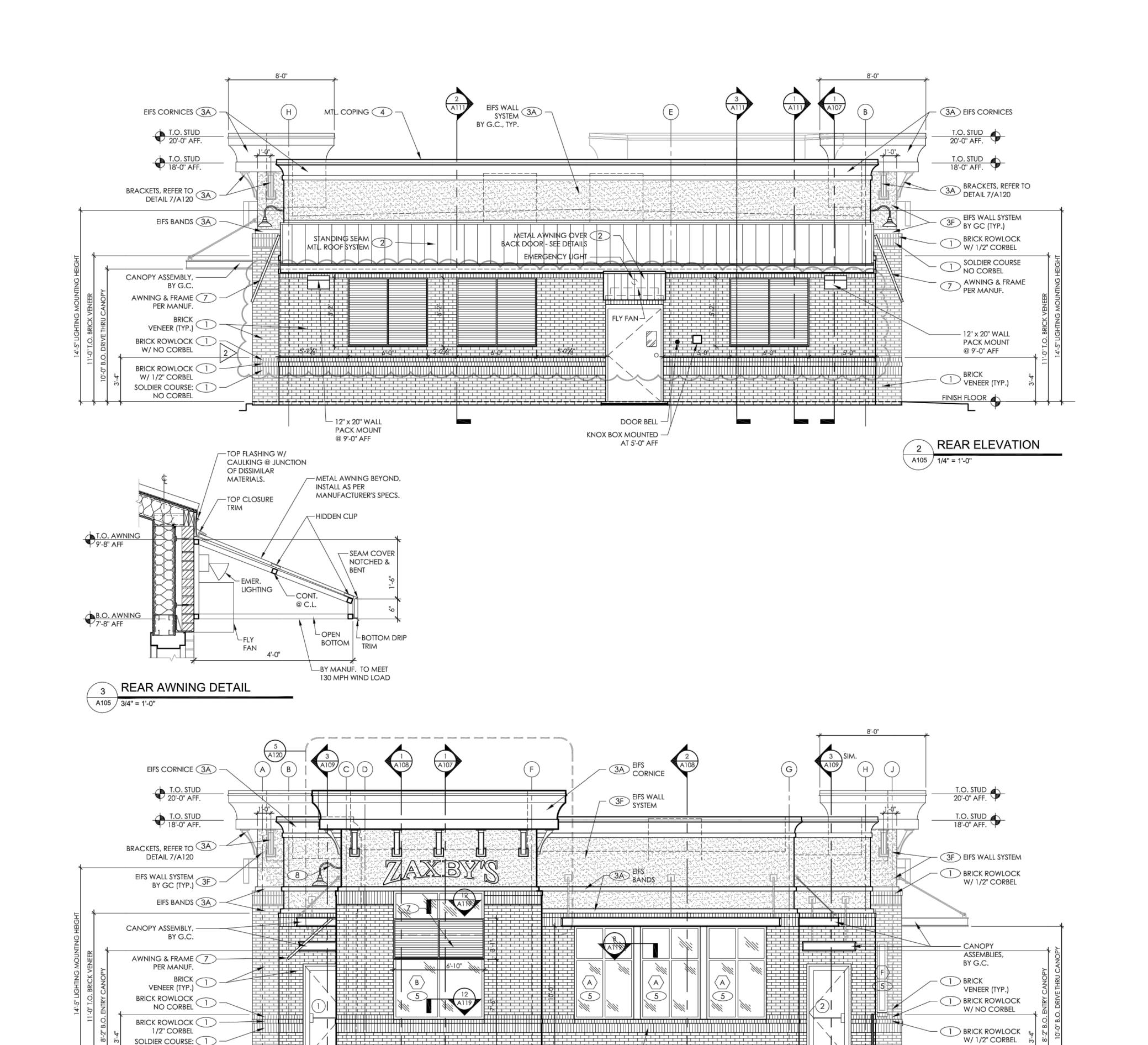
6 SELF-CLOSING & TEMPERED GLASS
DRIVE-THRU WINDOW: ROUGH OPENING = 6'-0" WIDE x 5'-8" HIGH
QUICKSERV MODEL SC-4030 SELF CLOSING HORIZ. SLIDER WINDOW
FIT INTO A "STOREFRONT" FRAME, MATCH STOREFRONT IN COLOR & GLAZING.

QUICKSERV-DIVISION OF MCE SYSTEMS CORP. P.O. BOX 40466, HOUSTON, TX 77240-0466 P. 800-388-8307 F. 713-462-1936 STOREFRONT & SLIDER ALSO BY: QUALITY GLASS P. 706-548-4481

- 7 PLANTATION STYLE AWNINGS
 PROJECT AWNING: ALUM. FRAME W/ RC SLATS, BLACK
 STRUCTURE BY AWNING SUPPLIER
 ANY FABRIC TO MEET: ASTM E84-95, FLAME SPREAD RATING CLASS A (10).
 FIELD VERIFY ALL AWNING CONDITION AND SIZES.
- 8 BLACK GOOSENECK FIXTURES ARE TO BE INSTALLED BY THE G.C.

ALL SPECIFIED MATERIALS MUST BE INSTALLED PER MANUFACTURER'S REQUIREMENTS. ANY DISCREPANCIES OR CONFLICTS BETWEEN THE DRAWINGS AND THE MANUFACTURER'S REQUIREMENTS SHOULD BE IDENTIFIED BY THE CONTRACTOR PRIOR TO INSTALLATION AND DURING THE BID PHASE TO AVOID CHANGE ORDERS.

- 9 LADDER, PAINT BLACK
- GLAZING, REFER TO SHEET A117
- DOORS, REFER TO SHEET A115



SOLDIER COURSE: NO CORBEL

NO CORBEL

* E.I.F.S. NOTE:
THESE DRAWINGS USE "STO" BRAND E.I.F.S. SYSTEMS AS A BASIS OF DESIGN.
"DRYVIT" OUTSULATION PLUS MD SYSTEMS WILL BE ALLOWED AS AN ALTERNATE.
ICC EVALUATION REPORT # ESR - 1543.

ARCHITECTURE
ENGINEERING

3680 Pleasant Hill Road
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f 770.622.9535
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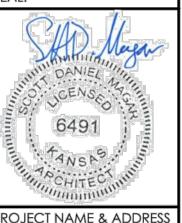
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ISSUED FOR CONSTRUCTION

ISSUE / REVISIONS:

DATE DESCRIPTION:
03-04-16 SUBMIT FOR BID
03-04-16 SUBMIT FOR PERMIT
04.25.16 BUILDING REVISIONS 2

SEAL:



PROJECT NAME & ADDRESS



70 BR (C) - RDT

SHEET TITLE:
FRONT & REAR
EXTERIOR ELEVATIONS

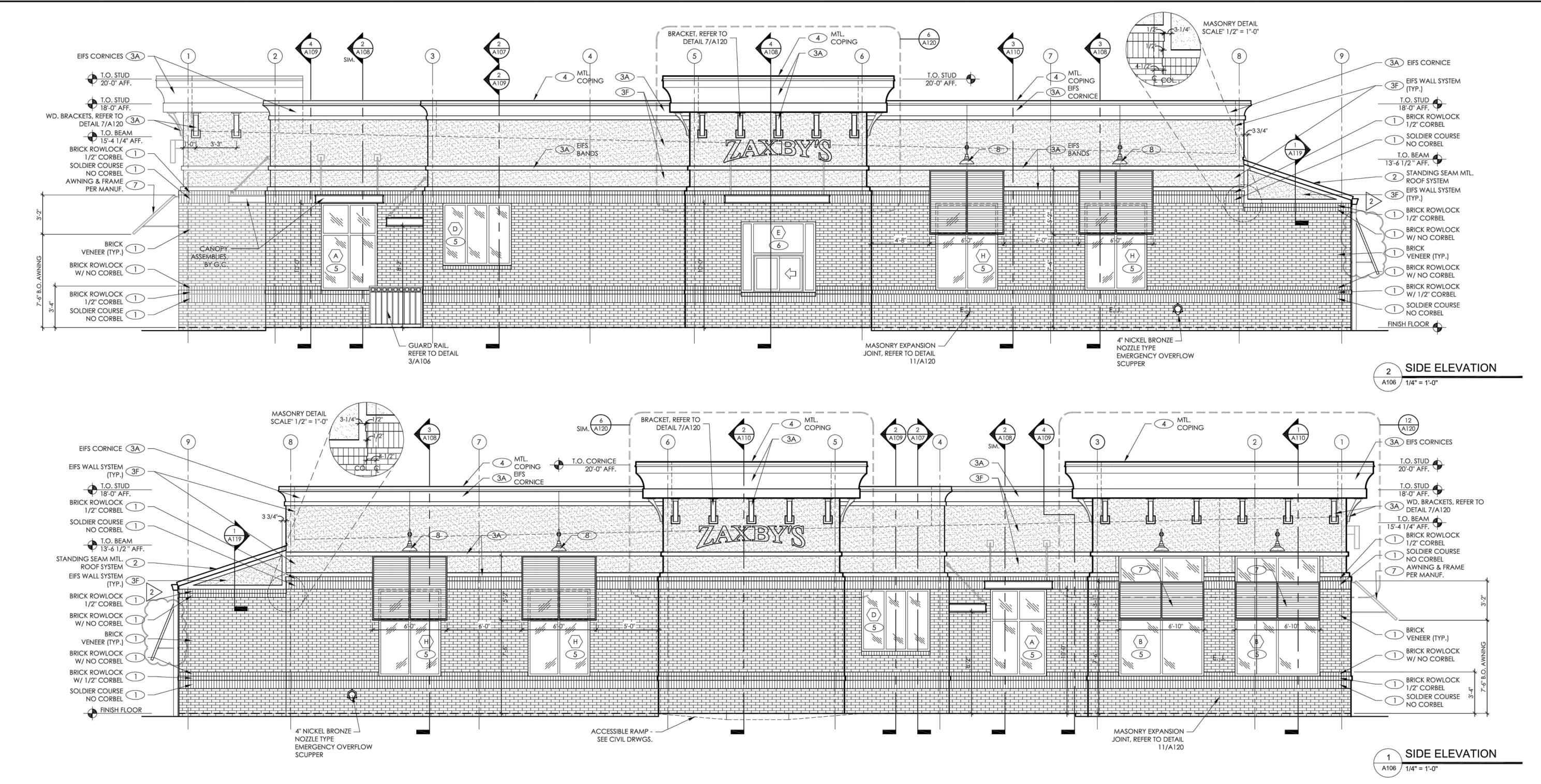
PROJECT #: 16-824.00

FINISH FLOOR

A105 / 1/4" = 1'-0"

FRONT ELEVATION

A105



EXTERIOR FINISH SCHEDULE

ALL MATERIALS & INSTALLATION SHALL CONFORM TO APPLICABLE ASTM STANDARDS UNLESS GOVERNING AUTHORITIES HAVE DIFFERENT REQUIREMENTS.

STANDARD BRICK UNIT 3 5/8" x 2" x 7 5/8" RUNNING 1/2 BOND

1) COLOR: BRICK - MOSSWOOD (BORAL BRICK) MORTAR - BUFF BRICK (BORAL BRICK)

ALTERNATE BRICK:

1) COLOR: BRICK - MOSSTOWN (CHEROKEE BRICK) MORTAR - ANTIQUE BUFF (CEMEX)

> 15# ROOFING FELT OVER ALL SUBSTRATES OR SEALED DENS GLASS GOLD SHEATHING. USE ICE & WATER SHIELD BY W.R. GRACE WHERE INDICATED, SEE SECTIONS &

GROUT VOID SOLID @ FENCE AND BELOW F.F. PROVIDE VAPOR BARRIER AS IDICATED.

CONTINUOUS, THROUGH WALL FLASHING, EPDM (BY W.R. GRACE) OR EQUAL, AT BOTTOM OF WALLS & ABOVE OPENINGS. CUT FLASHING FLUSH WITH EXT. FACE. PROVIDE CELL VENT TYPE WEEPS, ABOVE FLASHING @ 32" O.C. MAX.

3/8" THICK MORTAR JOINTS. ASTM C-270, TYPE N.

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- 2 STANDING SEAM MTL. ROOFING SEE 4
- 3 STO TO HAVE "SWIRL" FINISH AND CAN BE PAINTED.* FINISH COLORS, BELOW AND ON SHEET A-103 CAN BE PAINTED. STO BRAND EIFS ON CMU, OR SHEATHING: "STO ESSENCE" SPEC. E100 ICC EVALUATION REPORT NO. ESR-1720

- (3F) = FIELD COLOR: SHERWIN WILLIAMS ZAXBY'S CREAM (3A) = ACCENT COLOR: SHERWIN WILLIAMS COLOR DARK TAN
- 4 PREFINISHED, METAL COPING / GRAVEL STOP & ROOFING COLOR: COPINGS: MATCH SURROUNDING CORNICE COLOR ROOFING @ REAR SHED: BLACK MATERIAL SHALL BE 24 GAUGE, HOT DIPPED GALVANIZED STEEL OR 0.032" THICK ALUM. FINISH SHALL BE KYNAR 500 FLUOROCARBON COATING.
 - A. BERRIDGE MANUF. CO., HOUSTON, TX.
 - B. PAC-CLAD C. McELROY METAL CO., PEACHTREE CITY, GA.
 - D. APPROVED EQUALS WILL BE ACCEPTED

ACCEPTABLE MANUFACTURES ARE AS FOLLOWS:

PROVIDE ALL RELATED ACCESSORIES: FLASHING, END CLOSURES, GUTTERS, DOWNSPOUTS, ETC. IN MATCHING COLOR. INSTALL ALL MATERIALS PER MANUF.'S RECOMMENDATIONS. A MANUFACTURER'S WARRANTY AGAINST DISCOLORATION & WEATHER-TIGHTNESS WILL BE REQUIRED. SEE SPECIFICATIONS.

NOT ALL ROOF PENETRATIONS & ACCESSORIES ARE SHOWN ON THIS DRAWING. IT IS THE RESPONSIBILITY OF THE G.C. TO INSTALL ITEMS INDICATED ELSEWHERE.

5 STOREFRONT: WINDOWS & ENTRANCE DOORS (BLACK ANODIZED)

ACCEPTABLE MANUFACTURER: KAWNEER, REFER TO SPECIFICATIONS. GLAZING SHALL BE IN ACCORDANCE W/ THE FOLLOWING VALUES:

GLAZING - UNCOATED TINTED GLASS								
APPEARANCE	LOCATION	TYPE & THICKNESS	U FACTOR	SHGC	VLT	VLR		
GRAY TINT	EXTERIOR SIDE	1/4" TEMPERED	.47	.45	39.6	7.3		
CLEAR -NO TINT	INTERIOR SIDE	1/4" TEMPERED	.58	.69	79	15		

CONTACT PERSON @ QUALITY GLASS: JOHNNY DOSTER @ 706-548-4481

6 SELF-CLOSING & TEMPERED GLASS DRIVE-THRU WINDOW: ROUGH OPENING = 6'-0" WIDE x 5'-8" HIGH QUICKSERV MODEL SC-4030-IP SELF CLOSING HORIZ. SLIDER WINDOW FIT INTO A "STOREFRONT" FRAME, MATCH STOREFRONT IN COLOR & GLAZING. | "ZAXBY'S CREAM" - PAINT MIXTURE

QUICKSERV-DIVISION OF MCE SYSTEMS CORP. P.O. BOX 40466, HOUSTON, TX 77240-0466 P. 800-388-8307 F. 713-462-1936 STOREFRONT & SLIDER ALSO BY: QUALITY GLASS P. 706-548-4481

7 PLANTATION STYLE AWNINGS PROJECT AWNING: ALUM. FRAME W/ RC SLATS, BLACK STRUCTURE BY AWNING SUPPLIER ANY FABRIC TO MEET: ASTM E84-95, FLAME SPREAD RATING CLASS A (10). FIELD VERIFY ALL AWNING CONDITION AND SIZES.

8 BLACK GOOSENECK FIXTURES ARE TO BE INSTALLED BY THE G.C.

THESE DRAWINGS USE "STO" BRAND E.I.F.S. SYSTEMS AS A BASIS OF DESIGN. "DRYVIT" OUTSULATION PLUS MD SYSTEMS WILL BE ALLOWED

AS AN ALTERNATE. ICC EVALUATION REPORT # ESR - 1543.

ALL SPECIFIED MATERIALS MUST BE INSTALLED PER MANUFACTURER'S REQUIREMENTS. ANY DISCREPANCIES OR CONFLICTS BETWEEN THE DRAWINGS AND THE MANUFACTURER'S REQUIREMENTS SHOULD BE IDENTIFIED BY THE CONTRACTOR PRIOR TO INSTALLATION AND DURING THE BID PHASE TO AVOID CHANGE ORDERS.

- 9 LADDER, PAINT BLACK
- GLAZING, REFER TO SHEET A117
- DOORS, REFER TO SHEET A115

ACCENT COLOR: SHERWIN WILLIAMS "ZAXBY'S DARK TAN" - PAINT MIXTURE COLORANT 1/32 OZ. 2Y21 BASE 1 GALLON A82W51 A-100 SATIN EXTERIOR LATEX HOUSE PAINT EXTRA WHITE

BASE 1 GALLON A82W51

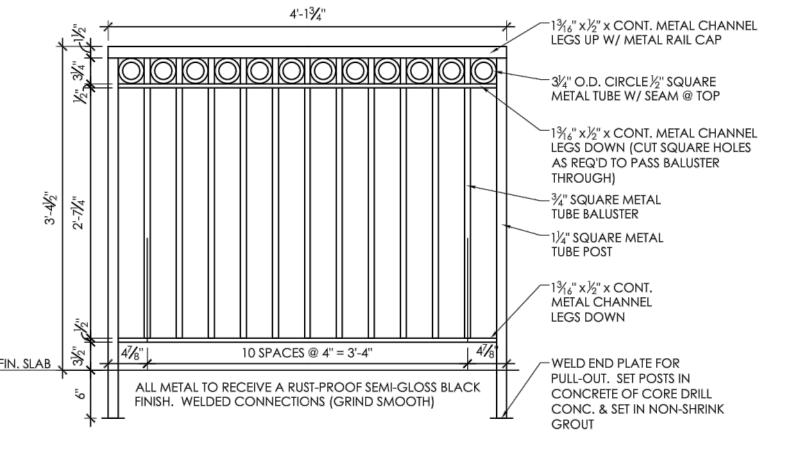
A-100 SATIN EXTERIOR LATEX

HOUSE PAINT EXTRA WHITE

FIELD COLOR: SHERWIN WILLIAMS

1/32 OZ.

COLORANT



DECORATIVE GUARD RAIL

ARCHITECTURE ENGINEERING 3680 Pleasant Hill Road

Suite 200

Duluth, Georgia 30096 p 770.622.9858 f 770.622.9535 www.hillfoleyrossi.com

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ISSUE / REVISIONS: DATE DESCRIPTION: 3-04-16 SUBMIT FOR BID 13-04-16 SUBMIT FOR PERMIT 4.25.16 BUILDING REVISIONS 2>

6491 PCHITECT

PROJECT NAME & ADDRES

PROTOTYPE SERIES 70 BR (C) - RDT

SHEET TITLE: DRIVE THRU & RIGHT **EXTERIOR ELEVATIONS**

PROJECT #:

16-824.00

A106

EXTERIOR FINISH SCHEDULE

ALL MATERIALS & INSTALLATION SHALL CONFORM TO APPLICABLE ASTM STANDARDS UNLESS GOVERNING AUTHORITIES HAVE DIFFERENT REQUIREMENTS.

STANDARD BRICK UNIT 3 5/8" x 2" x 7 5/8" RUNNING 1/2 BOND

1 COLOR: BRICK - MOSSWOOD (BORAL BRICK) MORTAR - BUFF BRICK (BORAL BRICK)

ALTERNATE BRICK:

1 COLOR: BRICK - MOSSTOWN (CHEROKEE BRICK)

MORTAR - ANTIQUE BUFF (CEMEX)

15# ROOFING FELT OVER ALL SUBSTRATES OR SEALED DENS GLASS GOLD SHEATHING GROUT VOID SOLID @ FENCE AND BELOW F.F. PROVIDE VAPOR BARRIER AS INDICATED.

CONTINUOUS, THROUGH WALL FLASHING, EPDM (BY W.R. GRACE) OR EQUAL, AT BOTTOM OF WALLS & ABOVE OPENINGS. CUT FLASHING FLUSH WITH EXT. FACE. PROVIDE CELL VENT TYPE WEEPS, ABOVE FLASHING @ 32" O.C. MAX.

3/8" THICK MORTAR JOINTS. ASTM C-270, TYPE N.

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- = FIELD COLOR: SHERWIN WILLIAMS ZAXBY'S CREAM = ACCENT COLOR: SHERWIN WILLIAMS COLOR DARK TAN
- 4 PREFINISHED, METAL COPING OR GRAVEL STOP & ROOFING COLOR: COPINGS: MATCH SURROUNDING CORNICE COLOR ROOFING @ REAR SHED: BLACK

MATERIAL SHALL BE 24 GAUGE, HOT DIPPED GALVANIZED STEEL OR 0.032" THICK ALUM. FINISH SHALL BE KYNAR 500 FLUOROCARBON COATING. ACCEPTABLE MANUFACTURES ARE AS FOLLOWS:

A. BERRIDGE MANUF. CO., HOUSTON, TX.

- B. PAC-CLAD
- C. McELROY METAL CO., PEACHTREE CITY, GA.
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5 STOREFRONT: WINDOWS & ENTRANCE DOORS (BLACK ANODIZED) ACCEPTABLE MANUFACTURER: KAWNEER, REFER TO SPECIFICATIONS ALL GLAZING SHALL BE IN ACCORDANCE W/ THE FOLLOWING VALUES:

GLAZING - UNCOATED TINTED GLASS								
APPEARANCE	LOCATION	TYPE & THICKNESS	U FACTOR	SHGC	VLT	VLR		
GRAY TINT	EXTERIOR SIDE	1/4" TEMPERED	.47	.45	39.6	7.3		
CLEAR -NO TINT	INTERIOR SIDE	1/4" TEMPERED	.58	.69	79	15		

CONTACT PERSON @ QUALITY GLASS: JOHNNY DOSTER @ 706-548-4481

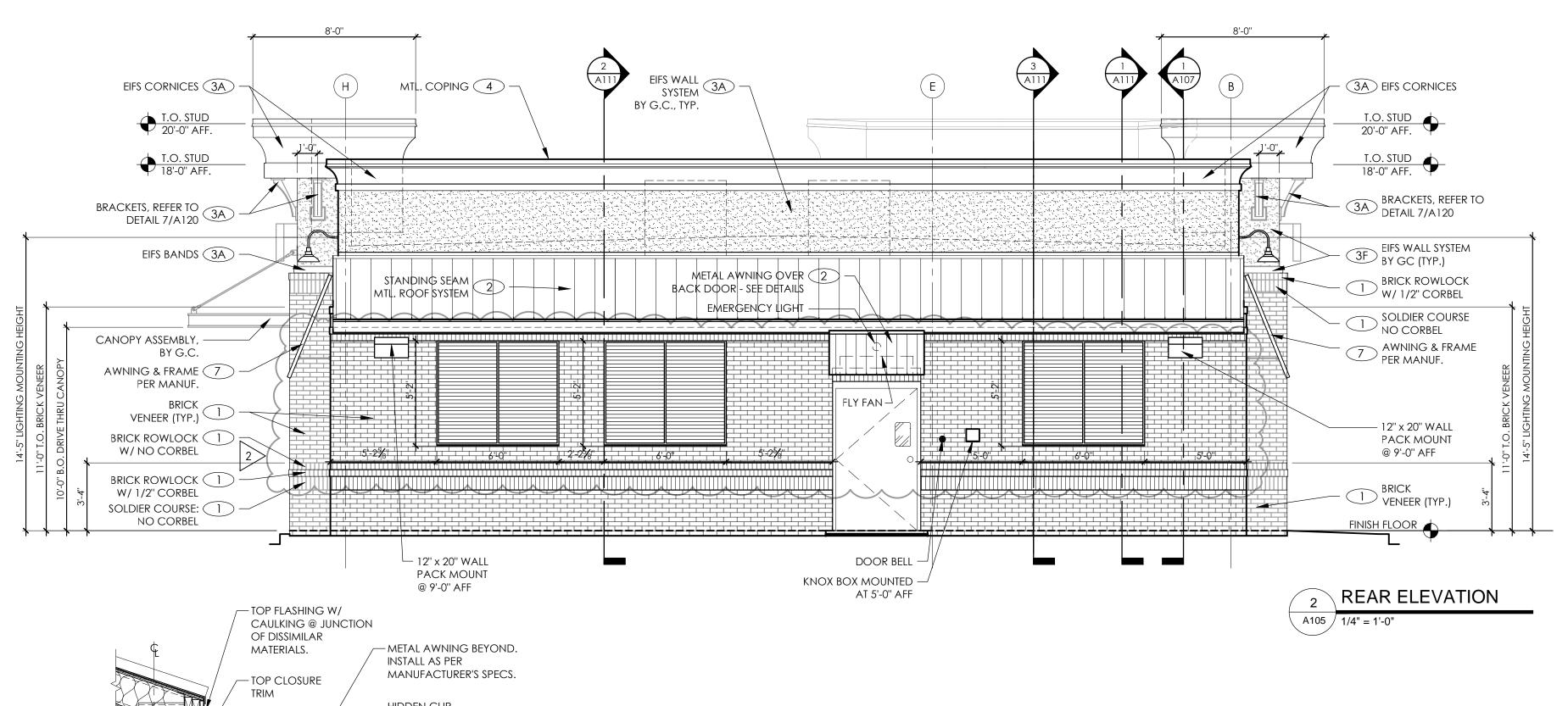
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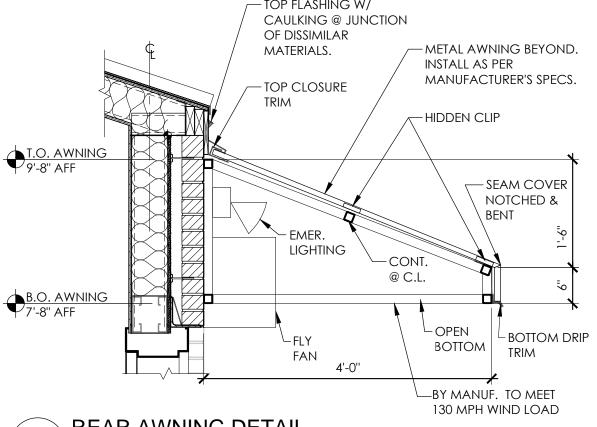
> QUICKSERV-DIVISION OF MCE SYSTEMS CORP. P.O. BOX 40466, HOUSTON, TX 77240-0466 P. 800-388-8307 F. 713-462-1936 STOREFRONT & SLIDER ALSO BY: QUALITY GLASS P. 706-548-4481

- 7 PLANTATION STYLE AWNINGS PROJECT AWNING: ALUM. FRAME W/ RC SLATS, BLACK STRUCTURE BY AWNING SUPPLIER ANY FABRIC TO MEET: ASTM E84-95, FLAME SPREAD RATING CLASS A (10). FIELD VERIFY ALL AWNING CONDITION AND SIZES.
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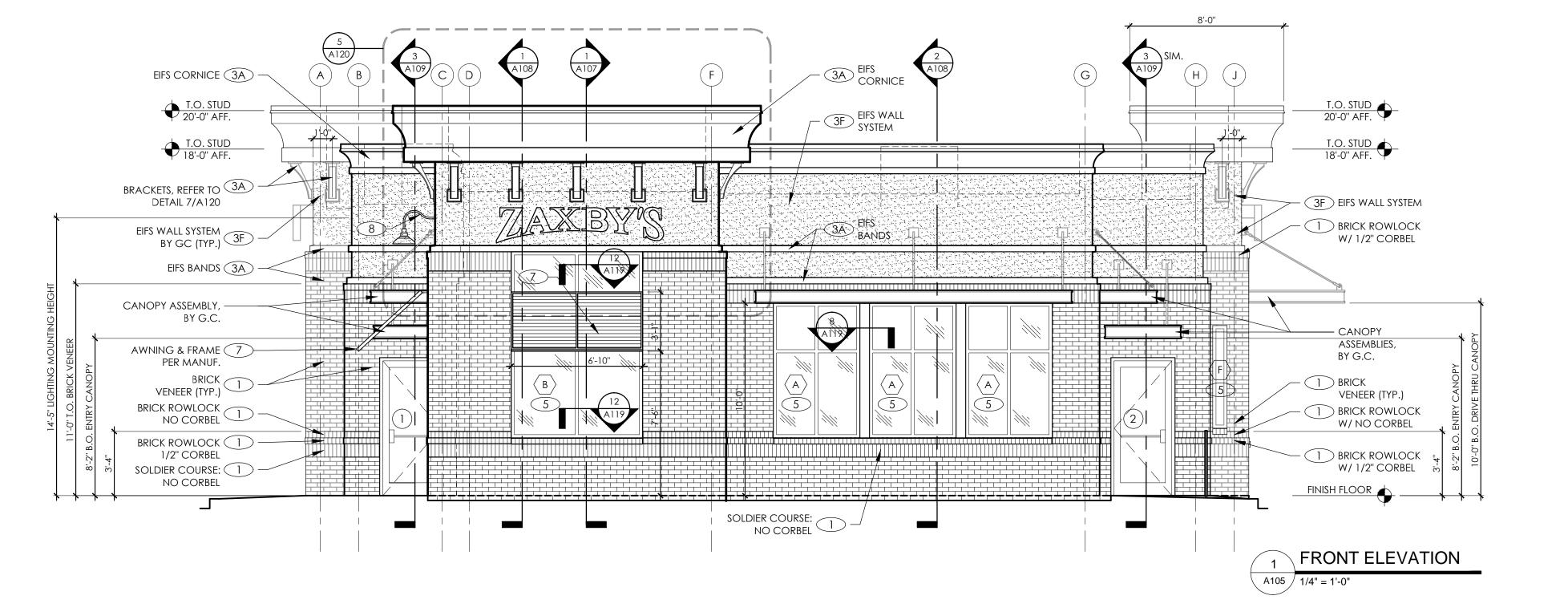
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- 9 LADDER, PAINT BLACK
- GLAZING, REFER TO SHEET A117
- DOORS, REFER TO SHEET A115





REAR AWNING DETAIL A105 $\sqrt{3/4"} = 1'-0"$



ARCHITECTURE ENGINEERING 3680 Pleasant Hill Road Suite 200 Duluth, Georgia 30096

p 770.622.9858

f 770.622.9535

www.hillfoleyrossi.com

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> 6491 POHITEC

PROJECT NAME & ADDRES PROTOTYPE SERIES

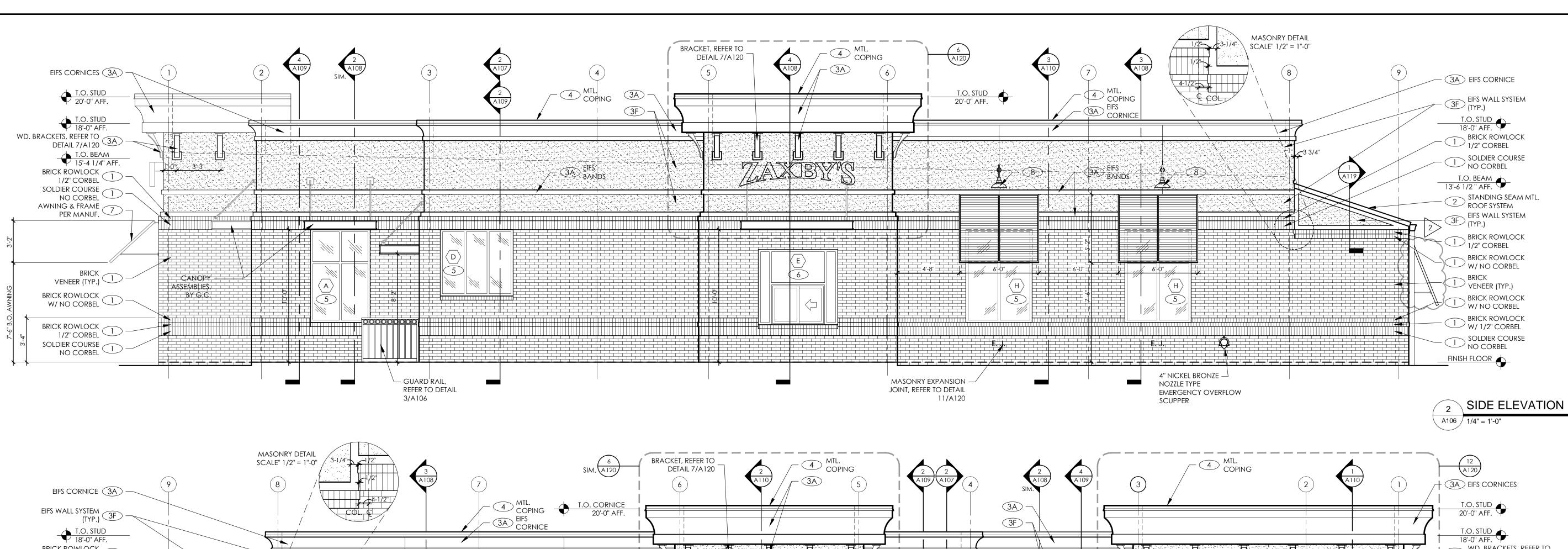
70 BR (C) - RDT

SHEET TITLE: FRONT & REAR **EXTERIOR ELEVATIONS**

> PROJECT #: 16-824.00

> > A105

E.I.F.S. NOTE: THESE DRAWINGS USE "STO" BRAND E.I.F.S. SYSTEMS AS A BASIS OF DESIGN. "DRYVIT" OUTSULATION PLUS MD SYSTEMS WILL BE ALLOWED AS AN ALTERNATE. ICC EVALUATION REPORT # ESR - 1543.



BRICK ROWLOCK 1/2" CORBEL WD. BRACKETS, REFER TO DETAIL 7/A120 SOLDIER COURSE 3 3/4" T.O. BEAM 15'-4 1/4" AFF. NO CORBEL BRICK ROWLOCK 1) 1/2" CORBEL T.O. BEAM 13'-6 1/2 " AFF. SOLDIER COURSE STANDING SEAM MTL. $^{\prime}$ NO CORBEL AWNING & FRAME ROOF SYSTEM PER MANUF. EIFS WALL SYSTEM (TYP.) 3F BRICK ROWLOCK 1/2" CORBEL BRICK ROWLOCK W/ NO CORBEL 6'-0" 6'-10" VENEER (TYP.) VENEER (TYP.) BRICK ROWLOCK BRICK ROWLOCK W/ NO CORBEL W/ NO CORBEL ' BRICK ROWLOCK / BRICK ROWLOCK W/ 1/2" CORBEL 1) 1/2" CORBEL SOLDIER COURSE NO CORBEL SOLDIER COURSE NO CORBEL FINISH FLOOR 4" NICKEL BRONZE -ACCESSIBLE RAMP MASONRY EXPANSION -NOZZLE TYPE SEE CIVIL DRWGS. JOINT, REFER TO DETAIL EMERGENCY OVERFLOW 11/A120 SIDE ELEVATION SCUPPER

EXTERIOR FINISH SCHEDULE

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3/8" THICK MORTAR JOINTS. ASTM C-270, TYPE N.

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- (3F) = FIELD COLOR: SHERWIN WILLIAMS ZAXBY'S CREAM (3A) = ACCENT COLOR: SHERWIN WILLIAMS COLOR DARK TAN
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 - 0.032" THICK ALUM. FINISH SHALL BE KYNAR 500 FLUOROCARBON COATING. ACCEPTABLE MANUFACTURES ARE AS FOLLOWS:
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 - C. McELROY METAL CO., PEACHTREE CITY, GA.
 - D. APPROVED EQUALS WILL BE ACCEPTED

PROVIDE ALL RELATED ACCESSORIES: FLASHING, END CLOSURES, GUTTERS, DOWNSPOUTS, ETC. IN MATCHING COLOR. INSTALL ALL MATERIALS PER MANUF.'S RECOMMENDATIONS. A MANUFACTURER'S WARRANTY AGAINST DISCOLORATION & WEATHER-TIGHTNESS WILL BE REQUIRED. SEE SPECIFICATIONS.

NOT ALL ROOF PENETRATIONS & ACCESSORIES ARE SHOWN ON THIS DRAWING. IT IS THE RESPONSIBILITY OF THE G.C. TO INSTALL ITEMS INDICATED ELSEWHERE.

5 STOREFRONT: WINDOWS & ENTRANCE DOORS (BLACK ANODIZED)

ACCEPTABLE MANUFACTURER: KAWNEER, REFER TO SPECIFICATIONS. GLAZING SHALL BE IN ACCORDANCE W/ THE FOLLOWING VALUES:

GLAZING - UNCOATED TINTED GLASS

APPEARANCE	LOCATION	TYPE & THICKNESS	U FACTOR	SHGC	VLT	VLR
GRAY TINT	EXTERIOR SIDE	1/4" TEMPERED	.47	.45	39.6	7.3
CLEAR -NO TINT	INTERIOR SIDE	1/4" TEMPERED	.58	.69	79	15
CONTAC	t person @ Quali	TY GLASS: JOHNNY	DOSTER @ 706-	 548-4481		

6 SELF-CLOSING & TEMPERED GLASS DRIVE-THRU WINDOW: ROUGH OPENING = 6'-0" WIDE x 5'-8" HIGH QUICKSERV MODEL SC-4030-IP SELF CLOSING HORIZ. SLIDER WINDOW FIT INTO A "STOREFRONT" FRAME, MATCH STOREFRONT IN COLOR & GLAZING. | "ZAXBY'S CREAM" - PAINT MIXTURE

- QUICKSERV-DIVISION OF MCE SYSTEMS CORP. P.O. BOX 40466, HOUSTON, TX 77240-0466
- P. 800-388-8307 F. 713-462-1936 STOREFRONT & SLIDER ALSO BY: QUALITY GLASS P. 706-548-4481
- 7 PLANTATION STYLE AWNINGS PROJECT AWNING: ALUM. FRAME W/ RC SLATS, BLACK STRUCTURE BY AWNING SUPPLIER ANY FABRIC TO MEET: ASTM E84-95, FLAME SPREAD RATING CLASS A (10). FIELD VERIFY ALL AWNING CONDITION AND SIZES.
- 8 BLACK GOOSENECK FIXTURES ARE TO BE INSTALLED BY THE G.C.

ALL SPECIFIED MATERIALS MUST BE INSTALLED PER MANUFACTURER'S REQUIREMENTS. ANY DISCREPANCIES OR CONFLICTS BETWEEN THE DRAWINGS AND THE MANUFACTURER'S REQUIREMENTS SHOULD BE IDENTIFIED BY THE CONTRACTOR PRIOR TO INSTALLATION AND DURING THE BID PHASE TO AVOID CHANGE ORDERS.

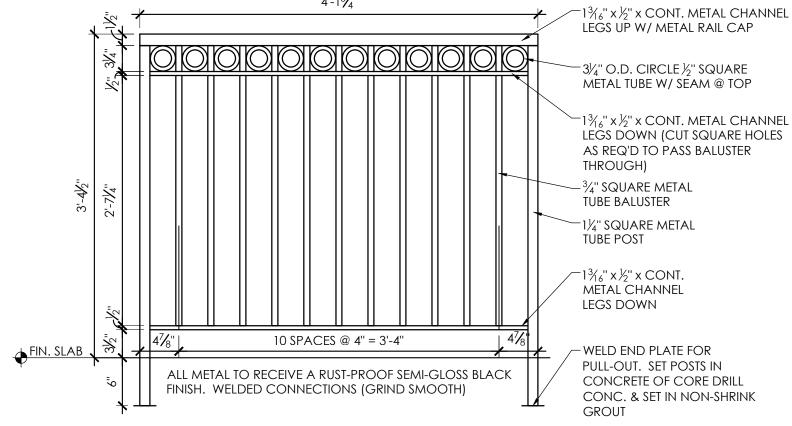
- 9 LADDER, PAINT BLACK
- GLAZING, REFER TO SHEET A117
- DOORS, REFER TO SHEET A115

THESE DRAWINGS USE "STO" BRAND E.I.F.S. SYSTEMS AS A BASIS OF DESIGN. "DRYVIT" OUTSULATION PLUS MD SYSTEMS WILL BE ALLOWED AS AN ALTERNATE. ICC EVALUATION REPORT # ESR - 1543.

FIELD COLOR: SHERWIN WILLIAMS COLORANT 1/32 OZ. BASE 1 GALLON A82W51 A-100 SATIN EXTERIOR LATEX

HOUSE PAINT EXTRA WHITE ACCENT COLOR: SHERWIN WILLIAMS "ZAXBY'S DARK TAN" - PAINT MIXTURE

COLORANT 1/32 OZ. 2Y21 BASE 1 GALLON A82W51 A-100 SATIN EXTERIOR LATEX HOUSE PAINT EXTRA WHITE



\ DECORATIVE GUARD RAIL

ARCHITECTURE

ENGINEERING

3680 Pleasant Hill Road

Suite 200 Duluth, Georgia 30096 p 770.622.9858 f 770.622.9535 www.hillfoleyrossi.com

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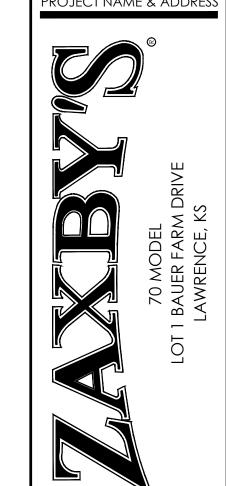
ISSUED FOR

CONSTRUCTION ISSUE / REVISIONS: DATE DESCRIPTION: -04-16 SUBMIT FOR BID

03-04-16 SUBMIT FOR PERMIT 04.25.16 BUILDING REVISIONS



PROJECT NAME & ADDRES



PROTOTYPE SERIES

70 BR (C) - RDT

SHEET TITLE: DRIVE THRU & RIGHT **EXTERIOR ELEVATIONS**

PROJECT #: 16-824.00

A106



Traffic Impact Study for

Revised Bauer Farm PCD

6th St., between Wakarusa Dr. and Folks Rd.

Lawrence, Kansas

Prepared for Treanor Architects P.A.

> Prepared By



<u>Servina Communities Through Excellence</u> Kansas - Missouri - Michigan - California



Mehrdad Givechi, P.E., P.T.O.E.

February 2014

Trip Generation Summary - Proposed Changes (Jan 2014) + Undeveloped Average Weekday Driveway Volumes

Project: Entire Bauer Farm Development (Revised)

Alternative: Proposed Changes (Jan 2014) + Undeveloped

Open Date: 1/14/2014

Analysis 1/14/2014

			Avera	age Daily	Trips		l Peak H			1 Peak H ent Street	
ITE Land Use			Enter_	_Exit_	Total	_Enter_	_Exit_	_Total_	_Enter_	_Exit_	_Total_
220	APT 1		406	405	811	12	50	62	49	27	76
	122 Dwelling U	Inits									
310	HOTEL 1		441	441	882	34	23	57	33	32	65
	108 Rooms										
826	CENTERSPECIAL	ΓΥ 1	643	642	1285				35	44	79
	29 Gross Lea	sable Area 1000 SF									
850	SUPERMARKET 1		1393	1392	2785	58	35	93	132	126	258
	27.24 Gross Floo	or Area 1000 SF									
912	BANKDRIVEIN 1		371	370	741	34	26	60	61	61	122
	5 Gross Floo	or Area 1000 SF									
932	RESTAURANTHT	1	401	400	801	37	31	68	37	25	62
	6.3 Gross Floo	or Area 1000 SF									
934	FASTFOODDT 1		670	670	1340	63	60	123	46	42	88
	2.7 Gross Floo	or Area 1000 SF									
Llaadi	usted Driveway Volu	mo	4325	4320	8645	238	225	463	393	357	750
•	usted Driveway volu	IIIC	0	4320	0	31	29	60	116	106	222
-	al Capture Trips		755	755	1510	0	0	0	49	49	98
			0.570	0505	7405			400	0.4.4	000	
-	ted Driveway Volume)	3570	3565	7135	238 31	225 29	463	344 102	308 91	652 193
•	ted Pass-By Trips ted Volume Added to	Adjacent Streets	0 3570	0 3565	0 7135	207	29 196	60 403	242	91 217	459
Aujus	tod volume Added to	Trajacent Otrocts	3370	3303	7 100	201	130	700	272	211	700

Total AM Peak Hour Internal Capture = 0 Percent

Total PM Peak Hour Internal Capture = 13 Percent

PLANNING COMMISSION REPORT Public Hearing Item

PC Staff Report 05/23/16

ITEM NO. 3 FINAL DEVELOPMENT PLAN FOR WESTRIDGE WASH, LUBE, & AUTO SALES; 3530 W 6TH ST (KES)

FDP-16-00103: Consider a Final Development Plan for Westridge Wash, Lube & Auto Sales. The site, located at 3530 W. 6th Street, contains approximately 0.77 acres. Submitted by Grob Engineering Services, LLC, for Westridge Lawrence, LLC, the property owner of record.

STAFF RECOMMENDATION: Planning staff recommends approval of the Final Development Plan based upon the findings of fact presented in the body of the Staff Report subject to the following conditions:

1. Provision of an executed Site Plan Performance Agreement prior to the recording of the Final Development Plan.

Applicant's

Reason for Request: The owner desires to move an existing building and construct a new

building where the easement is located.

Design Standards to Consider

- Consistency with approved Final Development Plan, FDP-04-08-04.
- Consistency with Article 10 of the 1966 Zoning Ordinance.
- Consistency with standards in the 2006 Development Code.

KEY POINTS:

- The property is zoned PCD (Planned Commercial Development).
- The Final Development Plan is a revision to the previously approved Final Development Plan.
- Final Development Plan for projects with previous approvals prior to the adoption of the 2006 Land Development Code require Planning Commission approval.

ASSOCIATED CASES

- Raco Westridge Addition: Final Development Plan.
- LS-04-03-05: Lot Split, Westridge Number Six Subdivision.
- FDP-04-08-04: Westridge Number Six Subdivision. Recorded on June 21, 2004.

OTHER ACTION REQUIRED

- When conditions have been met, the Final Development Plan will be recorded at the Douglas County Register of Deeds Office and released to Development Services for processing of building permits.
- Submittal of building permit application and construction plans. Construction may commence only after building permits have been issued.

PUBLIC COMMENT

• No public comment was received prior to the printing of this staff report.

PLANS AND STUDIES REQUIRED

- *Traffic Study* Study was not required as the proposed development was similar to that shown on previously approved Preliminary Development Plan.
- Downstream Sanitary Sewer Analysis Study was not required.

• Drainage Study – Study was not required.

GENERAL INFORMATION

Current Zoning and Land Use: PCD-[Westridge] (Planned Commercial Development)

District; Westridge Wash and Lube.

Surrounding Zoning: North: RS7 (Single-Dwelling Residential) District;

Detached Dwellings and PDC-[Westridge]; Multi-Dwellings.

East: PCD-[Westridge] (Planned Commercial Development) District; *Restaurant, Bank.*

South: CS (Commercial Strip) District; Commercial Strip

Center, Westlake Hardware.

West: GPI (General Public and Institution) District; Fire

Station. (Figure 1)

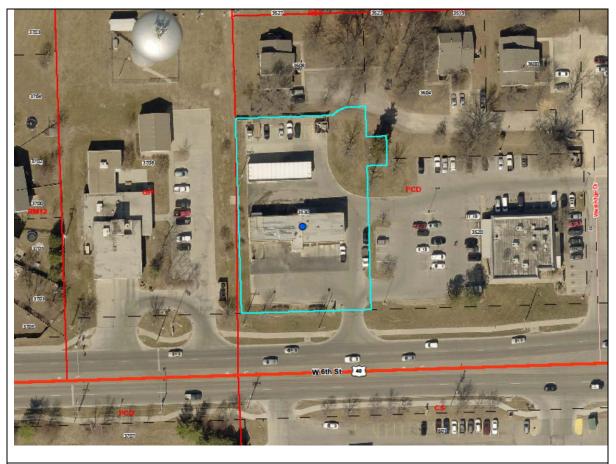


Figure 1. Zoning in the area.

STAFF SUMMARY

The revision to the previously approved Final Development Plan (FDP-04-08-04), proposes a new 36′ X 70′ service building on the northeast portion of the lot. The placement of the new structure will be an encroachment into open space which means there will be a reduction in the common open space that was approved on the previous Final Development Plan per Section 1013(b), Article 10 of the 1966 Zoning Ordinance. A reduction in common open space requires the revision to be heard by the Planning Commission.

SITE SUMMARY									
	Existing	Proposed	Change						
Land Use:	Developed Car Wash	Car Wash, Lube and Sales	No						
Land Area:	33,528 sq. ft.	33,528 sq. ft.							
Total Building:	4,693 sq. ft. (13%)	5,719 sq. ft. (17%)	+ 1,026 sq. ft.						
Total Pavement:	19,814 sq. ft. (59%)	19,814 sq. ft. (59%)							
Total Impervious Area:	24,507 sq. ft. (74%)	25,133 sq. ft. (75%)	+ 626 sq. ft.						
Total Pervious Area:	8,983 sq. ft. (26%)	8,395 sq. ft. (26%)	- 588 sq. ft.						

PARKING SUMMARY				
Use	Req. per Sec. 20-902	Parking Required	Parking Provided	
Car Wash	2 spaces per service bay 2 spaces per car wash 1 space per 500 Sq. ft. Open Rental/Sales area	4 Service Bays 1 Car wash 324 sq. ft. open sales (2 spaces)	8 spaces/service 2 spaces/wash 1 space /sales Total 11 Spaces	
Bicycle	0	0	0	
ADA Spaces	Req. per Sec. 20-912	1 accessible space	1 provided	

STAFF REVIEW

The property addressed as 3530 W. 6th Street includes an existing Car Wash. The proposed development includes relocating two vacuum bays and constructing a new 36' X 70' service building on the north portion of the lot just to the west of the open space. (**Figure 2**)

There is a cross-access/utility easement that was dedicated in a previous plat but has not been used. Replatting has split the property and the access was blocked by a dumpster and privacy fence. A vacation of easement request accompanies the Final Development Plan application and is scheduled for the June 7th City Commission meeting.

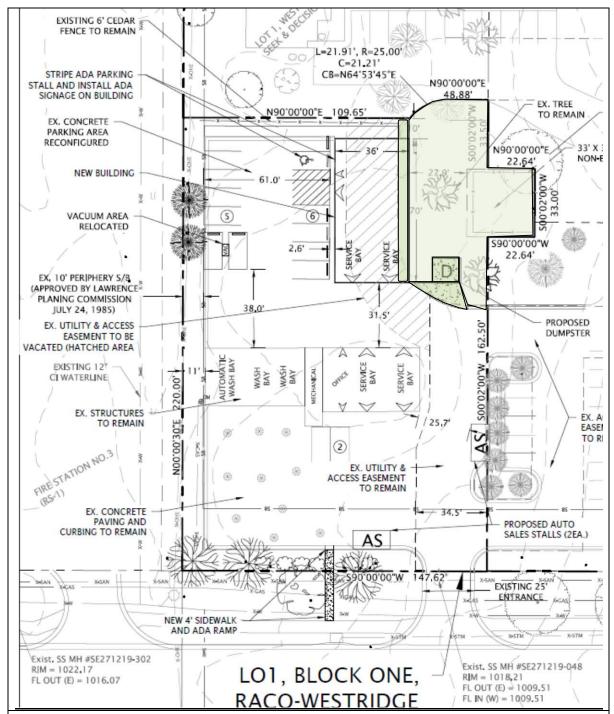


Figure 2. Proposed development and common open space encroachment. (shaded darker green) Remaining open space on the lot is lighter green.

Common Open Space

Per Section 20-701(j) of the Development Code, a minimum of 20% of the total site area must be provided as Common Open Space. This is defined as being open space that is available for the use and enjoyment of all the tenants of the development. The plan identifies the Common Open Space that was approved with the previous plat on this lot. (**Figure 2**).

The proposed building and dumpster location will encroach into the common open space set aside on the previously approved Final Development Plan. The building encroaches by approximately 5' X 70' along the edge of the existing open space resulting in a reduction of roughly 350 sq. ft. of open space. The new dumpster location will also encroach into the open space for an additional reduction of approximately 120 sq. ft. The total reduction in open space will be 470 sq. ft. (**Figure 2**).

Access and Circulation

The property access will remain the same. Parking is being provided to the west of the proposed service building.

Pedestrian Connectivity

A sidewalk connection is to existing sidewalk on W. 6th St. has been included with this proposal.

Lighting

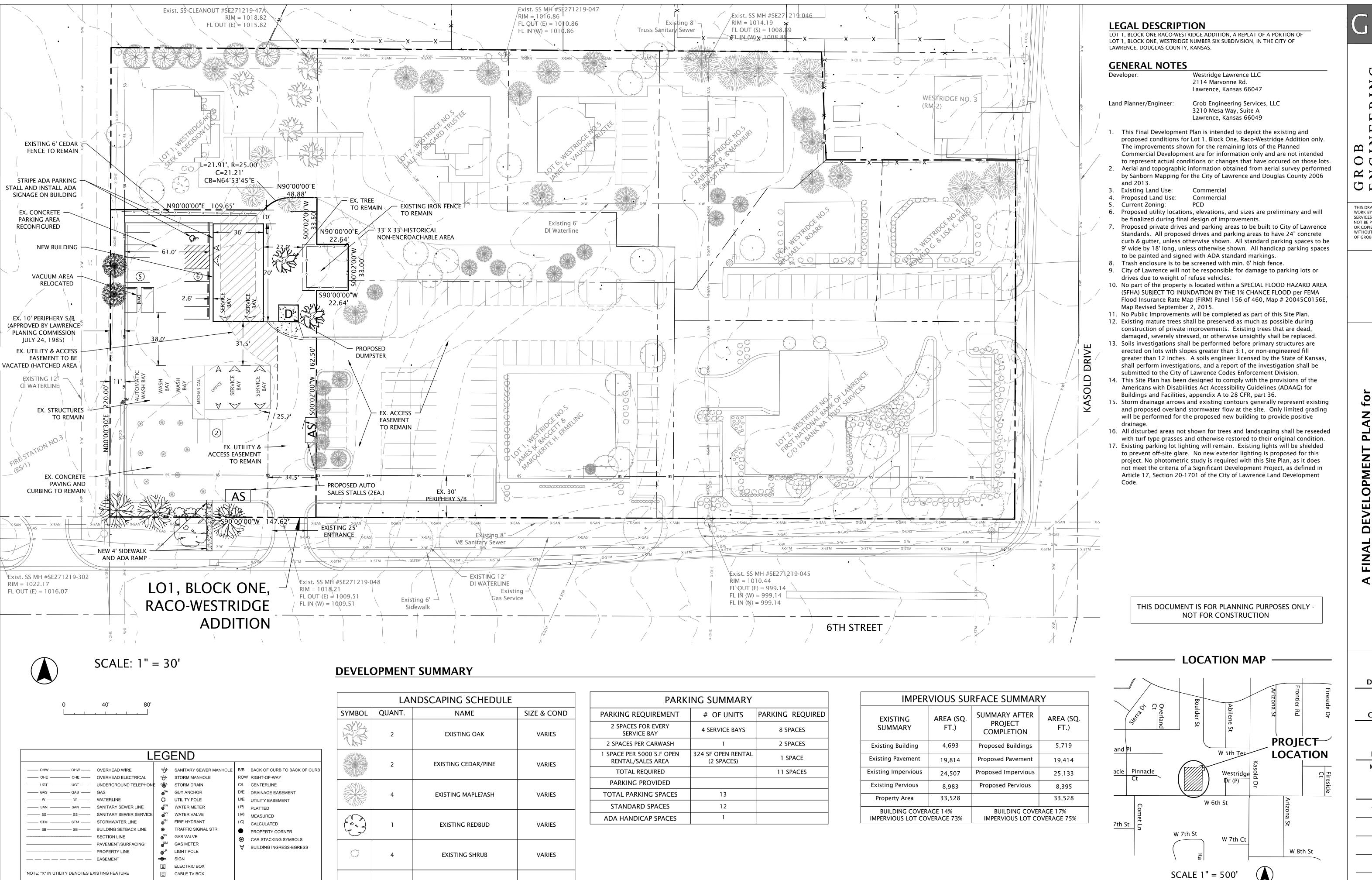
No exterior lighting is being proposed with this plan.

Landscaping

No proposed changes to landscaping. The existing landscaping is to remain.

Conclusion

The development proposed with this Final Development Plan conforms with the previously approved Final Development Plan and is compliant with the provisions of the Development Code.



a final development plan for WESTRIDGE WASH, LUBE & AUTO SALES

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UTO LUBE A FINAL I WESTRIDGE W

> **DESIGNED BY CHECKED BY**

> > JDG

ISSUE DATE March 21, 2016

REVISIONS

April 28, 2016

SHEET

PLANNING COMMISSION REPORT Regular Agenda —Public Hearing Item

PC Staff Report 5/23/16

ITEM NO. 4 CONDITIONAL USE PERMIT; CLEAN RUBBLE LANDFILL; 1736 E 1550 ROAD

CUP-16-00105: Consider a Conditional Use Permit for clean rubble landfill, on approximately 40 acres located at 1736 E 1550 Rd. Submitted by Grob Engineering Services, LLC, for Nunemaker-Ross, Inc., property owner of record.

STAFF RECOMMENDATION: Staff recommends approval of the CUP request for a clean rubble landfill at 1736 E 1550 Road subject to the following conditions:

- 1. Prior to the placement of this item on the Board of County Commissioner's agenda, the applicant will work with Grant Township and the County Engineer to determine an acceptable truck route from Hwy 24 to the site and establish maintenance provisions.
- 2. Truck traffic associated with this use is not permitted on City streets except to access the nearest State or Federal Highway.
- 3. Only 'Clean Rubble' which meets the KDHE definition of: "....the following types of construction and demolition waste: concrete and concrete products including reinforcing steel, asphalt pavement, brick, rock and uncontaminated soil as defined in rules and regulations adopted by the secretary." shall be used to fill the Borrow Pit.
- 4. When the Borrow Pit has been filled as shown on the plan, top soil shall be applied and the property seeded with brome or fescue type grasses. Top soil application and seeding shall be completed within 1 year following the completion of filling and grading activities on the site.
- 5. The Conditional Use Permit will be subject to a 5 year administrative review and will expire 10 years after the date of the Board of County Commissioners approval.
- 6. Submittal of a revised plan which lists the Conditions and Restrictions of Use 2-4 listed above and includes the approved truck route and maintenance measures.

Reason for Request: "The owner would like to fill a depressed area where soil had previously been removed."

KEY POINTS

- The property had been used as a borrow pit for improvements to I-70 in 2009 through 2010.
- A borrow pit is classified as a *Mining and Excavation* use and requires approval of a Conditional Use Permit (CUP); however, a CUP was not obtained for this borrow pit. A borrow pit CUP would include steps for the reclamation of the pit and note whether it would be converted into a pond or filled. In the absence of a CUP for the borrow pit, the addition of fill to the site will be processed as a landfill.
- The Zoning Regulations contain a list of land uses which are allowed with a Conditional Use Permit. Uses which are similar in nature and operational characteristics to the listed uses are generally included as a conditional use under that classification, even though they vary from the listed use. The landfill use proposed is most similar to the Conditional Use classification of a 'Sanitary Landfill'.
- The subject property contains approximately 40 acres. With this CUP, clean rubble fill would be placed on approximately 4.95 acres of the subject property.

OTHER ACTION REQUIRED

- Approval of the Conditional Use application and plan by the Board of County Commissioners.
- Applicant shall obtain a permit for the Conditional Use from the Zoning and Codes Office prior to commencing the use.

PUBLIC COMMENT

No public comment was received prior to printing this report.

GENERAL INFORMATION

Current Zoning and Land Use: A (County-Agricultural) District with F-F (County-Floodway Fringe Overlay) District; Residential Detached Dwelling and a lake which resulted from a sand pit. A CUP for the stabilization of the sand pit banks was approved in 1992.

Surrounding Zoning and Land Use: (Figure 1)

To the north: A (Agricultural) District with F-F (Floodway Fringe) Overlay District / and IG (City-General Industrial) District; I-70 Highway and right-of-way adjacent to the property. North of the highway: Agriculture, Residential Detached Dwelling, and the municipal airport in the City IG District.

To the west: I-1 (Limited Industrial) District; Agriculture.

To the south: A (Agricultural) and I-3 (Heavy Industrial) Districts with F-F (Floodway Fringe) and F-W (Floodway) Overlay Districts; *Agriculture* and Residential Detached Dwelling.

To the east: A (County-Agricultural) District with F-F (Floodway Fringe) Overlay District; Agriculture and a lake resulting from sand pit.

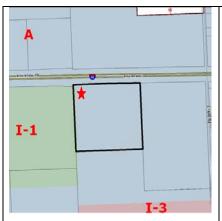


Figure 1a. Zoning: A (Agricultural), I-1 (Limited Industrial), IG (City-General Industrial) Districts. General location of proposed landfill marked with a star.



Figure 1b. Floodplain in the area.



Figure 1c. Land Use in the area. Lake is result of a sand pit in the area. A CUP for a landfill to stabilize the banks of the sand pit was approved in 1992.

Summary of Request

The subject property at 1736 E 1550 Road, approximately 40 acres, contains a depression of approximately 5 acres that was the result of a borrow pit for I-70 improvements in 2009 through 2010, a residence, several accessory structures, and a portion of a large lake which was developed with the reclamation of a sand pit. (Figure 2) The borrow pit operated without the required Conditional Use Permit; therefore, reclamation was not addressed. Reclamation of the pit through filling requires approval of a



Figure 2. Aerial of subject property with general area of landfill circled.

Conditional Use Permit. Adding clean rubble to the site to fill the borrow pit has been determined to be similar in nature and operational characteristics to the Conditional Use 'Sanitary Landfill which is listed in Section 12-319-4 of the Zoning Regulations as a use which may be approved in any district in which it is not permitted as a Conditional Use. The CUP is strictly for the addition of fill to the abandoned borrow pit.

CONDITIONAL USES:

The following proposed use is permitted in the A District when approved with a Conditional Use Permit, as noted below:

12-319-4.12 Sanitary Landfill

The proposed conditional use has been reviewed with the following criteria provided in Section 12-319-1.02 of the Zoning Regulations:

I. ZONING AND USES OF PROPERTY NEARBY

Nearby property is zoned A (Agricultural), I-1 (Limited Industrial), I-3 (Heavy Industrial), and F-F (Floodway Fringe Overlay) Districts and is used primarily for *Agriculture*, *Residential Detached Dwellings* and contains a large lake. This lake was created with the reclamation of a sand pit which was in operation prior to the adoption of the 1966 Zoning Regulations.

Staff Finding – Surrounding land uses are predominantly agricultural in nature. Open space is available in the area in the form of a large lake. The proposed Conditional Use should be compatible with the zoning and uses in the area.

II. CHARACTER OF THE AREA

The property is located within the Urban Growth Area of the City of Lawrence and is approximately one-half mile north of the city limits. The area is bounded on the north by Interstate 70, on the south by the railroad, on the west by E 1500 Road (N 7th Street) and on the east by the boundary of the Urban Growth Area. The subject property is located on the north boundary of this area which includes a mix of urban residential, commercial and industrial land uses within the City limits and primarily agricultural or rural residential land uses in the rural area.

Staff Finding – The area is predominately agricultural in character but contains urban uses to the west and southwest within the city limits.

III. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED

Applicant's Response:

"The depressed area is not uniform in shape to hold a sustainable pool of water and thus allow weeds to grow without a good means of keeping the area mowed. Property could remain as is, but not desirable."

The subject property is zoned A (Agricultural). Uses allowed in the A District include: farms, truck gardens, orchards, or nurseries for the growing or propagation of plants, trees and shrubs in addition other types of open land uses. It also includes residential detached dwellings, churches, hospitals and clinics for large and small animals, commercial dog kennels, and rural home occupations.

A lake covers the majority of the property leaving land along the west boundary and in the northwest portion of the parcel. The abandoned borrow pit creates an uneven area in the northwest portion that is not usable for agriculture. The applicant noted that the grade change makes it difficult to maintain the property. Filling this area and grading and seeding it will make it more suitable for use. The approved plan for the area, the *Northeast Sector Plan* recommends open space uses for this area. (Figure 3)

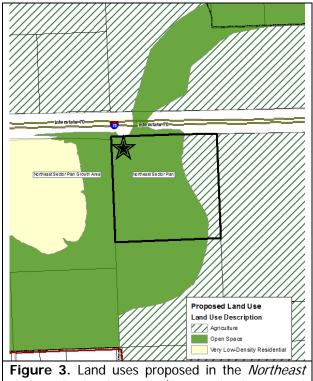


Figure 3. Land uses proposed in the *Northeast Sector Plan* for this area. (Star marks the general location of the Borrow Pit.)

Staff Finding – The property is not well suited

for the uses to which it is restricted with the A district due to the large lake on the property and the uneven terrain caused by the abandoned borrow pit. Filling the borrow pit and seeding it will result in an area that is suitable for uses permitted within the A District as well as the open space use recommended in the *Northeast Sector Plan*.

IV. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Staff Finding – The property is not vacant. The Douglas County Appraiser's records indicate that the residence on the property was built in 1997. A sand pit which operated on this property to facilitate the paving of I-70 before the adoption of the Zoning Regulations in 1966 was reclaimed as a large lake.

V. EXTENT TO WHICH REMOVAL OF RESTRICTIONS WILL DETRIMENTALLY AFFECT NEARBY PROPERTY

Applicant's Response:

"The removal of restrictions should have minimal affect on nearby property as the owner owns the property on 2 sides and the Kansas Turnpike Authority owns the property on the other 2 sides."

Section 12-319-1.01 of the County Zoning Regulations recognize that "certain uses may be desirable when located in the community, but that these uses may be incompatible with other

uses permitted in a district...when found to be in the interest of the public health, safety, morals and general welfare of the community may be permitted, except as otherwise specified in any district from which they are prohibited."

The proposed CUP will allow a landfill use for the reclamation of a Borrow Pit. The Borrow Pit, if properly permitted, would have included a reclamation plan. Reclamation typically includes the development of a pond or filling of the depression. When filling is proposed, it is important to determine the type of fill that will be utilized and to consider the impacts of the truck traffic on properties along the delivery route. Filling the pit will result in a more level grade that will facilitate mowing and other maintenance activities, as well as preventing standing water.

The application notes that this will be a clean rubble fill. KDHE provides the following definition: "Clean rubble means the following types of construction and demolition waste: concrete and concrete products including reinforcing steel, asphalt pavement, brick, rock and uncontaminated soil as defined in rules and regulations adopted by the secretary." This restriction on the type of fill should be included with the CUP as a condition.

The activity associated with the filling of the borrow pit would be trucks accessing the site to dump the clean rubble and heavy equipment to move the fill into place and to grade the site. The noise associated with this heavy equipment should not impact nearby properties as they are

in agricultural use, with the exception of Interstate 70 to the north. No exterior lighting is being proposed. Truck traffic to and from the site could impact residents along the truck route. The application notes that truck traffic will utilize the following route to access the property: N 2nd Street/Hwy 40/59 then east on Lyon Street or Locust Street, then north on E 1550 Road/N 9th Street. (Figure 4)

The applicant estimated that it would take 1,500 to 2,000 truckloads to fill the pit. The owner will not be paying for fill material so it will be delivered when contractors choose to deposit. Traffic to the site will vary with construction activity; there could be 30 to 40 trucks a day to the site, and then weeks with none. E 1550 Road/N 9th Street is gated approximately 188 ft north of the intersection with Lyon Street (Figure 4). Loads would be delivered by appointment only.



Figure 4. Proposed truck route shown with red arrows. Dotted lines show route past the gate on E 1550 Road. White line is a gate on E 1550 Road/N 9th Street.

The City Public Works Director indicated that the city streets proposed for a route to the landfill were not designed to accommodate heavy truck traffic. He noted the standard route for trucks with destinations outside the City limits (Figure 5) should be used to exit the City and that large trucks are permitted to use local streets only to access the highway system.

The County Engineer noted that E 1600 Road is also not designed to handle heavy truck traffic and that a maintenance agreement would need to be entered into with Grant Township before E 1600 Road could be used for trucks to access the site. The CUP will be placed on the Board of County Commissioners agenda for consideration when the truck route has been determined and a maintenance agreement has been reached.

Conditional Uses are typically required to be administratively reviewed by the Zoning and Codes Office every 5 years to insure compliance with the standards of the Conditional Use Permit. Expiration dates are often applied to Conditional Uses so they may be re-evaluated to determine if they remain compatible with the development in the area.



Figure 5. Route for trucks exiting Lawrence.

The landfill is compatible with the current development, but may become incompatible as the area develops further. A ten-year expiration period is recommended to allow for variation in the delivery schedule.

Staff Finding – The heavy equipment and earthwork required for the landfill are not expected to have negative impacts on nearby properties as a lake and I-70 are neighbors on 2 sides and other nearby properties are in agricultural production. The truck traffic to and from the site could negatively impact the uses along the truck route and the roads themselves if a large number of trucks would access the site daily. Heavy truck traffic is restricted to the highways within the City of Lawrence and may use city streets only to access the nearest highway. It would be necessary to use E 1600 Road to access the site, but this would require the township's approval and the execution of a maintenance agreement. The landfill is expected to be temporary and a 10 year expiration date is recommended.

VI. RELATIVE GAIN TO THE PUBLIC HEALTH, SAFETY AND WELFARE BY THE DESTRUCTION OF THE VALUE OF THE PETITIONER'S PROPERTY AS COMPARED TO THE HARDSHIP IMPOSED UPON THE INDIVIDUAL LANDOWNERS

Applicant's Response:

"The depressed area is not uniform in shape to hold a sustainable pool of water and thus allows weeds to grow and provides breeding ground for mosquitoes and is somewhat unsightly. Clean rubble fill will not destroy the value of the owner's land nor any adjoining land."

Approval of the request for the Conditional Use would allow the abandoned borrow pit to be filled and reclaimed to a more level grade. Denial of the request would not allow the pit to be reclaimed so it would remain uneven which leaves it relatively unusable for the property owner.

Denial of the request would benefit the public in that it would prevent the additional truck traffic along E 1600 Road, a fairly narrow township road (Figure 6).

Staff Finding – In staff's opinion, the gain to the public health, safety, and welfare by the denial of the request for the Conditional Use for the reclamation of the borrow pit, a landfill use, would be the prevention of heavy truck traffic on E 1600 Road. The damage of the heavy truck traffic could be mitigated if the property owner executes a maintenance agreement with the township; however, the narrowness of the road could present a safety issue. The hardship to the property owner of denial would be that the borrow pit could not be reclaimed.



Figure 6. E 1600 Road, between I-70 and the subject property. (Google Maps)

VII. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response:

"Generally filling on property is not outlined in the Comprehensive Plan. As the property is not a solid waste landfill, it complies with the Comprehensive Plan."

An evaluation of the conformance of a Conditional Use Permit request with the comprehensive plan is based on the strategies, goals, policies and recommendations contained within *Horizon 2020*. The comprehensive plan does not directly address Conditional Use Permits; however it does contain recommendations for properties within the Urban Growth Area in Chapter 4, <u>Growth Management</u>. The plan recommends that the Urban Growth Area not be developed to urban densities until infrastructure is in place and recommends that developing properties be annexed.

Specific plans are incorporated into *Horizon 2020* in Chapter 14. The subject property is located within the planning boundary of the *Northeast Sector Plan*, approved in 2012. The plan recommends that this property be used as Open Space. The filling of the borrow pit would create a more even terrain which could make the property more usable as Open Space.

Staff Finding –The proposed Conditional Use is in compliance with recommendations in the Comprehensive plan regarding development within the *Northeast Sector Plan*.

CUP PLAN REVIEW

Height, Area, and Density Standards: No new buildings are being proposed.

<u>Parking and Access:</u> There is no parking associated with the use. The trucks will deliver the fill and leave. Access is provided from E 1550 Road, which is the extension of N 9th Street. E 1550 Road is gated approximately 190 ft north of the intersection with Lyon Street. Deliveries will be by appointment only and the property owner will open the gate to allow access to the property at those times.

<u>Water/Sewage Management:</u> The residence on the property is served by rural water and a septic system. No utilities are required for the proposed Conditional Use.

<u>Screening:</u> Screening the use is not necessary as the property is bounded on the west by agricultural land, under the same ownership, and on the east and south by a large lake. The fill area will be visible from I-70 to the north. Given the height of the highway in relation to the property, screening would be difficult to accomplish. In staff's opinion, screening from I-70 is not

necessary based on the practical difficulties in providing screening and the fact that this is a temporary use.

A time-frame for the completion of reclamation should be established. Top soil application and seeding should be completed within 1 year following the completion of filling and grading activities on the site. This should all be completed within the 10 year life span of the Conditional Use Permit.

Conclusion

The Conditional Use proposed complies with the County Zoning Regulations and the land use recommendation of *Horizon 2020*. The use, as conditioned, should be compatible with the surrounding land uses provided a road maintenance agreement is executed with the township to insure the damage done by the heavy trucks is repaired at the applicant's cost.

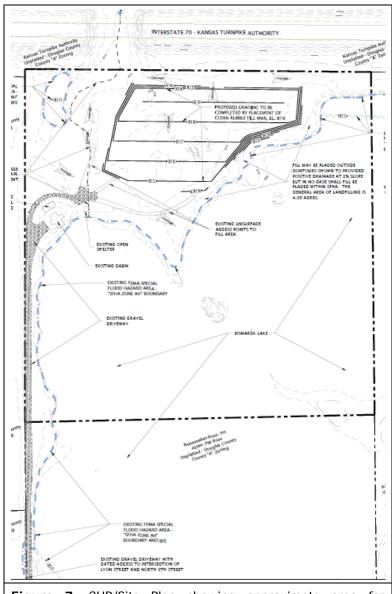
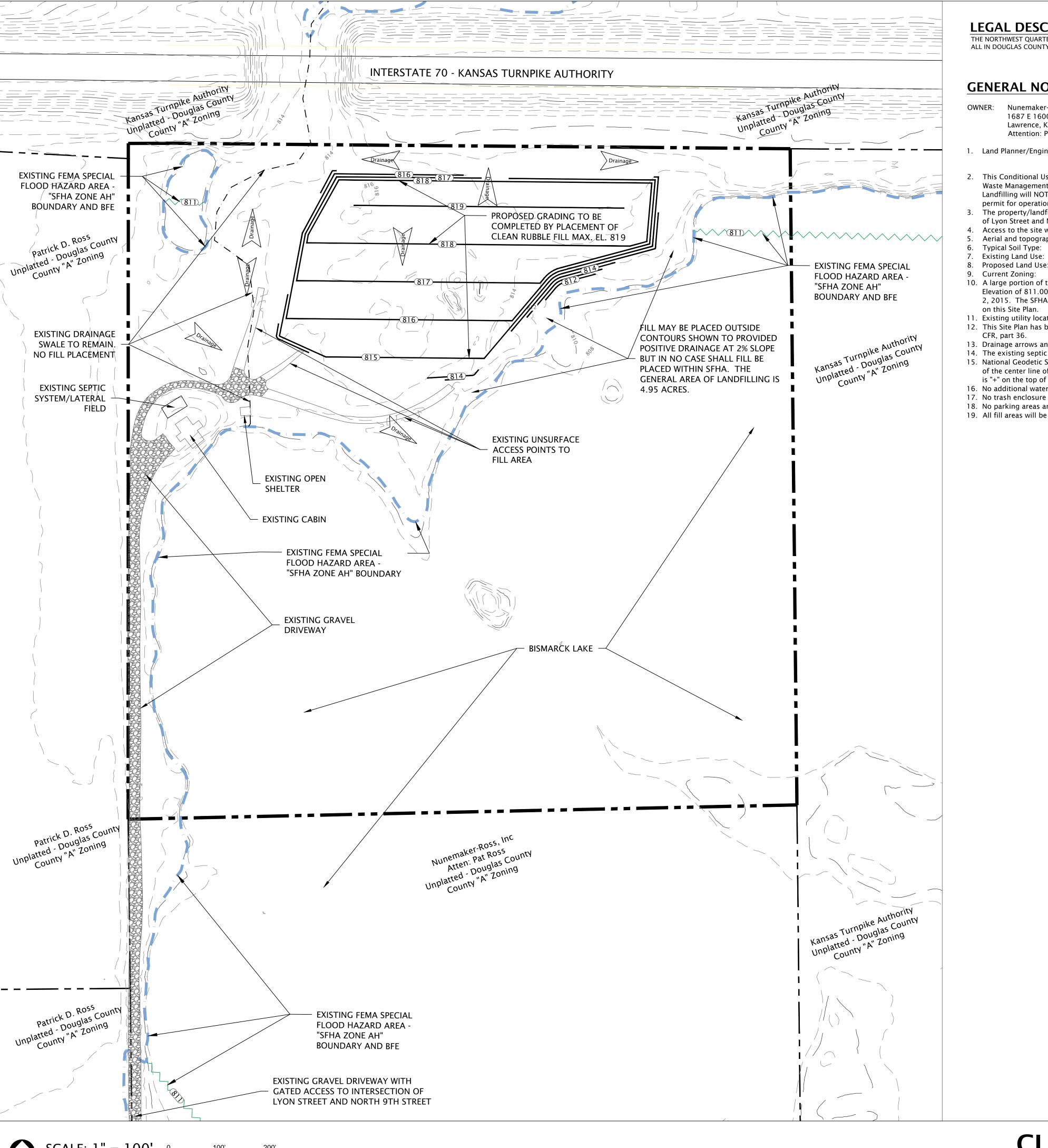


Figure 7. CUP/Site Plan showing approximate area for placement of fill.



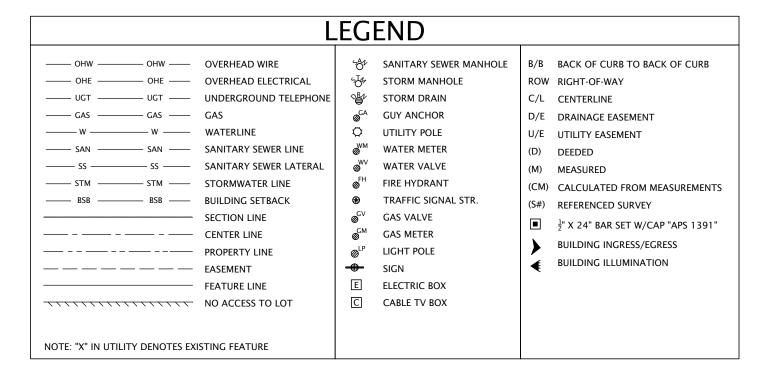
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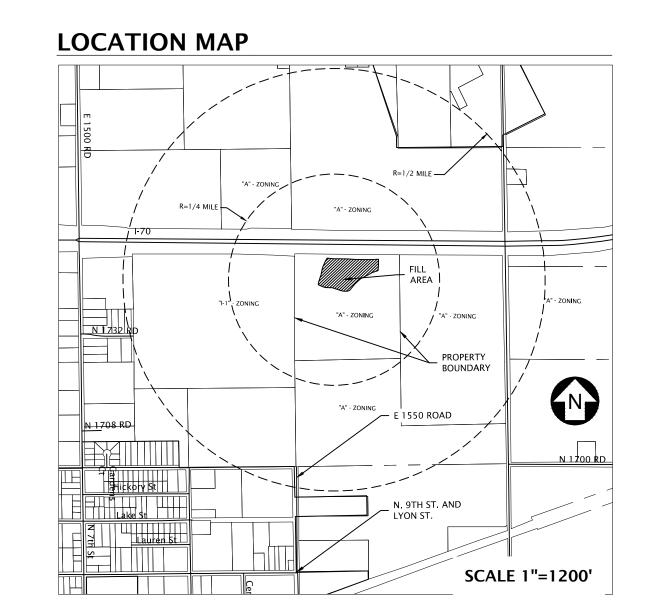
THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION TWENTY (20), TOWNSHIP TWELVE (12) SOUTH, RANGE TWENTY (20) EAST OF THE 6TH P.M. CONTAINING 40 ACRES MORE OF LESS,

GENERAL NOTES

Nunemaker-Ross Inc. 1687 E 1600 Road Lawrence, Kansas 66044 Attention: Patrick D. Ross

- 1. Land Planner/Engineer: Grob Engineering Services, LLC
 - 3210 Mesa Way, Suite A Lawrence, Kansas 66049
- This Conditional Use Permit (CUP) Site Plan was developed to allow clean rubble landfilling activities as outlined in the Kansas Department of Health and Environment, Bureau of Waste Management Construction and Demolition Wastes and Clean Rubble, Technical Guidance Document SW-1994-G2, to be performed to raise and level an area with depressions. Landfilling will NOT include construction and demolition (C&D) wastes as outline in the above referenced document. Unlike a C&D landfill, state statues do not require a solid waste permit for operation of a site that only accepts clean rubble.
- The property/landfill area will have controlled access. Access to/from this property will be via an existing driveway that is gated at the entrance on E 1550 Road near the intersection of Lyon Street and North 9th Street.
- Access to the site will be via Lyon Street and Locust Street which are defined as "Truck Delivery Routes" within the city limits and E 1550 Road within the county.
- Aerial and topographic information obtained from aerial survey performed by Sanborn Mapping for the City of Lawrence and Douglas County 2006 and 2013.
- Typical Soil Type: Ev Eudora-Kimo Complex
- Residential Detached Dwelling
- Residential Detached Dwelling 8. Proposed Land Use:
- A Agriculture
- 10. A large portion of the property is located within the Special Flood Hazard Area (SFHA) Zone AH per FEMA Map # 20045C0177E Effective Date: September 2, 2015 with a Base Flood Elevation of 811.00. The remainding portion of the property is located within the Special Flood Hazard Area (SFHA) Zone X per FEMA Map # 20045C0177E Effective Date: September 2, 2015. The SFHA is delineated on this Conditional Use Permit - Site Plan. No portion of the activities associated with this CUP include filling within the regulated SFHA illustrated
- 11. Existing utility locations, elevations, and sizes are shown for reference only. No additional utilities will be constructed as part of this CUP.
- 12. This Site Plan has been designed to comply with the provisions of the Americans with Disabilities Act Accessibility Guidelines (ADAAG) for Buildings and Facilities, appendix A to 28
- 13. Drainage arrows and proposed contours are shown on the plan to indicate general drainage patterns.
- 14. The existing septic waste lateral field from the existing cabin will remain and no changes are proposed with this CUP Site Plan.
- 15. National Geodetic Survey vertical benchmark is a disk marked "B 276 Reset 1956" set in a concrete headwall at the intersection of Highways 40,59, and 24. The disk is 74.0 feet east of the center line of the northbound lane of Highway 59, in the top of the northeast end of the south headwall of a 6-foot concrete culvert, Elevation = 816.37 ft. On-site bench mark is "+" on the top of the south end of the west headwall of culvert under E 1400 Road, Elevation = 839.82 ft.
- 16. No additional water service will be required as part of this CUP.
- 17. No trash enclosure or outdoor storage areas will be constructed as part of this CUP. Trash disposal will be by owner off-site.
- 18. No parking areas are designated as part of this CUP.
- 19. All fill areas will be restored with topsoil and all disturbed areas shall be reseeded with fescue or brome type grasses and otherwise restored to their original condition.





CONDITIONAL USE PERMIT SITE PLAN for

CLEAN RUBBLE LANDFILL - 1736 E 1550 ROAD

C E C E THIS DRAWING IS COPYRIGHTED WORK BY GROB ENGINEERING

SERVICES, LLC. THIS DRAWING MAY NOT BE PHOTOGRAPHED, TRACED, OR COPIED IN ANY MANNER WITHOUT THE WRITTEN PERMISSION OF GROB ENGINEERING SERVICES, LLC.

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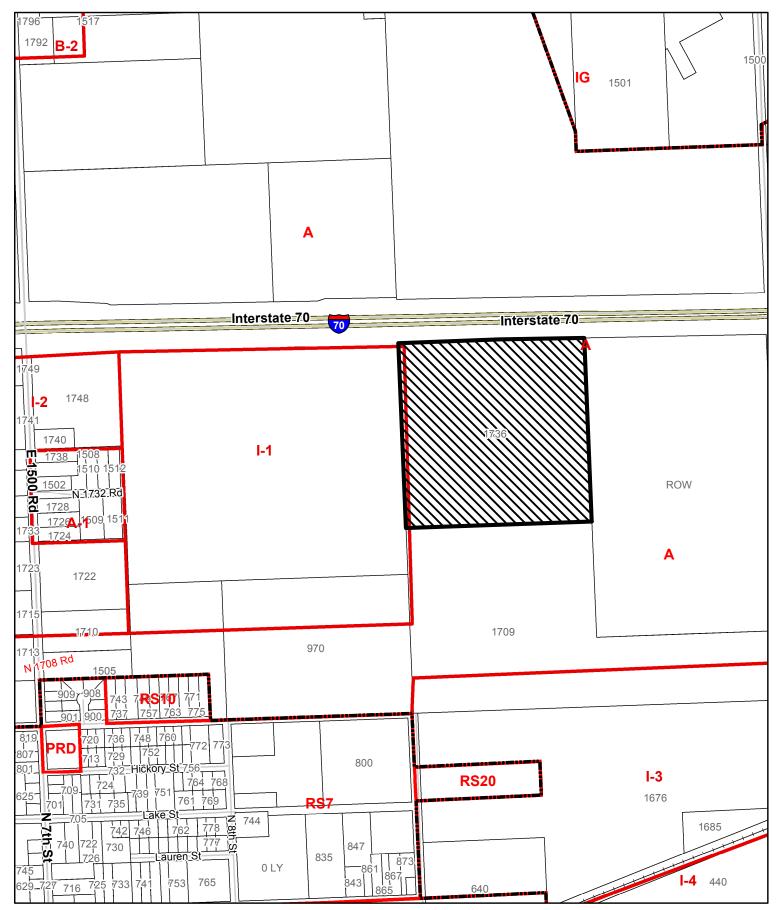
> **DESIGNED BY CHECKED BY**

> > **ISSUE DATE** March 22, 2016

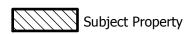
> > **REVISIONS**

April 28, 2016

SHEET



CUP-16-00105: Conditional Use Permit for a Clean Rubble Landfill Located at 1736 E 1550 Road





PLANNING COMMISSION REPORT Regular Agenda -- Public Hearing Item

PC Staff Report 5/23/16

ITEM NO. 5 TEXT AMENDMENT TO LAND DEVELOPMENT CODE; VALET PARKING (SLD)

TA-16-00128: Consider a Text Amendment to the City of Lawrence Land Development Code, Chapter 20, Article 17 and related sections of Article 9, for revisions related to the use and design standards for Valet Parking. *Initiated by the City Commission on 3/28/16.*

RECOMMENDATION: Staff recommends that the Planning Commission forward the proposed amendment, TA-16-00128, amending Article 17 and sections of Article 9 of the Lawrence Land Development Code to establish *Valet Parking* as a type of parking to the City Commission with a recommendation for approval.

Reason for Request: The City Commission initiated a text amendment at their April 5, 2016

meeting to consider adding valet parking to the Lawrence Land

Development Code.

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

None received

ATTACHMENTS

Attachment A - Draft Language

BACKGROUND

As part of the review and design of the HERE at Kansas mixed use development and in response to the loss of mechanical parking for the development, the project developers proposed providing comparable parking using a human rather than mechanical valet parking option. The design of that specific project includes a ful- service valet operation and parking stalls and access aisles that are narrower than conventional self-parking lots. There are currently no design standards for valet parking.

The general purpose of Article 9, Parking, Loading and Accessis, "intended to ensure that the off-street parking, loading and access demands of various land use will be met without adversely affecting surrounding areas. The regulations are also intended to help maintain a safe and efficient transportation system and advance other planning goals related to land use and the environment. In recognition of the fact that different approaches may be appropriate in different settings, the regulations allow flexibility in addressing vehicle parking, loading and access demand."

The existing Land Development Code provides design standards for parking lots (and structured parking). The standards assume that those using the parking lots are "self-parkers". The code assigns authority to the City Engineer to approve all off-street parking areas (20-913(b)).

Minimum dimensions for parking lot elements include width and length of the parking space, width of the access aisle, and overhang of vehicles beyond the stall length (20-913 (f)). This section also grants authority to the City Engineer to approve reductions as noted below.

"Where natural and/or man-made obstacles, obstructions or other features such as but not limited to Landscaping, support columns, or grade difference exist, the City Engineer may approve a reduction in stall width, stall length and/or module width in all instances where a reduction is requested, attention to emergency vehicle access shall be considered and incorporated into the parking lot design."

The City Commission concluded that it would be appropriate to include the specific activity -valet parking-- in the Land Development Code with applicable design standards and process for implementation for future projects.

Full Service Valet Parking	Self-Parking	
Area required for queued vehicles for drop-	Stacking not required for self-park lots.	
off/pick-up.		
No public access to parking area.	Full public access to parking area.	
Fewer drivers accommodate narrower access	Two way traffic circulation required.	
aisles.		
Access to one side (driver side) of vehicle for valet	Access to both sides of parking to accommodate	
staff. Allows vehicles to be parked closer.	driver side and passenger side access. Requires	
	standardized parking spaces.	
Requires on-site attendant for service	Does not require on-site attendant.	

OVERVIEW OF PROPOSED AMENDMENT

The following is a summary listing of the proposed changes:

- 1. Article 17: Terminology
- 2. Article 9: Parking Loading and Access

The portions of the Code being revised are attached to this staff report.

CRITERIA FOR REVIEW AND DECISION-MAKING

Section 20-1302(f) provides review and decision-making criteria on proposed text amendments. It states that review bodies shall consider at least the following factors:

1) Whether the proposed text amendment corrects an error or inconsistency in the Development Code or meets the challenge of a changing condition;

The purpose of this proposed text amendment is to provide a clear definition for a type of parking that was not contemplated in the Land Development Code. Lacking any clear land use regulations the use currently operates without any regulatory oversight and is a changing condition in the community.

2) Whether the proposed text amendment is consistent with the Comprehensive Plan and the stated purpose of this Code (Sec. 20-901)

Horizon 2020 articulates broad design standards that acknowledge the importance of infill development, appropriate land use transitions and the importance of good design, building and parking lot orientations and integration of land uses. These values are further articulated in the Community Design Manual: Commercial Development:

[&]quot;Ensure that parking areas provide safe and efficient access to buildings, but do not

dominate the overall site design (2-9)"

Horizon 2020 supports the concepts of Mixed Use development, and designs that rely less on automotive connectivity and more strongly on pedestrian connectivity. Valet parking, with or without reduced parking spaces, can be utilized to meet these goals in specific project developments.

Conclusion

The addition of valet parking standards are in response to a recent land development application and the acknowledgement of the existence of the practice within the community. These standards will provide clarity in the Code and insure compatibility with nearby land uses through an appropriate review process.

Term	Definition
Street, Local	Local Streets provide direct Access to adjacent land uses. Direct Access from a Local Street to an
	Arterial Street should be discouraged.
Street, Marginal	A Street that is generally parallel and adjacent to an Arterial Street or other limited-Access Street and
Access	that is designated to provide direct Access to adjacent property. Marginal Access Streets are
	commonly known as "Frontage Roads."
Street, Private	Any tract of land or access easement set aside to provide vehicular Access within a Planned
	Development that is not dedicated or intended to be dedicated to the City and is not maintained by the
	City. Owners of a private street may choose to gate access to this type of street from the general
	public.
Street, Public	A way for vehicular traffic, whether designated as a local, collector, arterial, freeway or other
	designation, which is improved to City standards, dedicated for general public use, and maintained by
Ctroot Illtimata Daciem	the City. The term shall also include alleys. The Street design that is based on the planned carrying capacity of the roadway consistent with its
Street, Ultimate Design	
Street Line	functional classification on the Major Thoroughfares Maps in the Comprehensive Plan.
Street Tree and	The line separating the Street right-of-way from the abutting property. An area designated within the Public Frontage in a Mixed Use development. Such zones shall reserve
Furniture Zone	space for street trees and other landscaping as well as street furniture including, but not limited to
i diffiture zone	benches, street lights and transit stops.
Streetscape	The built and planned elements of a street that define the street's character.
Structural Alteration	Any change in the supporting or structural members of a Building, including but not limited to bearing
Structural Attendion	walls, columns, beams or girders, or any substantial change in the roof, exterior walls, or Building
	openings.
Structure	A Building or anything constructed that requires permanent location on the ground or attachment to
	something having a permanent location on the ground, including but not limited to fences, signs,
	billboards, and Mobile Homes.
Subsurface Utility	A subsurface area designated within the Public Frontage in a Mixed Use development. Such zones
Zone	shall reserve space for public utilities.
Thoroughfare	Any public right-of-way that provides a public means of Access to abutting property.
Tract (of land)	An area, Parcel, site, piece of land or property that is the subject of a development application or
	restriction.
Transitional Use	A permitted use or Structure that, by nature or level and scale of activity, acts as a transition or buffer
	between two (2) or more incompatible uses.
Tree Protection	Means the measures taken, such as temporary fencing and the use of tree wells, to protect existing
T: 0 "	trees from damage or loss during and after construction projects.
Trip Generation	The total number of vehicle trip ends produced by a specific land use or activity.
Unnecessary Hardship	The condition resulting from application of these regulations when viewing the property in its
	environment that is so unreasonable as to become an arbitrary and capricious interference with the basic right of private property ownership, or convincing proof exists that it is impossible to use the
	property for a conforming use, or sufficient factors exist to constitute a hardship that would in effect
	deprive the Owner of their property without compensation. Mere financial loss or the loss of a potential
	financial advantage does not constitute Unnecessary Hardship.
Vertical Mixed Use	See Mixed Use Structure, Vertical
Structure	
Valet Parking	An operational system in which attendants (aka Valets) park and retrieves automobiles. Valet parking
, and the second	allows more automobiles to be parked in an area and may be used to resolve parking shortages or
	improve customer service where parking might only be available at long walking distances. Valet
	Parking may employ tandem and/or stacked parking layouts.
Valet Parking Plan	A document, submitted concurrently with a development application proposing the use of Valet
	Parking, that includes, but is not limited to the summarizing the layout and dimensions of the on-site
	parking area, on-site drop-off, operations of the service including hours of operation and maximum and
10/	minimum staffing level.
Woodlands	Natural hardwood forests, whether or not actively forested.
Working Days	Monday through Friday, 8AM to 5PM excluding city holidays

20-913 PARKING AND LOADING AREA DESIGN STANDARDS

The design standards of this section apply to all Parking Areas, including commercial parking lots and "non-required" Parking Areas.

(a) General Layout Principles

There shall be safe, adequate, well-lit, and convenient arrangement of pedestrian pathways, bikeways, roads, Driveways, and off-street parking and loading spaces within off-street Parking Areas. Streets, pedestrian walks, and Parking Areas shall be designed as integral parts of an overall site design, which shall be properly related to existing and proposed Buildings, adjacent uses and landscaped areas. There shall be defined pedestrian ways connecting all public entrances of Buildings to all modules of the Parking Area, to the required Bicycle Parking Area, to any adjacent bus stop and to the nearest public sidewalks. Such pedestrian ways shall, to the maximum extent practicable, be separated from driving lanes with curbs or other devices. At locations where walkways cross Driveways or travel lanes, the crossings shall be clearly marked with both signage and pavement markings.

(b) Approval

The layout and design of all off-street Parking Areas shall be approved by the City Engineer prior to the issuance of a Building Permit. Before approving any off-street parking plan, the City Engineer shall find that the spaces provided are useable and that they comply with the City's standard design criteria.

(c) Appearance

The materials used in the design of paving, lighting fixtures, retaining walls, fences, curbs and benches shall be easily maintained and designed to be indicative of their function.

(d) Maintenance

Parking lots shall be maintained in a safe operating condition so as not to create a hazard or nuisance. All materials used in the design of paving, lighting fixtures, retaining walls, fences, curbs and benches shall be continuously maintained and kept free of debris and hazards. Striping and other pavement markings shall be maintained in an easily readable condition.

(e) Surfacing

- (1) All off-street Parking Areas and Driveways, including those serving Attached Dwellings, Detached Dwellings and Duplexes, shall be surfaced with a minimum of one of the following:
 - (i) 4 inches of reinforced Portland cement concrete;
 - (ii) 5 inches of granular rock base with 2 inches of asphalt;
 - (iii) 7 inches of granular rock with a double asphaltic prime and seal;
 - (iv) 5 inches of full depth asphalt; or
 - (v) 4 inches of compacted gravel for residential Driveways constructed in Floodplains areas with a paved Driveway Apron constructed to city residential Driveway standards.
- (2) As an alternative to the surfacing required in the preceding paragraph, all off-street parking for uses allowed by right within residential Districts or areas of low off-street parking use as determined by the City Engineer (such as fire safety lanes or overflow Parking Areas), may be surfaced with the following alternative methods of paving. The surfacing shall be installed per the manufacturer's recommendations, with the pavement and base designed by a professional engineer licensed in the State of Kansas. The pavement cross-section shall demonstrate the structural ability to support the anticipated vehicle loads for the use. The pavement design shall be reviewed and approved by the City Engineer.

- (i) Grid unit pavers with grass; or
- (ii) Concrete, brick, or clay interlocking paver units.
- (3) Private Streets shall be built to City Street standards and maintained by the Landowner.
- (4) Driveway approaches (aprons) shall be built to City standards, including, where applicable, the Residential Driveway Requirements adopted by the City Commission on July 10, 1996 as amended, and maintained by the Landowner.

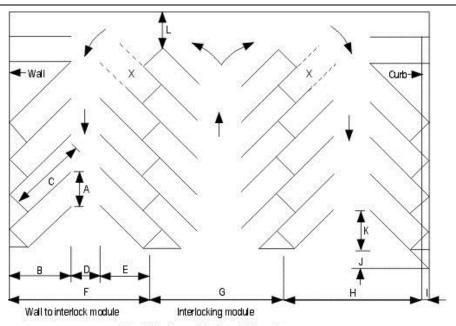
(f) Dimensions

(1) Automobile Parking

All off-street Parking Areas shall comply with the following dimensional standards:

Dimensional Feature (all	Diagram	Parking Angle				
dimensions in feet)		0	45	60	75	90
Stall width (parallel to aisle)	Α	8.5	12.2	9.9	8.8	8.5
Stall length	В	24.0	24.5	21.4	19.5	18.0
Stall length of line	С	9.0	17.0	18.5	19.0	18.0
Aisle width between stall lines	D	12.0	12.0	16.0	22.0	24.0
Stall depth, interlock	E	9.0	14.8	17.0	18.3	18.0
Module, wall to interlock	F	30.0	43.8	51.5	59.3	60.0
Module, interlocking	G	30.0	41.6	50.0	58.6	60.0
Module, interlock to curb face	Н	30.0	41.8	49.4	56.9	58.0
Bumper overhang (typical)	1	0.0	1.5	1.8	2.0	2.0
Offset	J	_	6.3	2.7	0.5	0.0
Setback	K	24.0	11.0	8.3	5.0	0.0
Cross-aisle, one-way	L	18.0	18.0	18.0	18.0	18.0
Cross-aisle, two-way		24.0	24.0	24.0	24.0	24.0

Where natural and/or man-made obstacles, obstructions or other features such as but not limited to Landscaping, support columns or Grade difference exist, the City Engineer may approve a reduction in stall width, stall length and/or module width. In all instances where a reduction is requested, attention to emergency vehicle Access shall be considered and incorporated into the Parking lot design.



X = stall not accessible in certain layouts

(2) Loading

Required loading spaces shall have a minimum vertical clearance of 15.5 feet. See Section 20-906 for other dimensional standards.

(g) Bicycle Parking

Every Bicycle Parking Space, whether used publicly or privately and including a commercial Bicycle Parking Space, shall be designed, built and maintained in accordance with the following specifications:

(1) Surfacing

A Bicycle Parking Space shall be surfaced with a minimum of:

- (i) 4 inches of concrete, or
- (ii) 4 inches of asphalt, or
- (iii) 2 inches of concrete with a 2-inch brick overlay, or similar material for overlay.

(2) Lighting

Bicycle Parking Space shall be located within a lighted area and within clear view of passersby.

(3) Barriers

If Bicycle and automobile Parking Areas or Accessways abut each other, there shall be provided a physical barrier between the Bicycle and automobile areas to prevent a Bicycle or its operator from being hit by a motor vehicle.

(4) Structure

Each Bicycle Parking Space shall provide for a secure method of locking a Bicycle and be located to accommodate Bicycle Parking in a manner that is convenient to use and does not interfere with other uses of the property.

(h) Striping

To facilitate movement and to help maintain an orderly parking arrangement, all Parking Spaces shall be clearly striped, with a minimum width of 4 inches. The width of each Parking Space shall be computed from the centers of the striping.

(i) Curbs

The perimeter of the parking lot shall have a curb and gutter in accordance with City standards for concrete curbs.

(j) Large Parking Lots

- (1) Parking lots of 220 Parking Spaces or more shall be divided into smaller Parking modules containing no more than 72 spaces. Landscape strips, Peninsulas, or Grade separations shall be used to reduce the adverse visual impacts of large expanses of paving, to direct vehicular traffic through the parking lot, and to provide a location for pedestrian walks. Protected pedestrian walkways, leading to Building entrances, shall be provided within such parking lots.
- (2) Parking lots of 450 Parking Spaces or more shall place Landscaping and trees on both sides of entrance drives to create tree-lined entrances, to direct vehicles into and out of the site, and to provide adequate space for vehicle stacking at exits onto perimeter roadways.

(k) Pedestrian Connections

Parking lots shall be designed to provide designated walkways for pedestrians. Walkways shall connect Building entrances with Parking Areas and with public sidewalks along adjacent streets.

(I) Valet Parking

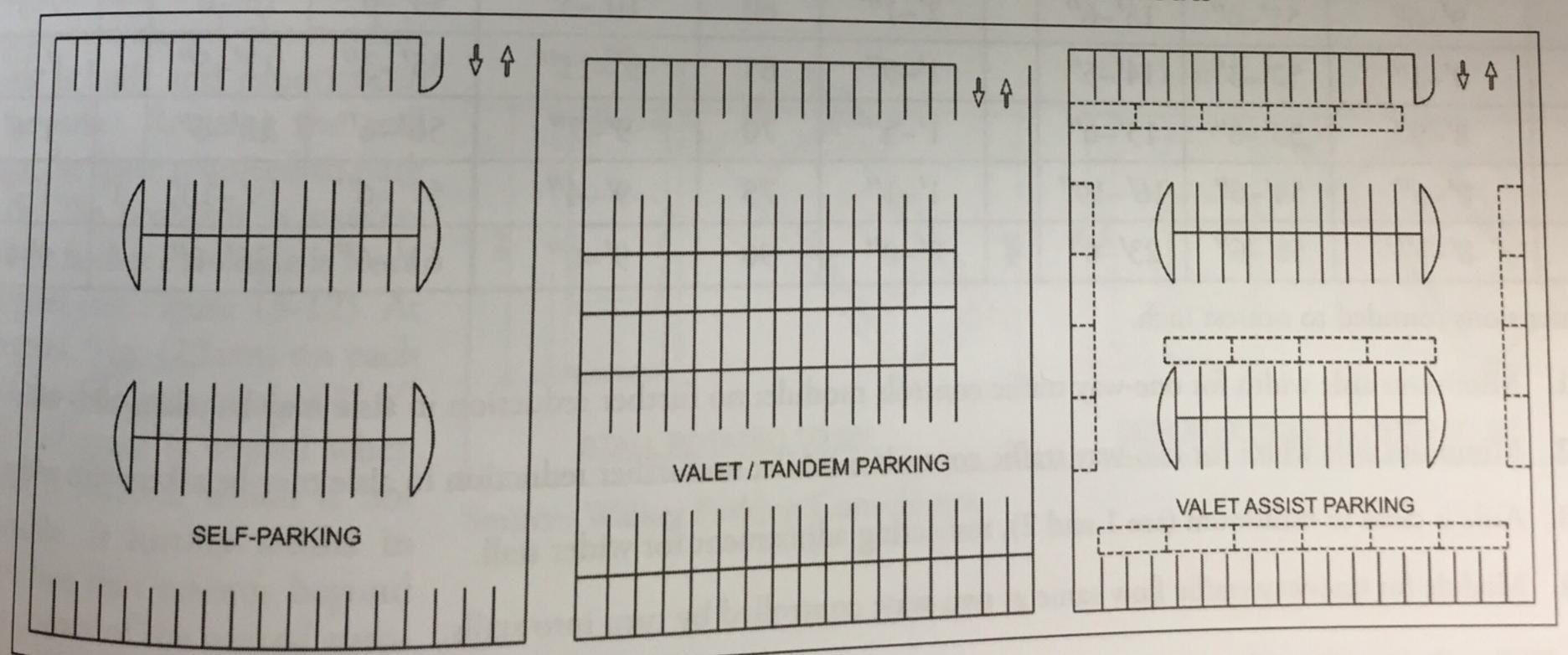
Valet Parking does not require individual striping and may take into account the tandem or mass storage of vehicles. Use of Valet Parking is permitted in the following instances.

- (1) when proposed as part of a development project and in conformance with the design standards of Section 20-913 (f) (1) without variances or exceptions may be permitted administratively as part of a site plan.
- (2) When proposed as part of a development project and not in full compliance with the design standards of Section 20-913(f)(1) use of Valet Parking shall require the submission of a Valet Parking plan and shall require City Commission approval.
 - (i) Valet Parking Plan shall include the following:
 - (a) layout and dimensions of the parking spaces and drive aisles showing sufficient parking and maneuverability for a variety of passenger automobiles, motor vehicles, and light trucks,
 - (b) on-site drop-off for vehicles using the parking services with sufficient queuing for vehicles that do not block the public right-ofway.
 - (c) If Valet Parking Plan includes parking spaces that are required for a specific use, Valet Parking services must be provided for those parking spaces during all operating hours of the use.
 - (ii) Changes to a Valet Parking lot or facility to a Self-Parking lot or facility.
 - (a) changes to a parking lot or facility with Valet Parking that are changed to be self-parking shall require a revised site plan per Section 20-1305 to show compliance with the parking lot design standards of Section 20-913 (f)(1)

ularly 101 F Lerritt IL are willing to accept it, partic-

what parking is an operational system in which attendants (aka valets) park and retrieve automobiles. Valet parking falt parking is all operations attendants (aka valets) park and retrieve automobiles. Valet parking allows more automobiles to be parked in an area and often is used to resolve parking shortages or improve usually allows more parking might otherwise only be available at long walking distances. Valet parking often distormer service which is a ser the customer, it is preferable to have to move only one automobile to retrieve another. A hybrid approach, called car to the customer, assist parking, occurs when most users self-park, but when self-park capacity is reached, attendants direct automobiles to park in parallel along one side of the aisle. The driver gives the keys to the attendant, who moves the automobile if the driver of a blocked automobile returns to depart, and then parks the blocking automobile in the the automobile in the newly vacant stall. In addition to the service to the public, valet parking typically increases parking capacity in areas of limited parking. The efficiency of parking is significantly improved; in Figure 13-10, the valet assist layout increases the self-park layout from 72 spaces to 95 spaces, an increase of over 30%. The valet/tandem layout increases the capacity to 104, an increase of nearly 45%.

Figure 13-10. Alternative Parking Layouts Reflecting Operational Characteristics



Memorandum City of Lawrence Douglas County Planning & Development Services

TO: Planning Commission

FROM: Scott McCullough, Sheila Stogsdill

Date: For May 23, 2016 meeting

RE: Item No. 6: TA-13-00235 -

Revisions to Development Code, Article 9 – Parking,

Sections 20-908 and 20-915

Attachments: Proposed Sections 20-908 & 20-915

Sections 20-908 and 20-915 address standards related to where parking areas may be located on property and standards specifically related to driveways and access. As previously presented, these amendments are intended to address several items.

20-908 - Parking Lot Locations:

- Establishes setbacks for parking lots serving various types of land uses Policy Question: Should setbacks for parking lots in RS Districts (such as for churches or day care centers) be different than when those nonresidential uses are located in RM Districts? Currently the code requires all parking lots in all residential districts to be located 25 feet from street right-of-way. Should access aisles for a parking lot be permitted within the 25 foot setback?
- Establishes a setback from adjacent nonresidential property lines for parking lots that are located in nonresidential zoning districts
- Relocates standards related to the location of driveways to Section 20-915

20-915 - Driveways and Access Management Standards:

Section 20-915 has been reorganized and revisions incorporated to clarify and specifically identify which standards apply to different types of development (low-density residential or non-residential uses).

When the 2006 code was adopted and the RS5 & RS3 districts were created, a 12' limitation on residential driveways was also adopted. This standard was intended to limit suburban width driveways in neighborhoods where narrow drives were predominant. There have been a number of variance requests from this standard where wider driveways already existed in RS5 areas. The revisions include an administrative waiver (instead of a BZA variance) where the narrow standard is not the norm.

Chapter 16 of the City Code provides standards related to driveways for single-family and duplex lots. When the Development Code was adopted in 2006, changes were made to Chapter 20 which resulted in lack of consistency in the regulations. According to Chapter 16 residential lots which have at least 100' of frontage are permitted two curb cuts. The Development Code changed that requirement to 200' which is double the widest lot width the code requires in residential zoning districts. Staff has found the requirement to be burdensome and has seen numerous variance requests. The proposed revisions align with the existing provisions in Chapter 16.

This section also includes modifications to street design standards including turn lanes, sight distance and driveway spacing. Waivers from access standards are proposed as an administrative process after consultation with the City Engineer.

This section has also been reorganized to include the following changes:

- Consolidates all locational criteria for residential driveways in one section
- Inserts standard driveway details from Chapter 16 of the City Code for Detached Dwellings and Duplex Dwellings
- Clarifies the lot frontage requirements for second curb cuts in various situations
- Incudes a preference for alley access to residential lots when an alley is available
- Suggests a standard layout for parking along alleys for Duplex Dwellings (based on recommended configuration presented in the draft Oread Design Guidelines)
 - o prohibits on tandem (stacked) parking
 - o limits garage size to a two-car bay

Policy Questions: Should this standard be applied City wide where alleys are constructed or limited to certain areas?

Should Duplex Dwellings constructed on an alley be limited to only one twocar garage?

PROPOSED 20-908

20-908 OFF-STREET PARKING LOT LOCATION

(a) General

Except as otherwise expressly provided in this section, required off-street parking and loading spaces shall be located on the same Lot as the Principal Use (See Section 20-909 for possible exceptions).

(b) Residential Districts

- No part of a Parking Area including access aisles, but excluding, other than a Driveway, may be located within 25 feet of a Street right-of-way in any residential Zoning District.
- (2) Parking lots serving residential uses shall be setback a minimum of 5 feet fromside or rear property lines when adjacent to other residentially-zoned property.
- (3) Parking lots serving nonresidential uses permitted in residential districts shall be setback a minimum of 10 feet from side or rear property lines when adjacent to residentially-zoned property.
- (4) Bufferyard standards set out in Section 20-1005 may affect parking lot locations.
- (4)(5) No more than 4 vehicles may be parked on Driveways or turnarounds within the required Front or Side Setback of any Lot in a residential Zoning District.
- (2) Single or double Driveways and turnarounds may not be used to provide required off-street parking within the required Front or Exterior Side Setback with the exception of when they are serving a Duplex or Detached House Dwelling.
 - With Planning Director permission approval, a parking area may be permitted closer than 25 feet to an adjacent street in an exterior side yard if the crientation of the structure allows the principal building to be located closer than 25 feet to the adjacent street.
- (3)(6) No parked vehicles required off-street parking spaces shall overhang extend into the right-of-way or block a portion of the any sidewalk.
- (4)(7) ADD REFERENCE TO 2' OFF SIDE PROPERTY LINES AND SKETCH?
- (5)(8) Where alleys are available, parking shall be provided along the alley in accordance with the following diagrams:

INSERT DIAGRAMS FROM CODE INTERPRETATION

http://www.lawrenceks.org/assets/pds/planning/documents/Alley_Park.pdf

- (6)(9) Driveway widths may not exceed 26 feet in residential Districts. All Driveway cuts into the Street require a permit and must be approved by the City Engineer in conformance with the standards outlined in Chapter 16, Article 3 of the City Code. [moved to 20-015]
- (7) In RS3 and RS5 Districts, residential Driveways may be constructed a maximum of 12' wide to reduce pavement width and maintain the character of the neighborhood. [moved to 20-915]

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(c) Nonresidential Districts
The location of off-street Parking Areas in Commercial and Industrial Nonresidential Zoning Districts shall comply with the adopted city design standards and the following standards:

		Minimum Setback (feet)**			
District	Allowed Location	From Right-of- Way	From Residential Lot Lines	From Nonresidential Lot Lines	
CN1	Not allowed between the Facade of the Building with the main entrance and the Street- when new/major development projects are proposed.				
CO	No restriction except as appointed in Article E				
CN2	No restriction except as specified in Article 5.				
CD	Prohibited between a Building and any Street				
CC		15	10	<u>5***</u>	
CR					
CS					
IBP					
IL	No contribution according to Author 5				
IM	No restriction except as specified in Article 5.				
IG					
Н					
GPI					
OS					
MU	Prohibited in the Primary Development Zone and prohibited between a Building and any Street in a Secondary Development Zone. No restriction in a Tertiary Development Zone.				

^{**} Setbacks may also be affected by required Bufferyards as identified in Section 20-1005.

WHAT IS SETBACK FROM NONRESIDENTIAL LOT LINES?

^{***} Unless reduced through approval of a site plan or development plan.

PROPOSED 20-915

20-905 DRIVEWAYS AND ACCESS

The standards of this section apply to all Driveways providing Access to multi-Family or nonresidential uses, developments as indicated below.

(a) General Standards

- (1) Vehicular Access to property from the adjacent street frontage is allowed only by way of Driveways. No other portion of the Lot Frontage may be used for vehicle ingress or egress..., nor may any Parking Area or Access Drive be arranged so that any vehicle may back directly onto a Street. All Driveway cuts into the Street shall require a permit from the Public Works department unless approved through site or development plan approval.
- When available, Alley Access is encouraged and preferred. Alley parking for Detached Dwellings and Duplex Structures shall conform to the locational standards established in Section 20-915 (b) and (c).
- (3) Driveways shall intersect the Street at right angles, unless approved by the City Engineer.
- (4)(4) All Driveway curb cuts require a permit from the Public Works Department unless the Driveway is approved through a building permit, site plan or development plan.
- (2) Driveway designs shall allow an entering vehicle turning speed of 15 miles per hour to help reduce interference with through Street traffic. Radii of Driveway shall be sufficient to achieve this standard for the types of vehicles that the Driveway is intended to serve.
- (3) There shall be sufficient on-site space to accommodate queued vehicles waiting to park or exit, without interfering with Street traffic.
- (4) Provisions for circulation between adjacent Parcel should be provided through coordinated planning or Cross Access Agreements.
- (5) Driveways shall be placed and designed so that leading and unleading activities will not hinder vehicle ingress or egress, and that vehicles entering the Driveway from the Street will not encreach upon the exit lane of a two way Driveway. Also, a right-turning exiting vehicle shall be able to use only the first through-traffic lane available without encreaching into the adjacent through-lane.
- (5) No-Lots that is less than 51 feet in width and that was created (by subdivision or re-platsubdivision) after the Effective Date with widths of 50 feet or less, shall only have a Driveway Access to a Public Street. Driveway Access to such a Lot shall be from an Alley or by a Shared Driveway.
- (6) Driveways serving residential properties shall be located at least 2 feet from side property lines.
- (7) No more than 4 vehicles may be parked on Driveways or turnarounds within the required Front or Side Yard Setback of residentially-zoned properties.

(6)

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(7) Driveways shall intersect the Street at right angles,

(b) Driveways serving Detatched Dwellings

(1) Driveways may not exceed 26 feet in width at the property line except in and shall conformance with the design standards outlined in Chapter 16, Article 3 of the City Code (see diagram below) or as limited below;

Property Line

34' Maximum

Sidewalk

B' Sidewalk

Bock of Curb

Bock of Curb

Centerline of Street

DRIVEWAY DETAIL FOR A SINGLE FAMILY HOME

SCALE : 1"=10'
(SKETCH A)

NOTE: DRIVEWAY APRON MAY BE FLARED, IF DESIRED, HOWEVER, THE MAXIMUM WIDTH AT THE CURB REMAINS AT 26 FEET.

Last Update: 6/18/96

(i) In RS3 and RS5 Districts, Driveways shall be a maximum of 12 feet wide to reduce pavement width and maintain the character of the neighborhood.

(ii) However in RS3 and RS5 Districts, Driveways may be constructed widerthan 12 feet when the Planning Director has determined that a majority of
the Driveways in the same block are more than 12 feet wide.

(2) Driveways within the required Front or Exterior Side Yard Setback may be used to provide the required off-street parking spaces for Detached Dwellings.

(3) Tandem or stacked parking spaces on Driveways may be utilized to provide the required off-street parking spaces for Detached Dwellings. Formatted: Font: Bold

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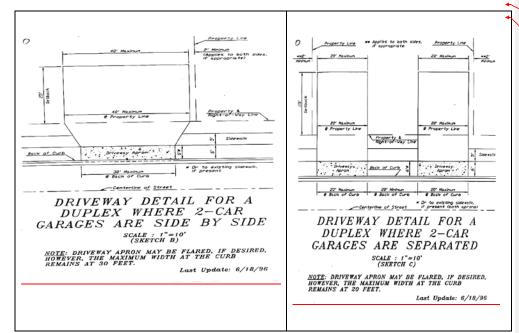
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(c) Driveways serving Duplex Dwellings

(1) Driveways shall conform to the design standards established in Chapter 16, Article 3 of the City Code for access from streets to Duplex Structures (see diagram below);



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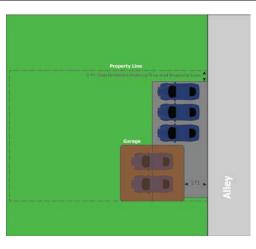
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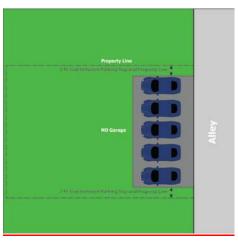
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Tandem or stacked parking spaces may only be utilized on Driveways accessed from a street to provide the required off-street parking spaces for Duplex Dwellings.

Where alley access is available, parking for Duplex Dwellings shall be provided from the alley and a Driveway(s) to the abutting street shall not be constructed to support the Duplex Dwellings.

(i) Parking configurations permitted to support Duplex Dwellings accessed from alleys must conform to one of the following diagrams: (insert diagrams from Oread Guidelines).





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(d) Privavas coving Multi Duelling Crown Living or Newscidenti

(d) Driveways serving Multi-Dwelling, Group Living or Nonresidential Developments

(1) No Parking Areas or and Access drives shall be arranged so that any vehicles may shall not back directly onto the Street.

(2) Driveways shall be designed to provide a minimum vehicle turning radii of 15 feet. Greater radii may be required by the City Engineer if needed to accommodate the types of vehicles that the Driveway is intended to serve.

(3) Access drives to Parking Areas shall be located to provide sufficient on-site space to accommodate queued vehicles waiting to park or exit without interfering

space to accommodate queued vehicles waiting to park or exit without interfering with on-street traffic.

(4) Provisions for circulation between adjacent Parcels on Collector and Arterial Streets should be provided through coordinated planning.

(i) Cross Access or Public Access Easements shall be dedicated to ensurecoordinated access between properties when a property is platted or a site plan/development plan is approved.

(ii) Easements should be recorded at the County Register of Deeds office

(b)(e)Turn Lanes and Tapers

Turn lanes lanes and tapers are required, unless determined to be unnecessary by the City Engineer, when:

- (1) Driveways intersect Arterial Streets. Turn lanes shall be a minimum of 150 feet in length plus the taper;
- (2) Driveways serving non-residential uses intersect Collector Streets.
 - (i) Left-turn lanes shall be a minimum of 100 feet in length plus the taper.
 - (2)(ii) Right-turn lanes shall be required when the projected or existing right-turning volume equals or exceeds 100 vehicles in the peak per-hour;
- (3) The City Engineer determines, based on a A traffic impact analysis, indicates that such treatment is necessary to avoid congestion and/or unsafe conditions on the Public Street.

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(c)(f) Driveway Grade

The Grade of a two-way, one-way or divided Driveway shall not exceed four percent (4%) for a minimum distance of 25 feet from the edge of the Street pavement.

(g) Sight Distance

(d)

- Direct-Access Driveways shall be located to allow for the fellowing minimum sighted distance based on the intersection type (full or partial Access) and the Street type.
- Sight distances shall be determined by a professional engineer licensed by the State of Kansas <u>utilizing the most recent AASHTO Green Book Standards*</u>, and shall be based on the design speed of the Street or on the 85th percentile speed, whichever is higher. (*A Policy on Geometric Design of Highways and Streets.)

	Speed		Minimum Sight Distance (in Feet)			
Street Type	miles per hour	feet per second	8-seconds	9 seconds	10 seconds	
Arterial	45	66	529	594	660	
	40	59	472	531	590	
Collector	35	51	408	4 59	510	
	30	44	352	396	440	
Residential	25	37	296	333	370	

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(e)(h)Driveway Spacing

All Direct Access to any Public Street shall be in accordance with the <u>City's adopted Access</u>

<u>Management Policy.</u> following standards:

(1) Multi-Dwelling, Group Living and Nonresidential Access

(1)(i) Arterial Streets

Direct Access to an Arterial Street is discouraged. prohibited except

- a. in redevelopment or infill situations where the subject property has no other reasonable Access to the Street system, the Access location shall be determined based on and the City Engineer's determination es that Access onto the Arterial Street, based on and considering the Street's Ultimate Design, configuration, can be safely accommodated.
- b. When direct Access to an Arterial Street is approved by the City* Engineer pursuant to the requirements of this section, the following standards apply. In the event that such standards cannot be met because of an unusually narrow or shallow Lot size, the City Engineer may reduce the spacing between cuts as long as the reduction does not result in an unsafe traffic condition. A Driveway Access allowed under this section shall be used only to serve a Detached Dwelling on the property or an existing business and will be reevaluated when the use or Lot size changes.

(i)1. Spacing from Signalized Controlled Intersections

All Driveways providing Access to Arterial Streets shall be constructed so that the <u>curb line extended from point of tangency of the curb return radius closest to a signalized or stop sign-controlled intersection is at least 300 feet or beyond the limits of the area of influence of the intersection as defined in the accepted TIS, whichever is greater. from the perpendicular curb face of the intersecting Street.</u>

(ii)2. Spacing from Other (Non-signalized) Access Points

All Driveways providing Access to Arterial Streets shall be constructed so that the curb line extended point of tangency of the curb return radius closest to all non-signalized Street or Driveway intersections is at least 300 feet from the closest perpendicular curb face of the intersecting Street or Driveway intersection.

(2)(ii) Collector Streets

Direct Access to Collector Streets shall be regulated in accordance—with the following standards. In the event that such standard cannot be met because of an unusually narrow or shallow Lot size, the City Engineer may reduce the spacing so long as the reduction does not result in an unsafe traffic condition.

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(i)(iii)Attached Dwelling, Detached Dwelling and Duplex-Lots

Direct Access to Collector Streets from Attached Dwellings, Detached Dwellings and Duplex Lots is prohibited except when the subject property has no other reasonable Access to the Street system and the City Engineer determines that Access can be safely accommodated.

(ii)1. Spacing from Signalized Controlled Intersections

All Driveways providing Access to Collector Streets shall be constructed so that the curb line extended from point of tangency of the curb return radius closest to a signalized or stop sign-controlled intersection is at least 300 feet to the closest from the perpendicular curb face of an intersecting Arterial Street or at least 250 feet to from the perpendicular curb face of an intersecting Collector or Local Street or Driveway.

(iii)2. Spacing from Other (Non-signalized) Access Points

All Driveways providing Access to Collector Streets shall be constructed so that the curb line extended from point of tangency of the curb return radius closest to a non-signalized Street or Driveway intersection is at least 250 feet from the closest perpendicular curb face of the intersecting Street or Driveway.

(2) <u>Detached Dwelling, Attached Dwelling and Duplex Access to Local and Collector Streets</u>

Direct Access to Collector Streets is prohibited except when the subject property has no other reasonable Access to the Street system and the City Engineer determines that Access can be safely accommodated.

(i) Detached Dwellings

(from Chapter 16) Each property containing a Detached Dwelling shall be allowed one Driveway curb cut with the following exceptions:

- a. Interior lots will be allowed two Driveway curb cuts if the length of the lot line adjacent to the street is at least 100 feet.
- Corner lots will be allowed two curb cuts if the length of either lot line adjacent to the street is at least 100 feet.
 - These two curb cuts may both be constructed along one lot line or one along each lot line; however, both curb cuts may only be constructed along one lot line if that lot line is at least 100 feet in length.
- c. Through lots will be allowed two Driveway curb cuts if the length of the lot line adjacent to the each street frontage is at least 100 feet. (new text)

(ii) Attached Dwelling Lots

<u>Driveway standards for Attached Dwelling Lots are provided in Section 20-503 of this Chapter.</u>

(iii) Duplex Dwelling Lots

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Two curb cuts are permitted on a Duplex Lot in accordance with Section 16-302, Sketch C of the City Code and as shown in Section 20-915(b) and (c).

(iv) Lots with Alley Access

Alley Access is permitted and a preferred Access alternative, where available.

(v) Cul-de-Sac Lots

Not more than one Driveway curb cut per lot is permitted on the bulb of a Cul-de-Sac as required in Section 16-302(A)(4).

(vi) Distance from Intersecting Streets (from Chapter 16)

- a. Driveway curb cuts on corner lots shall be at least 25 feet from the curb line extended of a Local Street.
- Driveway curb cuts on corner lots shall be at least 50 feet from the curb line extended of a Collector or Minor Arterial Street.
- c. Driveway curb cuts on corner lots shall be at least 75 feet from the curb line extended of a Major Arterial Street.

(vii) Distance from Other Driveways

Driveway curb cuts on the same lot shall have a minimum of 20 feet between the inner edge of the drives measured at the curb line.

(3) Waivers

- (i) Waivers from Section 20-915 these Access standards may be approved by the Planning Director City Engineer if the City Engineer determines that the requested waiver will not create a serious detriment to the safety or operation of traffic on the Street or roadway and only for infill or redevelopment projects where no other feasible option exists.
- (ii) The burden of proof that the requested waiver will not create a serious detriment to the safety or operation of traffic on the Street or roadway will be on the applicant. The City Engineer may require that the applicant for a waiver submit a traffic impact study pursuant to Section 20-916 Error! Reference source not found. if it is determined that such an analysis is necessary in order to render a competent decision on the requested waiver.
- (ii)(iii) Waivers from the Driveway standards outlined above may be reviewed by the Driveway Review Committee established in Chapter 16-302 of the City Code.
- (iii)(iv) A Driveway Access allowed with a waiver shall be used only to serve an existing Detached Dwelling or business on the property and for no other purpose.
- (iv)(v) The action of the City Engineer in granting or denying a waiver under this section shall be reported on the agenda of the next meeting of the Planning Commission after the action. Any party aggrieved may appeal the grant or denial of a waiver to the City Commission in writing within 14 days of the Planning Commission meeting at which the item appears on the agenda. The waiver shall not become effective until the expiration of the 14 days.

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appeal period or, in case of an appeal, until the City Commission has acted on the appeal.

(v)(vi) A Landewner-granted an Access waiver shall submit a letter to the City Engineer acknowledging the waiver and the fact that if circumstances change such that the property can meet the city's Access standards, the Access shall be immediately revised to comply with the city's Access standards.

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(f)(i) Driveways per Parcel along Local and Residential Collector Streets

(1) Unless otherwise specifically restricted, one Driveway opening shall be allowed per 200 feet of continuous Street Frontage. At least one Driveway shall be permitted for any Lot.

(2) Parcel with 200 feet of Frontage or less may apply for a second Driveway if it is to be shared with an adjoining Parcel, provided that the required minimum spacing is maintained. In such cases, only one Joint-Use Driveway will be permitted.

(3) Alleys are permitted and preferred Access alternatives.

(g)(i) Use of Joint-Use Driveways

(1) Joint-Use Driveways shall not be used as aisles for Access to Parking Spaces.

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The Lawrence Association of Neighborhoods (LAN) has been following the Text Amendments for Parking Standards for nearly one year. LAN requested that parking standards for duplexes be included in the Parking Text Amendment review. To date there has been no discussion that LAN is aware of regarding duplexes. More neighborhoods are coming forward regarding their concerns about problems created by *stacked parking* allowed for duplexes. **LAN neighborhood representatives unanimously support removing stacked parking from the duplex parking standard**, making the standard the same as all other multi-family dwelling units- one parking space per BR with **NO stacked parking**.

Building large duplexes has become a trend throughout the city. Stacked parking is a financial incentive for developers to add more renters, thus increasing the density by as much as 50% without providing adequate parking. Cars may be stacked, bumper-to-bumper, limiting adequate egress to the street. This is problematic and not practical.

Duplexes have in the past been allowed to have "stacked parking", which may have been appropriate for families due to the shared use of cars and that families often have children who have not reached driving age. Duplex zoning is intended to be lower density transitional zoning from single-family zoned areas to multi-family zoned areas. Today duplexes are no longer primarily family oriented or modest in size but rather resemble small apartment complexes. Many are being built with 4 BR's on each side. In many cases cars spill onto the streets, as parking is inconvenient, taking up shared public space and sometimes reducing emergency vehicle access.

Over-sized duplexes threaten the destruction of historic homes and degrade the livability and attractiveness to homeowners and tenants in all neighborhoods. A duplex is considered 2 units on one property that can house as many as 8 unrelated individuals in multi-family zoned areas and 6 unrelated individuals in single-family zoned areas. City code allows single dwelling units (usually a house) to have 4 unrelated individuals in multi-family zoned areas and 3 unrelated individuals in single-family zoned areas.

Given the high density of duplex units today and that they no longer target families, they should have the same parking standard as all other multi-family and congregate living units- one parking space per BR with NO STACKED PARKING.

Sincerely, Candice Davis LAN Chair



Lawrence Association of Neighborhoods

Dear Planning Commissioners,

5-18-16

The Lawrence Association of Neighborhoods (LAN) has been concerned in recent years about the evolution of duplex zoning and it's increasing negative impact on neighborhoods. We urge you to consider updating these regulations, especially with regard to parking.

Problems with the present Duplex Zoning:

Density should increase incrementally from duplex, triplex, fourplex, and higher multi-dwelling units. Problems have arisen because duplex parking codes allow stacked (tandem) parking. Stacked parking allows an increase in the number of occupants without providing adequate access and regress for vehicles. Additional logistical problems are created when located off of narrow allies versus wide city streets.

Current city parking codes for dwelling units require:

Single-family-lowest density zoning (usually houses, family-living, can stack cars; often in driveways).

Duplex –lowest density zoning in multi-family (can stack cars, may **not** be family-living).

Triplex – one parking space per BR (no stacked parking).

Fourplex- one parking space per BR (no stacked parking).

Larger multi-dwelling units - one parking space per BR plus one space per 10 units for visitors and guests (no stacked parking).

Duplex, triplex, fourplex indicate the number of units on one lot (4 unrelated individuals can live in one unit in multi-zoned areas, 3 unrelated in one unit in single-family zoned areas). The number of BR's is determined by the number of parking spaces that can be provided on each lot. Stacked parking can increase the number of BR's by as much as 50%.

Desirable property development that increase profits is now the 4 BR duplex=2 units=8BRs and the 3BR duplex=2units=6 BRs that allows stacked parking. Four BR and three BR duplexes "game" duplex regulations. The two BR family-oriented units of the past have now become small apartment complexes, appealing to investors, but problems for neighborhoods. Duplex occupants crowd neighborhood streets when they avoid stacking their vehicles and instead use on-street parking spaces.

Duplexes have become high-density, non-family, units in what should be low-density zoned areas. Parking is an effective tool to control density. Why should a duplex be allowed to count stacked spaces as if they were legitimate parking spaces? Such a parking policy leads to an increase in density without providing adequate access and egress for vehicles. This has incentivized building large duplexes in low-density zoned areas.

LAN believes that duplexes should have the same parking requirement as all other multi-dwelling units, one parking space per BR. Updated planning that meets present day use helps promote stability and vitality in the city and in all neighborhoods by encouraging responsible development and redevelopment.

Sincerely, Candice Davis, Lawrence Association of Neighborhoods Chair; Oread Resident